

**Item No:** C1023(1) Item 14  
**Subject:** PARRAMATTA ROAD CORRIDOR STAGE 1 - POST GATEWAY AND COMMUNITY ENGAGEMENT  
**Prepared By:** Daniel East - Acting Senior Manager Planning  
**Authorised By:** Simone Plummer - Director Planning

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**RECOMMENDATION**

1. That Council endorses a six week community consultation for the Planning Proposal for Parramatta Road Corridor - Stage 1, associated draft amendments to Development Control Plans (DCPs) (Leichhardt, Marrickville and Comprehensive Inner West Development Control Plan 2016 (former Ashfield) and supporting studies with:
  - a) A 'meet the planner' drop-in weekend sessions in each precinct (Leichhardt, Taverners Hill, Kings Bay/ Croydon); and
  - b) A community meeting in each precinct (Leichhardt, Taverners Hill, Kings Bay/ Croydon).
2. That Council write to the Department of Planning and Environment seeking alteration of the Gateway Determination to amend the timeframes for finalisation of the proposed Local Environmental Plan amendment by March 2024.
3. That Council delegate the authority to the General Manager to make any minor changes to correct any omissions, errors or inconsistencies to the Planning Proposal and supporting draft Development Control Plan amendments prior to exhibition.
4. That Council receive a post exhibition report for its consideration.

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**STRATEGIC OBJECTIVE**

This report supports the following strategic directions contained within Council's Community Strategic Plan:

2: Liveable, connected neighbourhoods and transport

**EXECUTIVE SUMMARY**

Planning Proposal for Parramatta Road Corridor – Stage 1 begins the incremental realisation of PRCUTS through amendments to the Inner West Local Environmental Plan 2022 (IWLEP 2022) in certain parts of Leichhardt, Taverners Hill and Kings Bay Precincts by facilitating new dwellings and jobs. The Planning Proposal delivers on a key state government priority relating to boosting housing supply and providing affordable housing to meet the 5-year dwelling targets set by the NSW Department of Planning & Environment (DPE).

[Parramatta Road Corridor Urban Transformation Strategy](#) (PRCUTS or 'the Strategy') is a State Government endorsed strategy for the revitalisation of Parramatta Road Corridor given statutory force via Section 9.1 Local Planning Direction under the *Environmental Planning and Assessment (EP&A) Act 1979* in December 2016.

Since the adoption of PRCUTS in 2016, the following has occurred in the corridor's infrastructure planning context:

- NSW Government commenced work on the delivery of Sydney Metro West - Five Dock Metro Station, 800m from Kings Bay Precinct
- Transport for NSW is progressing planning for public and active transport improvements along the road corridor, including linkages for cycleways, intersection improvement for pedestrians.
- The Parramatta Road Urban Amenity Improvement Program has progressed significantly and delivered public domain works along Crystal, Renwick and Catherine Streets.
- Completed transport studies in collaboration with Transport for NSW and DPE for future public and active transport improvements in the corridor
- Recommendations of the Productivity Commission's Infrastructure Contribution's Review (2021) have been adopted

On 20 October 2022, DPE issued a Gateway Determination (*Attachment 1*) for the Planning Proposal to proceed through the statutory Local Environmental Plan (LEP) making process and public exhibition, subject to conditions.

DPE's Gateway Determination required extensive work to be completed to address the Gateway conditions before the proposal can be publicly exhibited. This required additional technical studies relating to flooding, urban design, and sustainability. Recommendations of these studies have triggered the need to update the Planning Proposal and supporting draft Development Control Plans (DCPs).

This report summarises these post-Gateway changes to the Planning Proposal (*Attachment 2*) and draft DCPs (*Attachment 3*) and provides an overview of the upcoming statutory public exhibition process for the proposed LEP/ DCP amendments. A formal request to the DPE is required in recognition of the altered timeframes set out in the Gateway determination due to the additional technical studies, and to allow for sufficient time to undertake consultation with the community.

## BACKGROUND

At the Council Meeting held on 10 May 2022, Council resolved the following in part:

1. *Council supports the Planning Proposal for Parramatta Road Corridor - Stage 1 LEP Phase 2A and the associated draft amendments to Development Control Plans (DCPs) (Leichhardt, Marrickville and Comprehensive Inner West Development Control Plan 2016 (former Ashfield));*
2. *The Planning Proposal is forwarded to the Minister for Planning for a Gateway Determination in accordance with Section 3.33 and 3.34 of the Environmental Planning & Assessment Act 1979;*
3. *Council request delegation from Minister for Planning to manage the plan making functions of the Planning Proposal;*
4. *Should the Planning Proposal receive a favourable Gateway Determination, place the Planning Proposal and draft DCP amendments on public exhibition in accordance with Council's Community Engagement Framework to meet the requirements of the Gateway Determination, Environmental Planning and Assessment Act 1979 and Environmental Planning and Assessment Regulation 2021.*

**DISCUSSION**

**1. Planning Proposal and associated draft DCP amendments**

The Planning Proposal seeks to implement parts of PRCUTS for Leichhardt, Taverners Hill and Kings Bay precincts through amendments to the IWLEP 2022 and associated draft amendments to DCPs (Leichhardt, Marrickville and Comprehensive Inner West Development Control Plan 2016 (former Ashfield). This is the first step in the staged implementation of PRCUTS.

The below map shows the extent of NSW Government’s PRCUTS (in red) and parts of precincts included in this Planning Proposal (in green and pink). This Planning Proposal area has been identified to:

- address DPE’s required shortfall of up to 1600 dwellings in the Inner West; and
- ensure that development occurs in line with infrastructure.

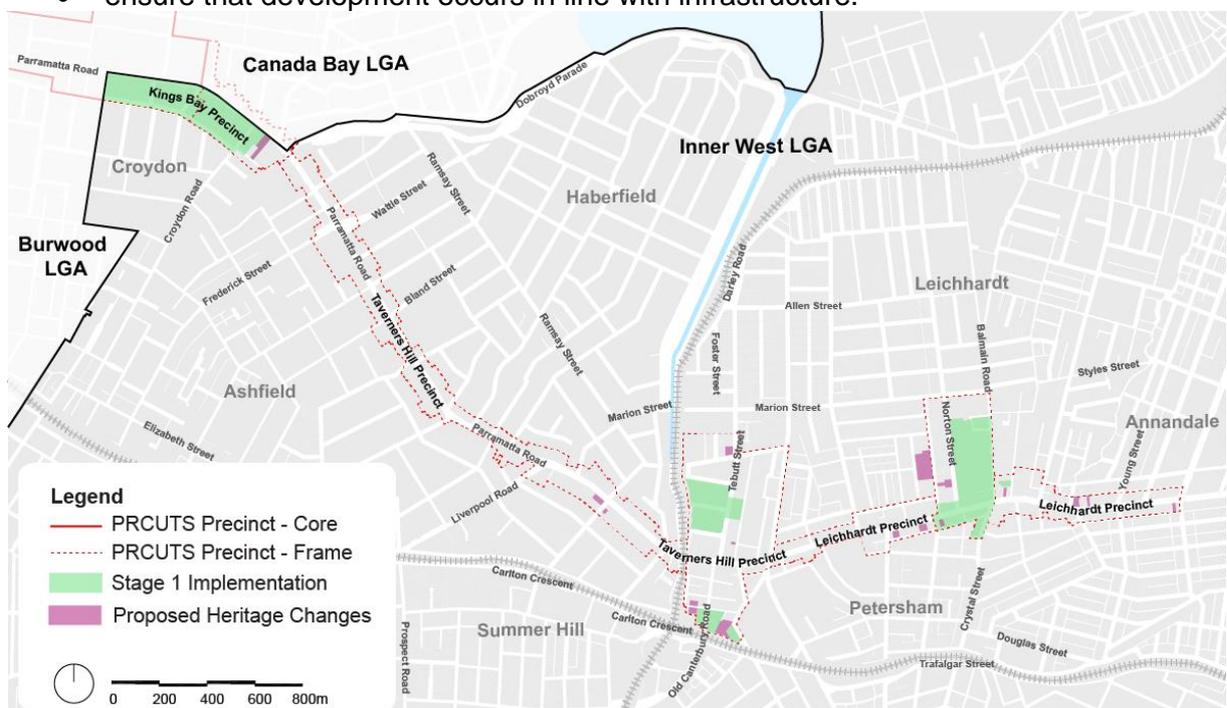


Figure 1 - Map showing the extent of PRCUTS (in red) and the area covered in this Planning Proposal (in green and pink)

**2. Gateway Determination**

A Gateway determination (*Attachment 1*) was issued by DPE on 20 October 2022, allowing the Planning Proposal to proceed through the statutory LEP making process, subject to conditions.

Gateway determination requires the Planning Proposal to address technical matters such as:

- Remove local provisions relating to:
  - Provision of on-street rapid transport along Parramatta Road
  - Tree canopy targets in the public domain
  - Additional heritage local provision
  - Community infrastructure contributions
- Urban design matters in Leichhardt and Taverners Hill Precincts
- Local Planning Direction 4.1 Flooding requiring additional flood impact analysis

- Sustainability targets in line with the Sustainable Buildings State Environmental Planning Policy (SEPP) 2022
- State infrastructure contributions

Since receiving the Gateway Determination, Council officers have been working to address the conditions, in preparation for public exhibition. This has included commissioning additional technical studies to address urban design, flooding and sustainability matters. These have informed adjustments to, and further justified the Planning Proposal.

Detailed consideration of Gateway Determination conditions and subsequent findings of technical studies are discussed in *Attachment 4*.

### 3. Post Gateway changes to the Planning Proposal and draft DCPs

The Planning Proposal and supporting draft DCPs have been revised to address the above Gateway conditions in response to:

- Flood Impact Risk Assessment (WMS Engineering, 2023) advice relating to mitigating flood impact (removal of sites in Kings Bay precinct and additional setbacks required for sites in Taverners Hill south)
- Recommendations in the Urban Design Study (Architectus, 2023) relating to revised Incentive Floor Space Ratios (FSRs), Incentive Height of Building (HOBs) and floor to floor heights
- Recommendations in the Sustainability Study (WSP, 2023) relating to revised sustainability targets and bicycle parking for industrial developments.
- Recommended actions outlined in Parramatta Road Corridor Traffic and Transport Study (Camperdown, Taverners Hill, Leichhardt) and Kings Bay Transport Assessment (Cardno/Stantec, 2022) relating to sustainable transport plan and unbundled parking.
- Inner West Architectural Excellence Design Review Panel feedback of 5 April 2022, such as use of figures and floor to floor heights.

Below is a brief summary of the revised Planning Proposal which has addressed all Gateway conditions:

- Amend land use zones for specific locations in Leichhardt, Taverners Hill and Kings Bay precincts to facilitate new dwellings and jobs.
- Introduce new heritage items and heritage conservation areas.
- Introduce new incentive Floor Space Ratio (FSR) and Height of Building (HOB) development standards.
- Introduce additional local provision that requires developments to satisfy the below criteria to access the incentive FSRs/HOBs:
  - achieve an appropriate development pattern and high-quality built form.
  - provide active street frontages where applicable.
  - achieve specific sustainability targets to transition to low carbon precincts
  - incorporate mechanisms to reduce urban heat.
  - support sustainable transport modes, minimise traffic congestion and reduce private car dependency.
  - provide Affordable housing contributions in Leichhardt precinct (where the change in planning controls is greater than the other 2 precincts).
  - make satisfactory State Infrastructure Contributions (SICs).

For further details regarding these proposed provisions and strategic merit justification, refer to *Attachment 2*. Supporting amendments to the Council's Leichhardt, Marrickville and Ashfield DCPs (*Attachment 3*) have been prepared which provide more detailed controls relating to future built form, public domain, parking, access, lot amalgamation etc.

#### 4. Gateway Determination timeframes

The Gateway Determination required that the Planning Proposal be reported to Council for a final recommendation no later than 9 months from the date of the Gateway determination and the LEP be completed within 12 months from the date of the Gateway Determination i.e. 20 October 2023.

Given the extensive additional work required to satisfy the conditions of the Gateway, these timeframes have not been met. Notwithstanding, Council officers have met with DPE to provide updates on the progress of the Planning Proposal. On 22 August 2023, DPE wrote to Council (*Attachment 1*) to *urgently progress work towards finalising the Parramatta Road proposal by the end of March 2024 to help unlock much needed housing opportunities as soon as possible.*

This requires Council to formally request alteration of the Gateway timeframes and seek an extension until end of March 2024 to finalise this LEP Amendment.

#### 5. Community Consultation

Preparation of PRCUTS and its suite of documents was led by Urban Growth NSW. Considerable community consultation has been undertaken by NSW Government in the preparation of PRCUTS which included:

- Initial community and stakeholder consultation in late 2013
- Community consultation on preliminary draft strategy in late 2014
- Public exhibition of draft strategy from September 2015 to December 2015
- Consultation Outcomes Report released in May 2016

In addition, PRCUTS and its intended outcomes formed a component of consultation undertaken for the:

- Greater Sydney Region Plan, the Eastern City District Plan were extensively exhibited from 2016 to their adoption in 2018, and
- Inner West Local Strategic Planning Statement, Local Housing Strategy, Employment and Retail Lands Strategy and Integrated Transport Strategy that were publicly exhibited by Council during 2018 and 2019.

Stage 5 of DPE's [LEP Making Guideline](#) (August 2023) relates to Public Exhibition and Assessment. DPE's Gateway Determination (*Attachment 1*) sets the minimum requirements relating to duration and extent of public exhibition and public authority consultation for this Planning Proposal.

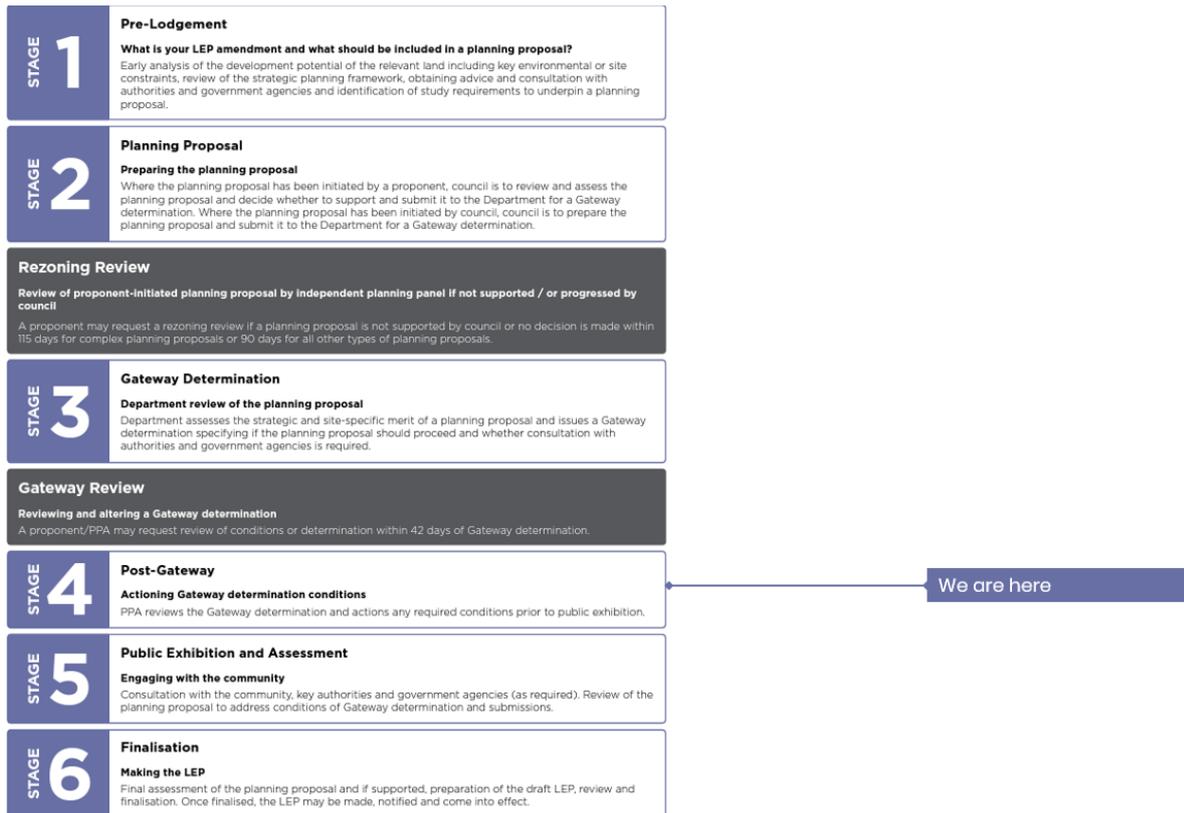


Figure 2 - LEP making process overview

The Planning Proposal associated draft DCP amendments and supporting documentation will be exhibited for a six (6) week period. While the Gateway Determination stipulates a minimum of 28 days, due to the complexity of the technical studies, an extended exhibition period is proposed to provide greater opportunity for the community to access information on the proposal and contribute to the engagement process.

It is anticipated that exhibition will occur later this year, with scheduling to ensure that the extended consultation period concludes prior to the Christmas holiday period. Public exhibition will be carried out in line with the requirements of DPE’s Gateway Determination, Council’s Community Engagement Framework, *Environmental Planning and Assessment Act 1979* and *Environmental Planning and Assessment Regulation 2021*.

Consultation is to include the following:

- Inner West Council’s YourSay website
- Notification letters to affected properties and the surrounding area (owners and occupiers)
- Three precinct-specific community meetings (2 hour weekday sessions) one in each precinct being Leichhardt, Taverners Hill, Kings Bay / Croydon
- Three in-person, precinct-specific, drop-in sessions (weekends) at each precinct being Leichhardt, Taverners Hill, Kings Bay / Croydon
- Contact details of Strategic Planners
- Exhibition material hard copies in Council Service Centres and libraries
- Notification to public authorities
- DPE’s Planning Portal
- Local Democracy Groups/Advisory Committees

Following community consultation council officers will review community submissions and update the documentation to address community concerns. The updated proposal will then be presented to the Architectural Excellence Design Review Panel and the Inner West Local Planning Panel. The updated proposal and post-exhibition report with community engagement outcomes will be presented to Council for consideration at the March 2024 meeting.

### **FINANCIAL IMPLICATIONS**

There are no financial implications associated with the implementation of the proposed recommendations outlined in the report. Budget is available for the community consultation process outlined above. Subject to this resolution, if there are any additional requirements, there may be funding implications which would need to be resolved.

### **List of Planning Proposal Supporting Studies**

Due to their large size, please follow the below link to Council's website to access the attachments listed below:

[Parramatta Road Corridor Stage 1 Implementation - Inner West Council \(nsw.gov.au\)](https://www.nsw.gov.au/inner-west-council/planning-proposals/parramatta-road-corridor-stage-1-implementation)

- Appendix 1 – Proposed LEP Maps
- Appendix 2 – Urban Design Studies
  - Appendix 2a: Leichhardt, Taverners Hill and Kings Bay/ Croydon Urban Design Review (Architectus, 2023)
  - Appendix 2b: Inner West Council's Draft Structure Plan October 2020
  - Appendix 2c: Leichhardt Council's Parramatta Road Urban Design Study 2016
- Appendix 3 – Heritage Study and Inventory Sheets
- Appendix 4 – Economic Assessment
- Appendix 5 – Flood Impact Risk Assessment (WMS, 2023)
- Appendix 6 – Preliminary Site Investigations
- Appendix 7 – Aircraft Noise Assessment
- Appendix 8 – Draft Affordable Housing Contribution Scheme
- Appendix 9 – Parramatta Road Corridor Stage 1 Sustainability Study (WSP, 2023)
- Appendix 10 – Parramatta Road Corridor Precinct-wide Traffic and Transport Study and its Implementation (Camperdown, Leichhardt, Taverners Hill and Kings Bay/ Croydon precincts)
- Appendix 11 – Parramatta Road Corridor Implementation Stage 1 - Justification Study
- Appendix 12 – Parramatta Road Urban Amenity Improvement Plan (Inner West Council Masterplan)

### **ATTACHMENTS**

1. [↓](#) Gateway Determination and Department of Planning letter to Council dated August 2023
2. [↓](#) Planning Proposal Parramatta Road Stage 1 - Post Gateway
3. [↓](#) Draft Development Control Plan Amendments
4. [↓](#) Summary of Post Gateway changes



Department of Planning and Environment

## Gateway Determination

**Planning proposal (Department Ref: PP-2022-1921):** to amend the Inner West Local Environmental Plan 2022 to implement Stage 1 of the Parramatta Road Corridor Urban Transformation Strategy 2016 (PRCUTS) in parts of the PRCUTS Precincts of Leichhardt, Taverners Hill and Kings Bay.

I, the Executive Director, Metro East and South at the Department of Planning and Environment, as delegate of the Minister for Planning, have determined under section 3.34(2) of the *Environmental Planning and Assessment Act 1979* (the Act) that an amendment to the Inner West Local Environmental Plan 2022 to implement Stage 1 of the Parramatta Road Corridor Urban Transformation Strategy 2016 (PRCUTS) in parts of the PRCUTS Precincts of Leichhardt, Taverners Hill and Kings Bay should proceed subject to the following conditions:

1. Prior to public exhibition, the planning proposal (including relevant appendices) is to be revised to address the matters set out below:
  - (a) include an explanatory note that future development will be subject to state/regional infrastructure contributions in accordance with the implementation actions in the Parramatta Road Urban Corridor Transformation Strategy Implementation Update 2021;
  - (b) include a figure or figures in the planning proposal that clearly identify all land and sites that are subject to the planning proposal;
  - (c) address consistency with section 9.1 Direction 1.5 Parramatta Road Corridor Urban Transformation Strategy, including:
    - i. the proposal seeks a height of 23m rather than 17m and a FSR of 3:1 rather than 1.9:1 for 97 Norton Street, Leichhardt. Appendix 12 to the planning proposal refers to the Urban Design Study, however it is unclear that the site is specifically discussed in the Urban Design Study;
    - ii. the proposal seeks a height of 23m rather than 17m and a FSR of 3:1 rather than 1:1 for 23 Norton Street, Leichhardt. Appendix 12 to the proposal acknowledges the FSR variation but not the height variation. Update Appendix 12 to the planning proposal to acknowledge the inconsistency and provide justification; and
    - iii. part of 35-53 Old Canterbury Road, Lewisham is proposed to remain with a FSR of 1.1:1 and is identified on the proposed FSR incentive map as 1.1:1. The incentive FSR map is to be updated accordingly to remove the area from the map.
  - (d) address consistency with section 9.1 Direction 4.1 Flooding, including:
    - i. update the planning proposal to address relevant recommendations of the NSW Government's 2022 Flood Inquiry Report;
    - ii. clearly address the requirements of Direction 4.1, providing clear assessment and consideration the level of flood hazard(s) that may impact the proposal; and
    - iii. remove references to outdated Direction 4.1 numbering.

- (e) to contemplate the suitability of the use of the R1 General Residential and/or R4 High Density Residential zones under Inner West LEP 2022 to remove the need to rely upon 'residential flat buildings' as an additional permitted use for land zoned R3 Medium Density Residential;
  - (f) remove the proposed additional heritage local provision;
  - (g) review and correct as required existing and proposed maximum building heights and floor space ratio provisions to ensure the planning proposal and proposed mapping are consistent;
  - (h) to include an assessment of the proposed sustainability provisions against *State Environmental Planning Policy (Sustainable Buildings) 2022* (Sustainable Buildings SEPP). This must outline how the proposed incentive targets relate to the targets set out in the Sustainable Buildings SEPP.
  - (i) in relation to the proposed performance standards for non-residential development, update the proposal to use the development type term 'office' rather than 'commercial development' or provide justification as to why the term commercial development is preferred;
  - (j) include a table in the planning proposal that clearly demonstrates indicative zoning under the Department's employment zones reforms;
  - (k) amend the proposed workers facilities provision to reframe it as an overarching clause setting out aims and objectives, the detailed requirements may be contained in a Development Control Plan (DCP);
  - (l) remove references to the finalisation of the draft Design and Place State Environmental Planning Policy (SEPP) 2021;
  - (m) remove the proposed clause that considers reduced sustainability requirements for heritage items;
  - (n) remove the proposed overall precinct/zone based tree canopy targets (including streets);
  - (o) remove the proposed incentive requirement for all car parking to be provided as unbundled parking in new developments;
  - (p) provide a plain English explanation of intent for the proposed community infrastructure contributions (CIC) clause for the Leichhardt Precinct, noting that the Department is unable to support a CIC levy that does not conform with the existing legislative framework for infrastructure funding under the *Environmental Planning and Assessment Act 1979*;
  - (q) remove the two proposed transport infrastructure provisions; and
  - (r) update the project timeline to reflect the progress of the planning proposal and Gateway timeframes.
2. Consultation is required with the following public authorities:
- Ausgrid;
  - Adjoining Councils;
  - Greater Cities Commission;
  - Commonwealth Department of Infrastructure, Transport, Regional Development and Communications;
  - Department of Education;
  - Environment and Heritage Group of the Department of Planning and Environment;
  - Environment Protection Agency;
  - Heritage NSW;

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- Jemena;
  - NSW Health;
  - State Emergency Service;
  - Sydney Airport Corporation;
  - Sydney Metro;
  - Sydney Trains;
  - Sydney Water Corporation; and
  - Transport for NSW.
3. Prior to finalisation, the planning proposal to be updated to:
    - (a) address the Implementation Actions in the *Parramatta Road Urban Corridor Transformation Strategy Implementation Update 2021* to:
      - i. ensure the planning proposal aligns with any transport or infrastructure plan developed by the NSW Government; and
      - ii. address the recommendations and outcomes of the Precinct-wide traffic studies.
    - (b) provide additional analysis demonstrating that the tree canopy targets (% of site area) and the deep soil target can be achieved on a site-by-site basis;
    - (c) provide feasibility analysis considering the zoning, height and floor space ratio and other requirements for development including design excellence, affordable housing contributions, local and community infrastructure requirements and contributions, state or regional contributions and sustainability requirements. This updated analysis should also account for any amendments to the planning proposal that occur as part of the plan making process; and
    - (d) ensure that the thresholds for BASIX standards which trigger the incentive provisions are appropriate having regard to the Sustainable Buildings SEPP.
  4. The planning proposal should be made available for community consultation for a minimum of 28 days.
  5. The planning proposal must be placed on exhibition no later than **5 months** from the date of the Gateway determination.
  6. The planning proposal must be reported to Council for a final recommendation no later than **9 months** from the date of the Gateway determination.
  7. The timeframe for completing the LEP is to be **12 months** from the date of the Gateway determination.
  8. Given the nature of the proposal, Council is not authorised to be the local plan-making authority.

Dated 20<sup>th</sup> day of October 2022.



**Amanda Harvey**  
Executive Director, Metro East and South  
Planning and Land Use Strategy  
Department of Planning and Environment

Delegate of the Minister for Planning

PP-2022-1921(IRF22/1918)

Department of Planning and Environment



Our ref: IRF23/1987

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**Subject: Gateway timeframes - Inner West Stage 1 PRCUTS planning proposal (PP-2022-1921)**

Dear Peter,

As discussed earlier today with you and Simone, I understand my team met with Council recently regarding ongoing delays to this planning proposal across Leichhardt, Taverner's Hill and Kings Bay. If progressed, this planning proposal would represent a critical step towards the delivery of Council's Local Housing Strategy.

Council previously committed to submit the proposal to the department for finalisation by the end of June 2023. However, Council has yet to exhibit the proposal.

I understand Council now seeks a further extension, and to submit the proposal to the department by June/July 2024 for finalisation. These timeframes significantly exceed those set out in the department's Local Environmental Plan Making Guideline and cannot be supported.

I want to emphasise the need for the proposal to be finalised by Council, and submitted to the department no later than March 2024. This already represents a five month extension beyond the date stipulated in the Gateway determination of 20 October 2022.

The Comprehensive Transport Assessment for the Parramatta Road Corridor Urban Transformation Strategy (PRCUTS), on which this proposal is dependent, was completed over 2 years ago. The department is aware that Council started to prepare this proposal prior to this transport assessment being completed.

While I support the Council's work to consult thoroughly with the community, this proposal has been in preparation for several years. I ask Council to urgently progress their work towards finalising this proposal by the end of March 2024 to help unlock much needed housing opportunities as soon as possible.

This is the Inner West Council's only substantive planning proposal that unlocks significant housing supply. This is important given Council is not projected to meet its housing target of 5,000 dwellings between 2021 and 2026: dwelling completions for the first year of this period were 559 and forecast growth for the period from mid-2021 to mid-2026 is between 2,589 and 3,104 – well short of the target.

Governments at all levels have a shared responsibility to address the housing crisis and meet the goals of the National Housing Accord. That shared responsibility also requires a strong sense of

Department of Planning and Environment



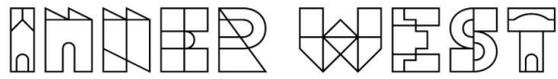
urgency. This proposal offers the opportunity to deliver up to 1,700 dwellings and is an important first step for Council to unlock more housing supply.

Should you wish to discuss this matter further please contact Katie Joyner, Director Eastern District City of Sydney on 9585 6083 or via [Katie.Joyner@dpie.nsw.gov.au](mailto:Katie.Joyner@dpie.nsw.gov.au).

Yours sincerely

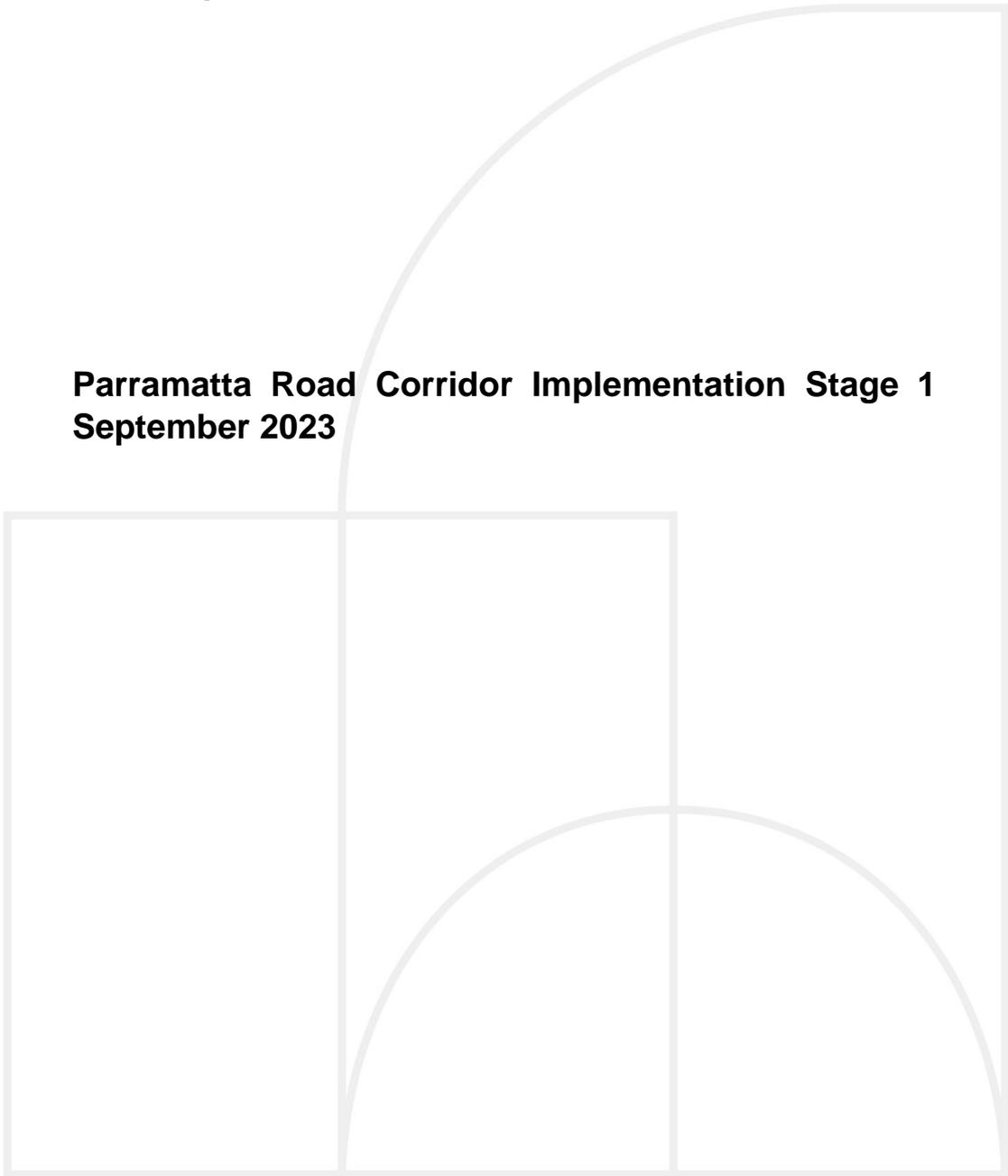
A handwritten signature in blue ink that reads "Marcus Ray".

Marcus Ray  
Deputy Secretary



**Planning Proposal**

**Parramatta Road Corridor Implementation Stage 1  
September 2023**



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### 1.0 Executive Summary

*Parramatta Road Corridor Urban Transformation Strategy* (PRCUTS) is the NSW Government’s endorsed strategy for revitalisation of the Parramatta Road Corridor. This Planning Proposal seeks to facilitate the vision and strategic objectives of PRCUTS through a range of amendments to the *Inner West Local Environmental Plan 2022* (IWLEP). The PRCUTS precincts of Leichhardt, Taverners Hill and Kings Bay/ Croydon affected by this IWLEP amendment are shown in Figure 1 below.

The Planning Proposal area has been identified to address the Department of Planning and Environment’s (DPE) requirement that a short to medium term shortfall of up to 1600 dwellings in the Inner West be met. This Planning Proposal brings forward development in only certain parts of the Inner West’s PRCUTS precincts to initiate the incremental transformation of the Corridor. There are opportunities for additional growth beyond this Planning Proposal area which will be delivered through future Council-led LEP amendments.

The Planning Proposal gives effect to several planning priorities and actions in Inner West Council’s Local Strategic Planning Statement (LSPS). It is also underpinned by a comprehensive evidence-based strategic planning process which has been devised to refine the PRCUTS recommendations and to give effect to the Eastern City District Plan and Council’s LSPS, Local Housing, Employment and Retail Lands and Integrated Transport Strategies.

This Proposal has been prepared in accordance with Section 3.33 of the *Environmental Planning and Assessment Act 1979* (the Act) and the DPE’s *Local Environmental Plan Making Guideline*, dated August 2023 (formerly ‘*A guide to preparing planning proposals*’ and ‘*A guide to preparing local environmental plans*’ 2016).

This Planning Proposal relates only to proposed amendments to the IWLEP. Supporting amendments to the existing Development Control Plans (DCPs) have been prepared concurrently with this Planning Proposal.

This Planning Proposal received a Gateway Determination from DPE on the 20<sup>th</sup> of October 2022. The conditions included in the Gateway Determination have been actioned, and as a result, changes have been made to this Planning Proposal. Many of these changes have been informed by further technical studies required by the Gateway conditions.

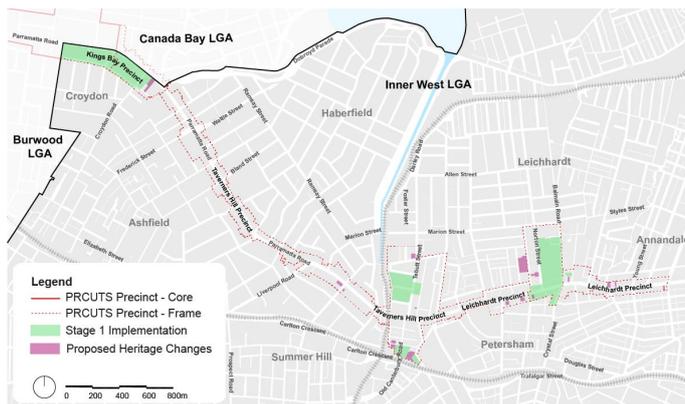


Figure 1 - Map showing the extent of PRCUTS (in red) and the area covered in this Planning Proposal (in green and pink)

## 2.0 Background

This Planning Proposal seeks to implement parts of the [Parramatta Road Corridor Urban Transformation Strategy](#) (PRCUTS or 'the Strategy') for Leichhardt, Taverners Hill and Kings Bay/ Croydon precincts. This is the first step in the staged implementation of PRCUTS which will kickstart the transformation of the Corridor.

PRCUTS is a State Government endorsed strategy for the revitalisation of Parramatta Road Corridor given statutory force via a Section 9.1 Local Planning Direction in December 2016. The Strategy sets out key actions associated with land uses; transport and movement; place-making; and open space, linkages, and connections; and makes zoning, height, and density recommendations. The Strategy is supported by an Implementation Tool Kit including the following documents:

Parramatta Road Corridor Urban Transformation Strategy  
Implementation Tool Kit:

Implementation Plan Update 2021  
Implementation Plan 2016 - 2023  
Planning and Design Guidelines  
Infrastructure Schedule  
Urban Amenity Improvement Plan

Reference Reports:

Sustainability Implementation Plan  
Precinct transport report  
Fine Grain Study

Since the adoption of the Strategy in 2016, Greater Cities Commission (GCC, formerly Greater Sydney Commission), DPE and Council have undertaken holistic strategic planning work including preparation of the Greater Sydney Region Plan 2018 (GSRP), the Eastern City District Plan 2018 (ECDP), Council's LSPS, Local Housing Strategy (LHS), Employment and Retail Lands Strategy (EaRLS), Integrated Transport Strategy (ITS).

These strategies provide an evidence base for strategic planning of the Parramatta Road Corridor and trigger the need to update and improvise the Strategy's recommendations to remain relevant to the current Inner West planning context. In June 2021, DPE also released the Parramatta Road [Implementation Update 2021](#) which is supplementary to the original PRCUTS Implementation Plan 2016-2023 and has been considered in the preparation of this Planning Proposal.

This Planning Proposal puts forward an innovative set of outcomes which will enable urban renewal of parts of the Corridor. These outcomes broadly align with PRCUTS' principles, and any variations to PRCUTS are improvements that respond to recent and emerging strategic planning issues to deliver better outcomes.

Council has also undertaken detailed technical studies for the Parramatta Road Corridor dealing with urban design, heritage, transport, feasibility, contamination, flooding, and recreation and community needs that inform this Planning Proposal. These studies and Council's own strategic plans support this Planning Proposal, which will deliver enhanced outcomes than those envisaged in the Strategy. The below image illustrates the supporting evidence base developed for this Planning Proposal:

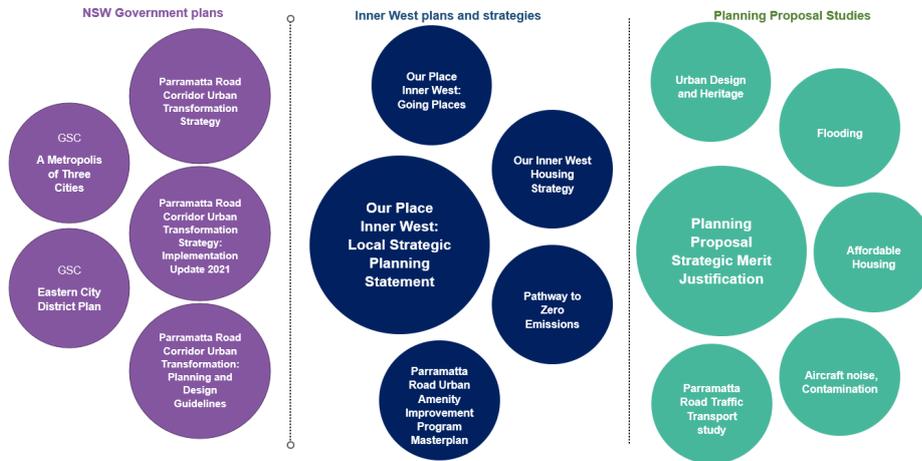


Figure 2 – Planning Proposal Evidence Base

6 key moves which will be actioned through this Planning Proposal are:

- **Key Move 1: Ensure a place-based approach – provide residential and employment opportunities (including affordable housing and employment) in new developments which respond to local context**

This will be delivered through new planning controls based on the recommendations of the Parramatta Road Corridor Planning and Design Guidelines and refined to ensure that new developments respond to the existing and desired future character of the local area.

PRCUTS is a broad-brush, high-level strategy covering a corridor of over 20km from Camperdown to Granville. Its land use and built form recommendations require review and refinement to ensure that these relate to the local context.

Parts of the PRCUTS’ precincts (including certain individual sites) have been strategically selected and included in this Planning Proposal to achieve this Key Move. The Planning Proposal also includes an Affordable Housing Contribution Scheme for Leichhardt precinct to assist with delivering diverse housing in the Inner West.

- **Key Move 2: Protect, enhance and expand Inner West’s heritage.**

The Planning Proposal will achieve this through introduction of new heritage items and addition/extension of heritage conservation areas whilst also protecting the significance of existing and proposed heritage items/areas as new development occurs in each precinct.

- **Key Move 3: Move people more easily by prioritising streets for walking and cycling and expanding Inner West’s walking, cycling and open space network.**

This Planning Proposal fully supports and implements PRCUTS vision of a revitalised Parramatta Road Corridor, which is dependent on the provision of improved public transport (on-street rapid transit system) and reduced reliance on private cars. This can be achieved through the Planning Proposal’s requirements for reduced car parking

to encourage sustainable transport as well as Council's advocacy to NSW Government to prioritise the delivery of on-street rapid transit system along Parramatta Road. The Planning Proposal also includes provisions for new open spaces and through-site links which would enhance and expand Inner West's walking and cycling network. The supporting DCP amendments include built form provisions which will enhance the public domain through provision of adequate building setbacks to key roads.

- **Key Move 4: Create a sustainable Inner West by implementing improved energy and water standards for new developments**

This Planning Proposal seeks to implement planning controls for high-performing buildings to achieve higher energy and water targets above the standard NSW mandatory requirements. This key move will be achieved through new planning controls that will allow developments to access additional development capacity if they deliver higher than minimum sustainability outcomes.

The Planning Proposal also includes provisions to address urban heat issues by maximising tree canopy cover and deep soil planting in the private domain. In addition, the supporting draft DCPs include provisions for mitigating urban heat island effect. This includes controls for maximising retention of existing mature trees.

- **Key Move 5: Stage development across the corridor to ensure that development aligns with infrastructure.**

This Planning Proposal brings forward development in only parts of the PRCUTS precincts to initiate the incremental transformation of the Corridor. This approach will help ensure the timing of new development is aligned with the delivery of necessary infrastructure in accordance with Objective 1.5(c) of Local Planning Direction 1.5 Parramatta Road Urban Transformation Strategy.

This Proposal has prepared in accordance with the phasing proposed in the Parramatta Road Corridor Implementation Plan 2016-2023 and the more recent timing of release as per [Parramatta Road Corridor Implementation Update 2021](#). The Implementation Plan Update 2021 expands the release area boundaries to include whole precincts identified with solid red boundary in PRCUTS. This would mean that the proposed Staging Plan in this Planning Proposal for Leichhardt and Taverners Hill precinct is fully consistent with the Staging Plan in the Implementation Plan Update 2021. For any properties outside these release areas, Appendix 11 provides detailed justification study regarding demonstrating the delivery of better outcomes.

This updated Staging Plan addresses the DPE's requirement that a short-term shortfall of up to 1600 dwellings in the Inner West be met, whilst aiming to deliver the remaining growth in the Corridor in the medium to long term. This approach of development having to be aligned with infrastructure provision is consistent with the objectives of the ECDP.

The Planning Proposal includes provisions for developments to access additional development capacity by making adequate Regional infrastructure contributions.

The timely delivery of infrastructure with growth will also be addressed through collection of local infrastructure contributions via the Inner West Local Infrastructure Contributions Plan 2023.

- **Key Move 6: Monitor outcomes and respond to issues that arise to ensure the Parramatta Road Corridor Urban Transformation Strategy's ongoing success in revitalising the corridor.**

The outcomes of this Planning Proposal such as delivery of number of jobs and dwellings, including affordable dwellings; and delivery of infrastructure will be monitored. Future planning proposals for medium and long-term growth in the Corridor will respond to the outcomes of this Planning Proposal and any other emerging issues to achieve successful transformation of the Corridor.

Elements of Key Moves 3, 5 and 6 go beyond the LEP and therefore cannot be acted upon in entirety through this Planning Proposal. These key moves require coordination and collaboration with the NSW Government. Inner West will continue to proactively discuss the full range of planning requirements, especially relating to transport and infrastructure with the DPE and Transport for New South Wales (TfNSW).

## 2a. Staging Plan

The Planning Proposal and associated technical studies deal with the following Inner West Council sections of the Parramatta Corridor:

- Leichhardt Precinct
- Taverners Hill Precinct
- Kings Bay/ Croydon Precinct

Part of the Camperdown precinct also falls within Inner West Council; however, it will be dealt with separately through a future LEP amendment in collaboration with the Greater Cities Commission to implement the strategic vision for Tech Central (formerly Camperdown-Ultimo Collaboration Area).

In line with Key Move 5, this Planning Proposal only covers those parts of the Leichhardt, Taverners Hill and Kings Bay/ Croydon Precincts that are necessary to:

- address DPE's required shortfall of up to 1600 dwellings in the Inner West; and
- ensure that development occurs in line with the timely delivery of infrastructure.

Figure 3, the map below shows the parts of the precincts which are being brought forward (Council's Stage 1 Implementation area coloured in green and pink; NSW Government's PRCUTS precinct and frame area boundaries in red).

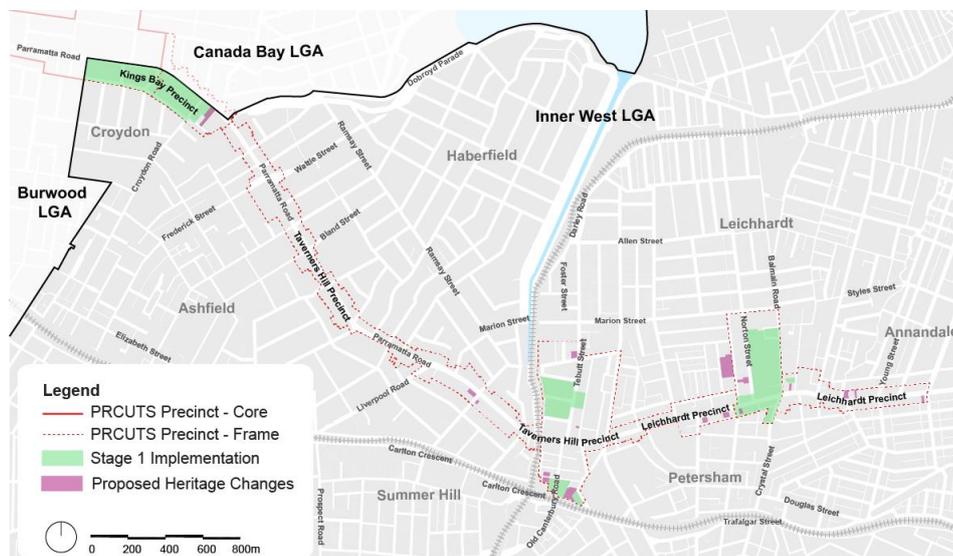


Figure 3 – Map showing the land within Council's Stage 1 of PRCUTS Implementation

The first stage of the Implementation Area has been selected to focus growth adjacent to key local economic centres, such as Norton Street in Leichhardt; and where there is existing transport infrastructure or committed improvements to transport infrastructure, such as Lewisham Railway station, Taverners Hill Light Rail Station and the proposed Five Dock Metro Station. This would enable Key Moves 1, 3, 4 and 5.

The Planning Proposal will support residential, economic and employment growth in these precincts aligned with the PRCUTS and subsequent commitment from NSW Government for Sydney Metro West. There are opportunities for additional growth in the remainder of the Parramatta Road Corridor beyond this Planning Proposal area which will be delivered through future Council-led LEP amendments.

The following sections discuss the individual precincts within the Stage-1 Implementation area.

**2.11 Leichhardt Precinct**

The Planning Proposal area has been strategically selected to align with the core precinct boundaries. It targets growth mostly around Parramatta Road/ Norton Street with the intention of revitalising the Norton Street town centre. The existing Hay Street car park owned by TfNSW will become new open space and a major active transport route. The west side of Norton Street has been excluded until further investigation/ studies are undertaken to resolve complex issues such as waste collection, services and parking in the heritage conservation area.

This Planning Proposal has the capacity to deliver 707 new homes with 1343 new residents and 1378 new jobs in the Leichhardt precinct (shown in green below).



Figure 4 – Map showing PRCUTS Leichhardt Precinct (in red), and the specific area included in this Planning Proposal (in green)

**Note: PRCUTS Implementation Plan 2016-2023**

PRCUTS proposes 880 new dwellings and 3250 new jobs in Leichhardt in the short-term 2016 – 2023.

PRCUTS assumed a household size of approximately 1.9 per dwelling which has been used to determine short-term population projections.

**2.12 Taverners Hill Precinct**

The Planning Proposal area has been strategically selected to locate new residential developments close to existing public transport infrastructure around Lewisham Railway Station and Taverners Hill Light Rail Station. Employment lands have been excluded from this first stage of implementation as at the time of preparation of this Planning Proposal, DPE was reviewing all Employment Zones. The next stage of implementation will review the remainder of the area

The large residential developments between George and Upward Streets, known as Kolotex and Labelcraft, have been excluded as these have already implemented the PRCUTS recommendations and contribute to meeting the Strategy’s dwelling and population growth targets.

This Planning Proposal has the capacity to deliver 393 new homes with 786 new residents in the Taverners Hill precinct (shown in green below).

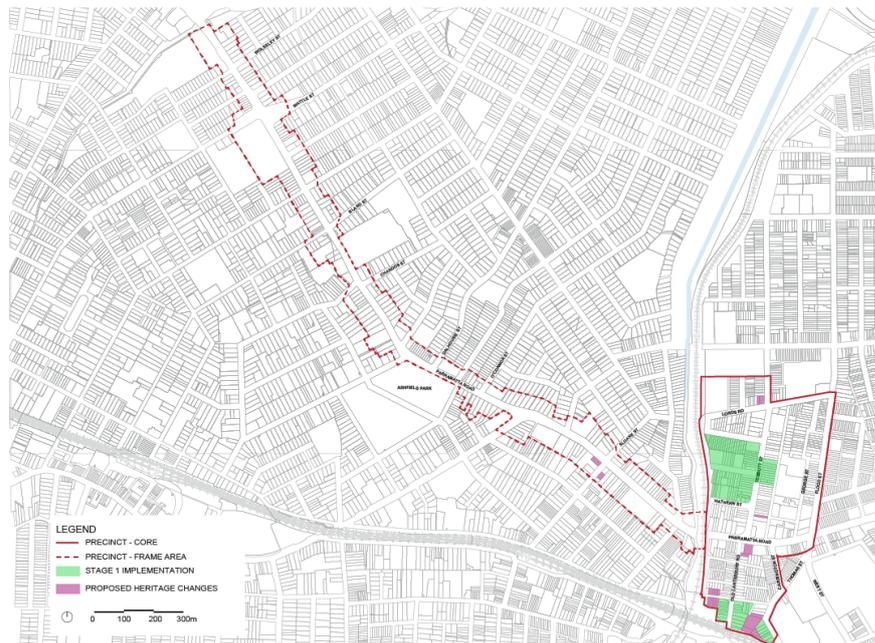


Figure 5 - Map showing PRCUTS Taverners Hill Precinct (in red) and the specific areas included in this Planning Proposal (in green)

**Note: PRCUTS Implementation Plan 2016 - 2023**

PRCUTS proposes 451 new dwellings and 3750 new jobs in Taverners Hill in the short-term 2016 – 2023. Part of this has already been achieved through redevelopment of Kolotex and Labelcraft sites.

PRCUTS assumed a household size of approximately 2 people per dwelling which has been used to determine short-term population projections.

**2.13 Kings Bay/ Croydon**

This Planning Proposal includes most of the Inner West Council portion of the Kings Bay/ Croydon precinct (including core precinct and frame area). This is to provide new residential and employment opportunities close to the proposed Kings Bay/ Croydon (Five Dock) Metro Station which will be approximately 800m from this part of the precinct.

Specific sites to the east of the precinct were investigated but excluded from this Planning Proposal because they are already built up to their maximum potential. Additionally, existing employment zoned sites along West St have been excluded.

This Planning Proposal has the capacity to deliver 416 new homes with 790 new residents and 566 new jobs in Kings Bay/ Croydon precinct (shown in green below).



Figure 6 - Map showing PRCUTS Kings Bay/ Croydon Precinct (in red) and the specific area included in this Planning Proposal (in green)

**Note: PRCUTS Implementation Plan**

PRCUTS does not specifically state Inner West’s anticipated new dwellings and jobs in the Kings Bay/ Croydon precinct as the precinct is split across three Council areas (Inner West, Canada Bay and Burwood). The Plan proposes a total of 1410 new dwellings and 2900 new jobs in Kings Bay/Croydon in the short-term 2016 – 2023 of which around 100 new dwellings and 275 new jobs would be in the Inner West.

PRCUTS assumed a household size of approximately 2 people per dwelling which has been used to determine short-term population projections.

The summary of potential dwellings and jobs which can be delivered through this Planning Proposal in the short-term 2022-2026 is provided below.

Table 1 - Proposed growth projections for this Planning Proposal

Precinct	Additional dwellings	Additional population	Additional jobs
Leichhardt	707	1343	1378
Taverners Hill	393	786	0
Kings Bay/ Croydon	416	790	566
<b>Total</b>	<b>1516</b>	<b>2919</b>	<b>1944</b>

For comparison, below are PRCUTS Implementation Area 2016-2023 projections:

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Table 2 – Proposed 2016-2023 growth projections in PRCUTS

Precinct	Additional dwellings	Additional population	Additional jobs
Leichhardt	880	1680	3250
Taverners Hill	451	900	3750
Kings Bay/ Croydon (part of IWC)	100	200	275
<b>Total</b>	<b>1431</b>	<b>2780</b>	<b>7275</b>

Refinements to PRCUTS Staging Plan are essential as its 2016-2023 target is insufficient to meet the DPE’s short to medium shortfall of 1600 dwellings.

This Planning Proposal will create the opportunity for 1516 new dwellings and 1944 new jobs in the selected areas of Leichhardt, Taverners Hill and Kings Bay/ Croydon precincts.

It is also worth noting that analysis undertaken by Council and DPE’s Employment, Population, Land use and Analytics team has confirmed that there are discrepancies in PRCUTS proposed growth projections. PRCUTS has significantly over-estimated the number of jobs and under-estimated the number of dwellings in the Inner West area i.e., if Council were to implement PRCUTS’ land use and density recommendations on face value, the resultant number of dwellings would be much higher than those envisaged by PRCUTS, and the resultant number of jobs would be much lower than those envisaged by PRCUTS. This triggers the need for Council to review PRCUTS recommendations to deliver the growth numbers envisaged in the Strategy whilst ensuring place-based outcomes.

For simplification, this Planning Proposal will refer to the area subject of this proposal as Leichhardt, Taverners Hill and Kings Bay/Croydon precincts.

In addition to the above areas, new heritage items and heritage conservation areas (including extension of an existing heritage conservation area) are proposed throughout the Inner West part of the Parramatta Road Corridor plus an additional site at 38-40 Renwick Street, Leichhardt which is outside the PRCUTS Leichhardt Precinct, although recommended for heritage listing by PRCUTS.

**2b. Council’s long-term Implementation Plan**

There are additional residential and employment opportunities in the Corridor beyond Council’s Stage-1 area which will be realised through future LEP amendments. Council will be implementing the PRCUTS in areas which are outside the scope of this Planning Proposal through future Council-led planning proposals or proponent-led proposals from 2026 onwards.

The outcomes of this Planning Proposal, such as delivery of jobs and dwellings, including affordable dwellings, and delivery of infrastructure will be monitored. The future Planning Proposals for medium- and long-term growth in the Corridor will respond to the outcomes of this Planning Proposal and any other emerging issues to achieve successful transformation of the Corridor. The medium- to long-term scenario also relies on adequate availability of infrastructure before any new development takes place. This is consistent with Council’s LHS Implementation Plan and the corresponding approval conditions imposed by DPE.

Council will also continue collaborating with DPE and TfNSW to implement the actions relating to transport and infrastructure which require joint and coordinated efforts from different levels of government and are essential to support revitalisation of this Corridor.

### 3.0 Existing Planning Controls and Character

The section below summarises the existing controls, character and built form of the specific areas of the three precincts which are subject of this Planning Proposal. Detailed character analysis and planning controls can be found in the Urban Design Study (Appendix 2).

#### Leichhardt Precinct

The Leichhardt Precinct Stage 1 Area ('Leichhardt Precinct') is predominantly zoned E1 Local Centre with a maximum Floor Space Ratio (FSR) of up to 1.5:1 in the IWLEP. The area has a fine grain land use pattern, with mixed-uses fronting Parramatta Road and Norton Street. It includes a mix of building typologies and uses including offices and commercial premises, shops, restaurants, Norton Street Plaza and cinema, and is surrounded by low and medium density residential areas. The Norton Street Plaza is the Precinct's commercial core and Leichhardt Public School defines the northern boundary of the Precinct.

Building heights across the area generally range from 1-3 storeys. The tallest building is the Italian Forum at 7 storeys. The buildings fronting Parramatta Road are largely 2 storey Victorian shop top premises with a consistent medium to fine-grain, zero setbacks and awnings that cover the pavement. An array of building typologies is found throughout the remainder of the area, including warehouses, mid-block commercial buildings and terrace houses.

The local character is a mix of new and old developments, adaptively re-used terraces, highly articulated and detailed heritage buildings and contributory items. Much of the area is within heritage conservation areas, with many buildings contributing positively to the streetscape and heritage.

#### Taverners Hill Precinct

The Taverners Hill Precinct Stage 1 area ('Taverners Hill Precinct') includes two distinct residential areas: Taverners Hill South in Lewisham, currently zoned R2 Low Density Residential and R4 High Density Residential; and Taverners Hill North in Leichhardt, currently zoned R1 General Residential.

The Taverners Hill South area predominantly includes single-storey residential Federation style detached and semi-detached dwellings, approximately 5-6m in height. West of Old Canterbury Road and Thomas Street have an FSR of 0.6:1 (up to 1.1:1) and maximum height of 9.5m in the IWLEP. While east of Old Canterbury Road, between Barker Street and the Heavy Rail Line has maximum FSRs of 1:1 and 1.1:1 and a maximum height of 17m. Lewisham Station is within 200-400m walking distance. The surrounding area is served by a range of uses including warehouses, storage facilities, offices and commercial premises, shops, places of worship and a public school.

The Taverners Hill North area includes low-scale semi-detached and detached dwellings, predominantly 1-2 storeys in height. The maximum FSRs range from 0.5:1 – 0.8:1 with no height controls in the IWLEP. Taverners Hill Light Rail Station is in approximately 200-400m walking catchment of the area.

The more recent developments along Upward Street and George Street are the tallest buildings in the area ranging from 4 to 8 storeys. The surrounding area also provides an important cluster of communal facilities including the large Marketplace shopping centre, Kegworth Public School, the Boomalli Aboriginal Artist Co-operative at Flood Street, the ArtEst art school at Lords Road, the Leichhardt Community Church, two family day-care centres and Lambert Park. The Greenway runs along the western boundary of the Precinct.

**Kings Bay/ Croydon Precinct**

The Kings Bay/ Croydon Precinct Stage 1 area ('Kings Bay/ Croydon Precinct') includes E3 Productivity Support zoned sites fronting Parramatta Road and R2 Low Density Residential area fronting Dalmar Street. The E3 area fronting Parramatta Road includes commercial and bulky goods, supplies and services, car showrooms, vehicle repair, retail, food and drink premises, service station, shop-top housing and some existing dwelling houses. The building typology includes primarily single storey industrial showrooms, workshops and warehouses. Dalmar Street provides a consistent single-storey streetscape with a mix of Federation, bungalow and modern style housing in dominant brick palette to the street front.

The residential areas of the precinct are subject to an FSR of 0.7:1 and height control of 8.5m, while the business properties have an FSR of 1.5:1 to 2:1 and height control ranging between 10-15m in the IWLEP.

## 4.0 PLANNING PROPOSAL

### Part 1 – Objectives and intended outcomes

The **Objectives** of the Planning Proposal are to:

- 1) Ensure that Inner West is a place that supports a high quality of life; embraces diversity and adapts to the population needs of the future.
- 2) Ensure that Inner West communities are connected to one another through space, social engagement, and transport.
- 3) Protect, enhance and expand Inner West’s heritage.
- 4) Ensure that new development in the Inner West is commensurate with the provision of physical, social and community infrastructure.
- 5) Ensure the ongoing provision of dwellings and employment floor space in the Inner West to meet the NSW Government’s dwelling and employment targets.
- 6) Increase the provision of affordable housing in the Inner West.
- 7) Ensure that development in the Inner West sets a new benchmark with regards to ecologically sustainable development.
- 8) Reduce energy consumption and greenhouse gas emissions; and improve the resilience of the built environment to the impacts of climate change by creating sustainable, liveable and cool neighbourhoods.

#### Intended Outcomes

This Planning Proposal will:

- 1) Facilitate development within the Parramatta Road Corridor that is either consistent with the Parramatta Road Corridor Urban Transformation Strategy or that achieves better outcomes than those identified in the Strategy, underpinned by Council’s evidence-based strategic planning work.
- 2) Provide opportunities for employment and housing growth (including affordable housing) through provision of additional floorspace and building heights, subject to achieving certain criteria which meet the above objectives.
- 3) Deliver a proportion of dwellings that are affordable to very low, low- and moderate-income households.
- 4) Protect and enhance the built and historic heritage of the Parramatta Road Corridor.
- 5) Encourage new development which is appropriate to its size and local context.
- 6) Encourage new development to be ecologically sustainable and reduce greenhouse gas emissions by minimising the consumption of energy and water.
- 7) Protect and enhance the urban tree canopy.
- 8) Create walkable and rideable communities with reduced dependence on cars and increased use of public transport.
- 9) Deliver local, community and state infrastructure commensurate with the planned intensity of development.

These Objectives and Intended Outcomes align with the Key Moves listed in the Introduction section and the broader Priorities/Actions of the PRCUTS, GSRP, ECDP and Council’s LSPS. These will ensure that growth in the Inner West is balanced with good amenity and infrastructure outcomes for Inner West’s public spaces, open spaces and streets.

## Part 2 – Explanation of provisions

The following amendments to the *Inner West Local Environmental Plan (IWLEP) 2022* are sought to facilitate the Objectives and Intended Outcomes in Part 1 of this Planning Proposal.

### 2.1 Land Use

- i. Amend land use zones for specific locations in Leichhardt, Taverners Hill and Kings Bay/ Croydon precincts as per the proposed Land Use Zoning maps in Appendix 1.
- ii. Rezone 2 Hay Street, Leichhardt to RE1 Public Recreation for new open space.
- iii. Introduce residential flat buildings as an Additional Permitted Use on specific sites (as further outlined below in Section 2.2).

### 2.2 Additional Permitted Uses

- i. Introduce a new local provision to permit residential flat buildings as an Additional Permitted Use (APU) in the below locations:
  - All land proposed to be zoned R3 Medium Density Residential in Leichhardt, Taverners, Hill and Kings Bay/ Croydon precincts.
  - 590-624 Parramatta Road, Croydon, 210 Croydon Road and 235-237 Croydon Road, Croydon (also identified as Opportunity Sites in Section 2.11 under Site-specific provisions).

These sites have also been identified on the APU Map at Appendix 1.

Note: For housekeeping purposes, APU Maps include the existing R3 zoned site at 30-40 George Street, Part 24 George Street and Part 45 Upward Street (former Labelcraft site) to permit residential flat buildings to reflect its existing use. These sites were zoned R3 Medium Density Residential under the now repealed Leichhardt Local Environmental Plan 2013 and residential flat buildings were permitted with consent in the zone. This permissibility was inadvertently omitted in the consolidation of the LEPs of the three former LGAs to make to IWLEP 2022.

### 2.3 Heritage

- i. Introduce 24 new Heritage Items and 1 new Archaeological Site.
- ii. Introduce a new Heritage Conservation Area (HCA) along Barker Street, Lewisham.
- iii. Extend the existing Excelsior HCA to include 20-24 Norton Street, Leichhardt.

#### 2.3.1 Heritage Items

24 properties are proposed to be heritage listed including 17 items in Leichhardt, 6 items in Taverners Hill and 1 item in the Kings Bay/ Croydon Precinct as per Table 3.

Table 3 – List of proposed heritage items

No.	Locality	Item Name	Address	Property Description	Significance
<b>Leichhardt Precinct</b>					
1.	Annandale	Former Westgate Post Office – excluding apartment block at rear	225 Parramatta Road	CP for Lot 1-8 SP 83952	local

No.	Locality	Item Name	Address	Property Description	Significance
2.	Leichhardt	Shop façade – façade above awning only	335 Parramatta Road	Lot 1, DP 205912	local
3.	Leichhardt (Outside PRCUTS Precinct boundaries)	Former St Martha's Industrial School for Girls	38-40 Renwick Street	Lot 10 Sec 8 DP 612, Lot 11 Sec 8 DP 612, Lot 26 Sec 8 DP 612, Lot 29 Sec 8 DP 612, Lot 30 Sec 8 DP 612, Lot 8 Sec 8 DP 612, Lot 9 Sec 8 DP 612, Lot 28 Sec 8 DP 612, Lot 121 DP 114338, Lot 122 DP 114338, Lot 123 DP 114338, Lot 124 DP 114338	local
4.	Petersham	Shop and residence	514 Parramatta Road,	Lot 4 DP 572103	local
5.	Petersham	Shop and residence	516 Parramatta Road	Lot 3 DP 572103	local
6.	Petersham	Shop and residence	518 Parramatta Road	Lot 10 DP 1104985	local
7.	Petersham	Shop and residence	520 Parramatta Road	Lot 2 DP 572103	local

No.	Locality	Item Name	Address	Property Description	Significance
8.	Petersham	Shop and residence	522 Parramatta Road	Lot 1 DP 572103	local
9.	Petersham	Former Petersham Arcade	538-542 Parramatta Road	Lot 1 DP 217301, Lot 2 DP 217301, Lot 3 DP 217301	local
10.	Petersham	Former Petersham Arcade	544 Parramatta Road	Lot 4 DP 217301	local
11.	Petersham	Former Petersham Arcade	546-548 Parramatta Road	Lot 1 DP 586519	local
12.	Petersham	Former Petersham Arcade	550 Parramatta Road	Lot 7 DP 217301	local
13.	Petersham	Former Albert (later Brighton) Hall and shops	576 Parramatta Road	Lot 1 DP 115621	local
14.	Petersham	Former Albert (later Brighton) Hall and shops	582-584 Parramatta Road	Lot 1 DP 115620, Lot 1 DP 81775	local
15.	Stanmore	Shop and residence	174 Parramatta Road	Lot 4, DP 443402	local
16.	Stanmore	Shop and residence	176 Parramatta Road	Lot 3, DP 443402	local
17.	Stanmore	Shop and residence	178 Parramatta Road	Lot 1, DP 443196	local
<b>Taverners Hill</b>					
18.	Leichhardt	Baptist Church and Hall	51-55 Lords Road	Lot 11 DP 1135, Lot 12 DP 1135	local
19.	Leichhardt	Electricity Substation No. 1520	9 Tebbutt Street	Lot A DP 310751	local
20.	Lewisham	Convent of the Sisters of Charity	40A Thomas Street	Lot C DP 943062, Lot 1 DP 171989,	local

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No.	Locality	Item Name	Address	Property Description	Significance
				Lot 1 DP 436187, Lot 2 DP 436187	
21.	Summer Hill	“Sloane Court” flat building	51a Sloane Street	Lot 2 DP 338439	local
22.	Summer Hill	“Asturia” flat building	58 Sloane Street	SP11853	local
23.	Lewisham	Lewisham Hotel (Previously Wheelwrights Arms Hotel, The Louis Hotel)	794 Parramatta Road	Lot 1 DP 983757, Lot 1 DP 658435	local
<b>Kings Bay/ Croydon</b>					
24.	Croydon	Electric Lighting Sub-Station No. 72	590 Parramatta Road	Lot 1 DP 592800, Lot 2 DP 592800	local

### 2.3.2 Heritage Conservation Areas

3 properties are proposed to be included in the extension of Excelsior Heritage Conservation Area within the Leichhardt Precinct. A new Heritage Conservation Area is proposed which will include 8 properties along Barker Street, Lewisham in the Taverners Hill Precinct.

Table 4 – Proposed new heritage conservation areas

No.	Locality	Conservation Area	Address	Property Description	Significance
<b>Leichhardt Precinct</b>					
1.	Leichhardt	Excelsior Heritage Conservation Area	20-24 Norton Street, Leichhardt	Lot 1 DP 793141, Lot A DP 437525, Lot 89 DP 455747, Lot 90 DP 455747	local
<b>Taverners Hill</b>					
2.	Lewisham	Barker Street Heritage Conservation Area	10-16 Barker Street, Lewisham	Lot 100 DP 1165292, Lot 1 DP 1001640, Lot 1 DP 865485,	local

No.	Locality	Conservation Area	Address	Property Description	Significance
				Lot 102 DP 812726	
3.	Lewisham	Barker Street Heritage Conservation Area	27-33 Barker Street, Lewisham	Lot B DP 439965, Lot A DP 439965, Lot 2 DP 222932, Lot 1 DP 222932	local

### 2.3.3 Archaeological Sites

Five properties in the Leichhardt Precinct are identified within the Annan Grove Cottage archaeological site as per Table 5.

Table 5 – Proposed new Archaeological Heritage Item

No.	Locality	Item Name	Address	Property Description	Significance
<b>Leichhardt Precinct</b>					
1.	Annandale	“Annan Grove” cottage	239-245A Parramatta Road	Lot A DP 440944, Lot 3 DP 108278, Lot 4 DP 108278, Lot 5 DP 108278, Lot 6 DP 108278, Lot 7 DP 108278, Lot 8 DP 108278	local

### 2.4 Built form Controls (FSR and HOB)

- i. Retain existing FSR and HOB controls in the IWLEP 2022 except for 2 Hay Street, Leichhardt where existing FSR control is to be removed as it is proposed RE1 Public Recreation open space.
- ii. Introduce additional HOB and FSR controls where an uplift is proposed. These are to be known as ‘Incentive FSR and Incentive HOB’.
- iii. Introduce a new local provision that requires developments to satisfy certain criteria (see below) in order to use the incentive FSR/HOB controls.

The ‘base’ height is shown on the ‘Height of Buildings Map’, the ‘incentive’ height is shown in the ‘Incentive Height of Buildings Map’, the ‘base’ FSR is shown on the ‘Floor Space Ratio Map’, and the ‘incentive’ Floor Space Ratio is shown on the ‘Incentive Floor Space Ratio Map’.

The proposed built form controls are generally aligned with the recommendations of PRCUTS and have been further tested at a site scale in Appendix 2. Any variations to recommended

PRCUTS built form controls is to achieve better design outcomes based on urban design testing.

**2.5 Additional Local Provision to facilitate development within Leichhardt, Taverners Hill and Kings Bay/Croydon Precincts**

- i. Identify the Planning Proposal Areas 1, 2 and 3 in the Key Sites Map
- ii. Introduce a new local provision for Areas 1, 2 and 3 to enable proposed development to access the incentive FSRs and building heights only where criteria below are satisfied:
  - a) achieves an appropriate development pattern and high-quality built form in terms of bulk, massing, height, separation, setbacks, amenity and modulation
  - b) provides an active street frontage as required under Section 2.6.
  - c) achieves higher building performance targets as detailed in Section 2.7.
  - d) incorporates mechanisms to reduce urban heat as detailed in Section 2.8.
  - e) supports sustainable transport modes, minimises traffic congestion and reduces private car dependency as prescribed in Section 2.9.
  - f) If in Key Site 1, makes affordable housing contributions as detailed in Section 2.10.
  - g) makes satisfactory State/ Regional Infrastructure Contributions (RICs) as detailed in Section 2.12.

Under the terms of this clause, development consent must not be granted in relation to a development seeking to access the Incentive FSR and HOB, unless the consent authority is satisfied that the above requirements have been satisfied, where relevant. This incentives-based approach is key to achieving the Objectives and Intended Outcomes of this Planning Proposal and delivering the best urban design, community and infrastructure outcomes.

Summary of proposed FSR and HOB incentives provisions is provided in Section 2.14.

**2.6 Active street frontages**

- i. Introduce a clause to provide active street frontages to attract pedestrian traffic and provide vibrancy, surveillance and activity in certain areas as per the Active Street Frontages Map in Appendix 1.
- ii. Prohibit residential accommodation and limit parking infrastructure such as driveways, ground level car parks and servicing areas where active street frontages are required.
- iii. Active street frontages will be required for all of the ground floor except for any part of the building which is used for residential entrances and lobbies (as part of mixed-use developments), access for fire services and vehicular access.

These requirements applied as per the Active Street Frontages maps at Appendix 1 will ensure appropriate retail and commercial activities are provided to activate streets and plazas as well as facilitate vibrancy and passive surveillance.

Associated controls in the DCP will guide the provision of active street frontages.

**2.7 High Performance Buildings and Improved Environmental Outcomes**

- i. Introduce a clause for developments, including residential accommodation, office, shopping centres, hotels and serviced apartments, that seek an uplift associated with FSR and HOB incentives, to satisfy energy and water minimum performance standards as outlined below in Table 6 and Table 7.
- ii. Introduce new definitions specific to the local provision for gross-lettable area – retail, net lettable area, shopping centre and refurbishment.
- iii. Introduce a clause encouraging on-site workers facilities (such as showers, change rooms, lockers and bicycle storage areas) for employment generating uses to facilitate active transport commuting, healthy workplaces and cater for worker needs.

Table 6 – Development threshold and performance standards for residential development

Development type and threshold	Minimum energy target	Minimum water target
Low rise residential buildings: 2-3 storeys	8-point increase over the Sustainable Buildings SEPP ≈ BASIX Energy 75	BASIX Water 50 (and up to 60 where recycled water is available) for all new dwellings
Mid-rise residential buildings: 4-5 storeys	5-point increase over the Sustainable Buildings SEPP ≈ BASIX Energy 66	
High Rise residential buildings: 6-20 storeys	5-point increase over the Sustainable Buildings SEPP ≈ BASIX Energy 65	
Residential as a component of mixed-use development	As above relevant to the number of storeys	

Note. The Sustainable Building SEPP was published on 29 August 2022 and will come into effect from 1 October 2023. Upon commencement, the existing State Environmental Planning Policy (SEPP) (BASIX) 2004 will be repealed and integrated into the Sustainable Buildings SEPP.

Table 7 – Development threshold and performance standards for non-residential development

Development type and threshold	Minimum energy target	Minimum water target
<b>Office (base building)</b> <ul style="list-style-type: none"> <li>• A new office building ≥ 1,000m<sup>2</sup> nett lettable area (NLA) or more, or</li> <li>• A refurbishment to an existing office building that contains a NLA or 1,000m<sup>2</sup> or more, or</li> </ul>	<ul style="list-style-type: none"> <li>• Maximum 45 kWh/yr/m<sup>2</sup> of gross floor area (GFA) or,</li> <li>• 5.5 star NABERS Energy Commitment Agreement (CA) + 25% or,</li> <li>• Certified Green Star Buildings rating with a “credit achievement” in Credit 22: Energy Use, or</li> </ul>	NABERS Water 4 star (5 star where recycled water is available)

<ul style="list-style-type: none"> <li>An existing office building of 1,000m<sup>2</sup> NLA or more with an addition of 500m<sup>2</sup> or more NLA</li> </ul>	<ul style="list-style-type: none"> <li>equivalent</li> </ul>
<p><b>Shopping centre development (base building)</b></p> <ul style="list-style-type: none"> <li>A new shopping centre containing a gross lettable area – retail (GLAR) of 5,000m<sup>2</sup> or more</li> <li>An existing shopping centre of 5,000m<sup>2</sup> GLAR or more with an addition of 2,500m<sup>2</sup> or more GLAR</li> </ul>	<ul style="list-style-type: none"> <li>Maximum 44 kWh/yr/m<sup>2</sup> of GFA or,</li> <li>5 star NABERS Energy CA, or Certified Green Star Buildings rating achieving the “minimum expectation” in Credit 22: Energy Use, or</li> <li>equivalent</li> </ul>
<p><b>Hotel (whole building)</b></p> <ul style="list-style-type: none"> <li>A new hotel of 100 rooms or more</li> <li>A refurbishment to an existing hotel that contains 100 rooms or more</li> </ul>	<ul style="list-style-type: none"> <li>Maximum 199 kWh/yr/m<sup>2</sup> of GFA or,</li> <li>4.5 star NABERS Energy CA, or</li> <li>Certified Green Star Buildings rating achieving the “minimum expectation” in Credit 22: Energy Use, or</li> <li>equivalent</li> </ul>
<p>Serviced Apartments (whole building)</p> <ul style="list-style-type: none"> <li>A new building with 100 serviced apartments or more</li> <li>A refurbishment to an existing serviced apartment building that contains 100 serviced apartments or more</li> </ul>	<ul style="list-style-type: none"> <li>Maximum 199 kWh/yr/m<sup>2</sup> of GFA or,</li> <li>4.5 star NABERS Energy CA, or</li> <li>Certified Green Star Buildings rating achieving the “minimum expectation” in Credit 22: Energy Use, or</li> <li>equivalent</li> </ul>

**Mixed Use**

As above relevant to proposed uses and the Table 6 for residential developments.

These sustainability performance targets (energy and water) will only have to be met if a proposed development relies on FSR and HOB bonus incentives.

These are based on the recommended targets outlined in the PRCUTS Parramatta Road Corridor Sustainability Implementation Plan and Planning and Design Guidelines (2016), the Sustainable Buildings SEPP 2022 and national best practice policies and strategies. Higher sustainability performance targets beyond PRCUTS have been proposed where better outcomes can be demonstrated. Refer to Appendix 9 – Parramatta Road Corridor Stage 1 Sustainability Study for more information.

The intent of the proposed facilities for workers clause is to improve the amenity of the workplace, encourage alternative forms of transport to work and for businesses to attract and retain staff. The supporting draft DCPs include controls that will support the implementation of the performance standards and worker facilities.

The following definitions will be introduced in the LEP for the purposes of this clause:

- **Gross lettable area – retail (GLAR):** means the area of the building as set out in the Property Council of Australia Method of Measurement.
- **Net lettable area (NLA):** means the area of the building as set out in the Property Council of Australia Method of Measurement.
- **Refurbishment:** means carrying out of works to an existing building where the works affect at least half the total volume of the building measured over its external roof and walls and where there is no increase in the gross floor area. In calculating the extent of the building’s volume that is being changed, the proposed works and all other building work completed or authorised within the previous three years is to be included.
- **Shopping centre:** means two or more retail premises within a building that has shared plant and services which are managed by a single person or entity. This does not include specialised retail premises, garden centres, hardware and building supplies, landscaping material supplies, plant nurseries, roadside stalls, rural supplies, timber yards or vehicle sales or hire premises.

**2.8 Urban Heat Mitigation**

- Include an objective to mitigate the effects of urban heat island and climate change by expanding tree canopy cover and incorporating measures for cooling the private and public domain.
- Introduce local provision to achieve specific deep soil and tree canopy targets as set out in Table 8 8 below.
- Where sites are constrained (e.g., existing high-density mixed-use urban sites, existing high streets), consider alternative design solutions for greening.

Table 8 8 – Tree canopy and deep soil targets based on Zone

Zone	Site area (m <sup>2</sup> )	Canopy target (% of site area)	Deep soil
R3 Medium Density Residential	Less than 650m <sup>2</sup>	15%	-
	650-1500m <sup>2</sup>	20%	
	1500-3000 m <sup>2</sup>	25%	
	Greater than 3000m <sup>2</sup>	35%	
E3 Productivity Support	All lots	35% (25% for bulky goods)	25%

The above provisions are based on the vision of Parramatta Road Corridor Planning and Design Guidelines and Sustainability Implementation Plan which recommends measures to address urban heat island effect. The above tree canopy and deep soil targets are adopted directly from the NSW Government Architect’s [Greener Neighbourhoods Guide](#). (Refer to pgs 37-38). In addition, the deep soil and tree canopy requirements as per SEPP Design Quality of Residential Apartment Development – No. 65 (Apartment Design Guide) will continue to apply for residential flat buildings.

**2.9 Car Parking**

- Introduce a clause which outlines the objective to minimise the amount of vehicular traffic generated from proposed developments which rely on FSR/HOB incentive provisions.

- ii. Identify the maximum number of car parking spaces that may be provided as set out in Table 9 9 below.
- iii. Introduce the following objective:
  - Support sustainable transport including active and public transport by minimising private car parking and encouraging the provision of unbundled parking, car share schemes and decoupled parking.
- iv. Include the following definitions:
  - **Unbundled parking** means parking that is separated from the cost or rent of a dwelling, commercial units and building ownership.
  - **Car share scheme** means a scheme in which any car share operator provides vehicles for shared use and hires those vehicles exclusively to members of the scheme for occasional use for short periods of time, on demand and on a pay-as-you go basis.
  - **Decoupled parking** means provision of off-site car parking, usually in the form of consolidated car parking in close proximity to the development to satisfy the parking requirements.

This will be achieved by identifying the applicable area in the Land Use and Transport Integration Maps in Appendix 1.

Table 9 9 – Maximum car parking rates

Category	Residential (maximum space per dwelling)	Other (maximum space per GFA)
Category A (Leichhardt and Taverners Hill Precinct)	<ul style="list-style-type: none"> <li>• Studio – 0</li> <li>• 1 Bed – 0.3</li> <li>• 2 Bed – 0.7</li> <li>• 3 Bed – 1</li> <li>• Visitor – 0</li> </ul>	<ul style="list-style-type: none"> <li>• Commercial:                             <ul style="list-style-type: none"> <li>○ Commercial and office premises – 1 space per 150sqm</li> <li>○ Restaurants- 1 per 50 sqm</li> <li>○ Retail – 1 space per 100 sqm</li> <li>○ Bulky goods – 1 per 50 sqm</li> </ul> </li> <li>• Industrial – 1 space per 150sqm</li> </ul>
Category B (Kings Bay/ Croydon Precinct)	<ul style="list-style-type: none"> <li>• Studio – 0.3</li> <li>• 1 Bed – 0.5</li> <li>• 2 Bed – 0.9</li> <li>• 3 Bed – 1.2</li> <li>• Visitor – 0.1</li> </ul>	<ul style="list-style-type: none"> <li>• Commercial –                             <ul style="list-style-type: none"> <li>○ Commercial and office premises: 1 space per 100sqm</li> <li>○ Restaurants- 1 per 50 sqm</li> <li>○ Retail – 1 space per 70 sqm</li> <li>○ Bulky goods – 1 per 50 sqm</li> </ul> </li> <li>• Industrial – 1 space per 120sqm</li> </ul>

This Planning Proposal sets maximum car parking rates based on Parramatta Road Corridor Planning and Design Guidelines maximum car parking rates for majority of land uses except restaurants and bulky goods. These are based on alternate rates proposed in the Parramatta Road precinct-wide Traffic and Transport Studies (**Error! Reference source not found.**) by Cardno which are slightly higher than PRCUTS maximum rates – this approach is necessary to address the specific needs of these specialist land uses.

To achieve PRCUTS vision, the proposal encourages people to reduce car use and dependence and lessen the impact on local roads. Resident parking permits will not be issued to occupants of these new developments. Note: this will not impact existing entitlements for these permits. Purchasers of new residential apartments in this Planning Proposal area will not be eligible for Council's Resident Parking Scheme. Further discussion included in Part 3 of this Planning Proposal.

The approach of encouraging unbundled parking, car share schemes and decoupled parking is also consistent with PRCUTS. Unbundled parking has synergies with the removal of minimum parking requirements and allows for parking to be separately sold from the development of dwellings or commercial units, which in turn allows the market to allocate that parking to those with a need for parking and have a willingness to pay – this is a key move to improve housing affordability in the Inner West. PRCUTS notes that *'this is not only more equitable but can also reduce the total amount of parking required for a building'*.

Supporting draft DCP amendments will also include minimum rates for bicycle parking, provision of worker facilities and electric vehicle charging infrastructure.

**2.10 Affordable Housing**

- i. Introduce a provision to require contributions for affordable housing to be made in accordance with an Affordable Housing Contributions Scheme (AHCS) for new developments in Key Site Area 1 (Leichhardt Precinct).

This provision is to be read in conjunction with Council's Draft Affordable Housing Contribution Scheme (Appendix 8).

The intent of this provision is that development for residential purposes identified within urban renewal areas in the Inner West Council LGA must contribute towards affordable housing, specifically for Leichhardt precinct – 2% of the Residential Strata Area, defined as the sum of areas attributed to lots under a plan of subdivision for the development that are to be used for residential purposes including related parking and storage areas.

DPE's viability tool indicates that an AHCS would not be viable for Taverners Hill and Kings Bay/ Croydon precincts. As such, the AHCS is only proposed to apply in the Leichhardt precinct.

**2.11 Site-Specific Provisions**

Introduce new site-specific provisions for certain sites:

*(i) Land at 612-624 Parramatta Road, Croydon and 210 Croydon Road, Croydon (Opportunity Site 1)*

- o permit redevelopment of the above land as an amalgamated site to allow access to FSR and HOB incentives.
- o permit residential flat buildings as an additional permitted use, but only if the proposed development provides commercial/ business uses on the entirety of the ground floor that are compatible with residential uses above.
- o manage urban hazards of environmental impacts including air quality and noise.
- o provide a minimum of 1.5m setback to Parramatta Road, 3m to Croydon Road and associated public domain improvements.

*(ii) Land at 590-610 Parramatta Road, Croydon and 235-237 Croydon Road, Croydon (Opportunity Site 2)*

- o permit redevelopment of the above land as an amalgamated site to allow access to FSR and HOB incentives.
- o permit residential flat buildings as an additional permitted use, but only if the proposed development provides commercial/ business uses on the entirety of the ground floor that are compatible with residential uses above.
- o manage urban hazards of environmental impacts including air quality and noise.
- o provide a minimum of 1.5m setback to Parramatta Road, 3m to Croydon Road and associated public domain improvements.

*(iii) Land at 29-67 Norton Street, Leichhardt and 40-50 Balmain Road, Leichhardt (Opportunity Site 3)*

- o permit redevelopment of the above land as an amalgamated site to allow access to FSR and HOB incentives.
- o provide a minimum of 18m wide landscaped through-site link for active transport connections and public domain improvements between Norton Street and Balmain Road.
- o provide a minimum of 25m wide public open space/ plaza with appropriate landscaping fronting Norton Street
- o provide active street frontage along the proposed through-site link between Norton Street and Balmain Road.

The purpose of these site-specific provisions is to ensure suitable redevelopment and amalgamation of lots, to achieve built form outcomes which are proportional to the site's area and consistent with the objectives. These sites provide unique opportunities for new housing in key locations of Leichhardt and Kings Bay/ Croydon precincts; and the opportunity to contribute towards on-site public domain improvements which will enhance the precincts' connectivity and accessibility. These sites are also unique opportunities in the context of PRCUTS as the urban design study (Appendix 2) has recommended potential for intensified uses, density or height beyond what is envisaged in PRCUTS.

These site-specific provisions will be supported by accompanying DCP provisions which further outline built form and design requirements.

**2.12 Arrangements for contributions to designated State Public Infrastructure**

- i. Include a new clause which requires satisfactory arrangements to be made for the provision of 'designated State public infrastructure' before the development of land for residential or commercial purposes in Key Sites Area 1, 2, 3.

Under the terms of this clause, development consent must not be granted unless the Secretary of DPE has certified in writing to the consent authority that satisfactory arrangements have been made to contribute to the provision of designated State public infrastructure in relation to that development. This is in accordance with the actions in the Parramatta Road Corridor Urban Transformation Strategy Implementation Update 2021, as also required by Gateway Condition 1(a). The clause will not apply if a development does not result in an increase in residential or commercial floor space. In the proposed clause, 'designated State public infrastructure' includes:

- a) State and regional roads,
- b) bus interchanges and bus lanes,
- c) land required for regional open space,

- d) social infrastructure and facilities (such as schools, hospitals, emergency services and justice purposes),
- e) light rail infrastructure.

**2.13 Maps**

This planning proposal seeks to amend the following IWLEP maps. The proposed LEP maps are provided at Appendix 1. The following broadly summarises the key changes and additions for the applicable area.

- i. Amend the existing Land Zoning Maps as outlined in Section 2.1.
- ii. Create Additional Permitted Uses maps as outlined in Section 2.2
- iii. Amend the existing Heritage Maps as outlined in Section 2.3.
- iv. Amend the existing FSR map to remove the FSR control for 2 Hay Street, Leichhardt.
- v. Create Incentive Height of Buildings Maps.
- vi. Create Incentive Floor Space Ratio Maps.
- vii. Create Key Sites Maps to identify the affected area relating to the proposed Additional Local Provisions as outlined in Section 2.5.
- viii. Create Active Street Frontages Maps relating to the proposed active street frontage provisions as outlined in Section 2.6.
- ix. Create Land Use and Transport Integration Maps relating to the proposed Car Parking provisions as outlined in Section 2.9.
- x. Create Opportunity Sites Maps relating to the proposed Site-specific Local provisions as outlined in Section 2.11.

**2.14 Tables of changes to existing planning controls**

Summary of existing and proposed controls is provided in the below tables:

*Table 10 10 – Leichhardt Precinct – Stage 1 – Existing and proposed planning controls*

Area	Current zone	Proposed zone	Current FSR (n:1)	Proposed incentive FSR (n:1)	Current HOB	Proposed incentive HOB
North of Parramatta Road	E1	E1	1.0 + 0.5 bonus for active street frontages	1.9, 2.7, 3.0	Not defined	20.5m, 23.5m, 27m, 32m
	R1	R3 (+ RFB as APU)	0.5 – 0.8 as per site area	1.4, 1.5, 1.9, 2.1, 2.3	Not defined	15.5m, 21.5m, 25m
2 Hay St, Leichhardt	E1	RE1	1.0 + 0.5 bonus for active street frontages	N/A	Not defined	N/A
South of Parramatta Road	E1	E1	1.5	3.0	14m	23.5m
	E1	R3 (+ RFB as APU)	1.5	2.2	14m	18.5m

*Table 11 11 – Taverners Hill Precinct – Stage 1 – Existing and proposed planning controls*

Area	Current zone	Proposed zone	Current FSR (n:1)	Proposed incentive FSR (n:1)	Current HOB	Proposed incentive HOB
Lewisham South (South of	R2	R3 (+ RFB as APU)	0.6 – 1.0 as per site area	1.0	9.5m	12m, 15.5m

Parramatta Road)	R4	R3 (+ RFB as APU)	1.0, 1.1	1, 1.2	17m	18.5m
West Leichhardt (North of Parramatta Road)	R1	R3 (+ RFB as APU)	0.5 – 0.8 as per site area	0.5, 0.9, 1.2, 1.4	Not defined	12m, 15.5m, 21.5m

Table 12.12 - Kings Bay/ Croydon Precinct – Existing and proposed planning controls

Area	Current zone	Proposed zone	Current FSR (n:1)	Proposed incentive FSR (n:1)	Current HOB	Proposed incentive HOB
Kings Bay/ Croydon	E3	E3	1.5, 2.0	2.4	10m,15m	17.5m
	R2	E3	0.7	2.4	8.5m	17.5m
	R2	R3 (+ RFB as APU)	0.7	1.0, 1.3	8.5m	12m,15.5m,
Opportunity Sites 1 and 2	R2, R3 and E3	E3 + RFB	0.7, 1.5, 2.0	2.4 (Site 1) 2.1 (Site 2)	8.5m, 10m, 12.5m 15m	19.5m

### 2.15 Other Relevant Matters

- **Amendments to the draft Leichhardt, Marrickville and Ashfield DCPs**

Amendments to the three former Council’s DCPs have been prepared to provide more detailed controls with respect to future built form and support the changes being made under this Planning Proposal. These are being exhibited for community feedback at the same time as this Planning Proposal.

- **Inner West Local Infrastructure Contributions Plan – Section 7.11 and Section 7.12**

The Inner West Local Infrastructure Contributions Plan commenced on 20 February 2023. The Contributions Plan guides infrastructure investment to ensure that the level of growth anticipated under this Planning Proposal and any future proposals across the Inner West is supported. It contains approximately \$597million of local infrastructure to be delivered between 2022 and 2036.

The Contributions Plan was informed by a suite of supporting studies:

- Inner West Community Assets Needs Study (2021)
- Inner West Recreational Needs Study (2021)
- Inner West Traffic and Transport Needs Study (2021) and
- the Parramatta Road Corridor Precinct-wide Traffic and Transport Study (Cardno, DPE).

- **Draft Affordable Housing Contributions Scheme**

A draft Affordable Housing Contributions Scheme (AHCS) under *State Environmental Planning Policy (Housing) 2021* (Housing SEPP) has been prepared to support the new clause and is included as Appendix 8. The draft AHCS sets out how, where, and at what rate development contributions will be collected by Council for affordable housing. It includes the evidence base supporting the need for affordable housing and provides operational details including how to calculate contributions and in what form contributions are to be made.

### Part 3 – Justification of strategic and site-specific merit

This section of the Planning Proposal provides the rationale for the amendment to the IWLEP 2022 and responds to questions set out in the DPE's *Local Environmental Plan Making Guideline* August 2023 (former 'A guide to preparing planning proposals' 2016).

#### Section A – Need for the planning proposal

##### Q1. Is the planning proposal a result of an endorsed local strategic planning statement (LSPS), strategic study or report?

Yes. The Planning Proposal is consequential to the NSW Government's Parramatta Road Corridor Urban Transformation Strategy (PRCUTS) which was adopted in November 2016.

In addition to responding to NSW State government strategic plans, this Planning Proposal is based on Inner West Council's [Local Strategic Planning Statement](#) which was endorsed on 31 March 2020 and is also supported by the Greater Cities Commission. The Proposal is also complemented by Council's [Local Housing Strategy](#) (LHS) , [Employment and Retail Land Strategy](#) (EaRLS) and [Integrated Transport Strategy](#) (ITS).

The Planning Proposal is informed by technical studies, which support and refine the implementation of PRCUTS recommendations. They also provide evidence-based justification where variations to these recommendations are proposed. These include:

##### Appendix 2 – Urban Design Studies

These studies for the Leichhardt, Taverners Hill and Kings Bay/ Croydon precincts justify the height and floorspace incentives sought in this Planning Proposal. The set of studies reviewed the recommendations of the Parramatta Road Corridor Planning and Design Guidelines (PRC P&DG) to establish what measures will deliver the best urban design and community outcomes.

The built form testing undertaken as part of the urban design studies demonstrated that the Planning Proposal should seek variations to some PRCUTS land use zoning, FSR and height recommendations to achieve better outcomes than those anticipated by the original Strategy. The proposed new built form controls have taken desired future and existing character, heritage and any recent development in the area into consideration.

These studies also include detailed urban design controls for built form elements such as building setbacks, street wall heights and frontages which have informed Council's supporting DCP amendments. This study has been revised in 2023 to address Gateway Determination conditions 1(c) (i) – (iii) and recent policy changes such as with the National Construction Code.

##### Appendix 3 – Heritage Study

This study supports the proposed new heritage items and heritage conservation areas included in this Planning Proposal. The study investigated the potential heritage significance of the sites recommended in the PRCUTS P&DG and former Leichhardt Council's 2016 Parramatta Road and Norton Street Corridor Heritage Study.

Heritage Inventory Sheets for the proposed Heritage Items and Heritage Conservation areas are included in this study.

##### Appendix 4 – Economic Assessment

This study investigated EaRLS' recommendations to improve the economic and productivity outcomes of PRCUTS of the Leichhardt and Kings Bay/ Croydon precincts as follows:

- i. Feasibility tipping point analysis to determine minimum non-residential FSR controls for Norton Street Leichhardt to secure commercial floor space on ground floors and podium levels.
- ii. Feasibility analysis for E3 employment zoned lands in Kings Bay/ Croydon precinct.
- iii. Feasibility tipping point analysis to determine minimum non-residential FSR controls for Opportunity Sites in the Kings Bay/ Croydon precinct.
- iv. Design recommendations to ensure flexible use of employment floorspace and improve economic productivity outcomes.

This study informs the economic aspects of this Planning Proposal including built form considerations relating to high floor-to-floor heights for flexible use of employment floorplates in Leichhardt and Kings Bay/ Croydon precincts. The urban design study took the findings of the feasibility assessment into account in recommending proposed land uses, FSRs and HOBs.

Economic assessment was not required for Taverners Hill as this Planning Proposal only deals with residential zoned land in Taverners Hill precinct.

#### **Appendix 5 – Flood Impact Assessment**

Flood Impact Risk Assessment study for Kings Bay/ Croydon, Taverners Hill, and Leichhardt precincts are provided in Appendix 5. This study demonstrates that the Planning Proposal is consistent with the requirements of section 9.1 Local Planning Direction 4.1 Flooding and relevant recommendations of the NSW Government's 2022 Flood Inquiry Report. This study was prepared to address DPE's Gateway Determination Condition 1(d) (i)-(iii).

#### **Appendix 6 – Contamination Assessment (Preliminary Investigations) Study**

This study investigated the implications of introducing sensitive uses or rezoning sites to permit sensitive uses on the following sites which may be potentially contaminated:

- i. 590, 596-598, 600-610 and 612-624 Parramatta Road, Croydon and 210 Croydon Road, Croydon
- ii. 2-18 Crystal Street, Petersham

The desktop analysis undertaken as part of this study concludes that these sites can be made suitable for the proposed uses subject to appropriate site-by-site contamination management and/or remediation at the development application stage.

#### **Appendix 7 – Aircraft Noise Assessment**

This study investigated the implications of introducing additional residential capacity in the Leichhardt precinct which is affected by the ANEF 25-30 contour. The recommended mitigation measures in relation to internal noise levels in new dwellings have informed this Planning Proposal. Council's supporting draft DCPs include provisions to mitigate road noise and aircraft noise impacts.

#### **Appendix 8 – Draft Affordable Housing Contributions Scheme**

A draft Affordable Housing Contribution Scheme (AHCS) has been prepared in accordance with the DPE "guideline for developing an affordable housing contributions scheme".

The draft AHCS sets out how, where, and at what rate development contributions will be collected by Council for affordable housing. It includes the evidence base supporting the need

for affordable housing and provides operational details including how to calculate contributions and in what form contributions are to be made.

**Appendix 9 – Parramatta Road Corridor Stage 1 Sustainability Study (WSP, 2023)**

This study informs the proposed sustainability provisions and addresses the Gateway Determination conditions 1 (h), (i) and 3 (d). This study includes a review of the PRCUTS sustainability performance targets against current minimum compliance (BASIX SEPP 2004 and Sustainable Building SEPP 2022) and national best planning policy practice. It also considers the implications of the proposed FSR and HOB incentives mechanism to ensure that the proposed planning controls are appropriate to incentivise the uplift bonus. The Study further demonstrates that the proposed sustainability provisions in this Planning Proposal can achieve better outcomes than those recommended in PRCUTS.

**– Parramatta Road Corridor Precinct-wide Traffic and Transport Study**

This jointly funded DPE and Inner West Council study (the Transport Study) is required by PRCUTS to assess the traffic impacts from the proposed levels of development in the Corridor. The Transport Study comprises the Transport Plan and several supporting technical reports, including the Context Report, Future Modelling Report and Parking Policy and Rate Review Report.

The key recommendations of this Transport Study have informed the transport and parking provisions of this Planning Proposal. The Transport Study was commissioned to Cardno in 2018 and completed in March 2022 through a series of iterations. The Transport Study covers Camperdown, Leichhardt and Taverners Hill precincts of Parramatta Road Corridor in the IWC area and includes the following sub-reports: Context Report, Future Modelling Report, Parking Paper and Transport Plan. The study is discussed in further detail under Section B of this Planning Proposal.

Canada Bay Council Traffic and Transport Study for the Kings Bay/ Croydon Precinct was completed in February 2022 and provides detailed traffic analysis and recommendations taking a precinct- wide approach for Kings Bay/ Croydon. Additionally, further discussion of traffic considerations that apply to the Inner West component of the Kings Bay/ Croydon Precinct are outlined in the IWC Kings Bay Precinct Traffic Impact report, finalised in April 2022; also included in **Error! Reference source not found.**

Further, Council has progressed work on the implementation of this study and prepared a response to address the actions outlined in the Transport Plan and Kings Bay/ Croydon Transport Assessment which are enclosed within this appendix. This has been completed to address the Gateway Determination condition 3(1)(ii). Responsible authorities against each action have been identified, noting many actions are beyond Council’s remit requiring NSW Government support. Further details can be seen in Table 1 of Appendix 10.

**Appendix – Parramatta Road Corridor Implementation Stage 1 Justification Study**

This study addresses the Section 9.1 Local Planning Direction 1.5 Consistency Clause (b) which requires that the Planning Proposal demonstrate better outcomes than those envisaged in the PRCUTS November 2016, Parramatta Road Corridor Implementation Plan 2016-2023 and Parramatta Road Corridor Implementation Plan Update 2021 having regard to the vision and objectives of these documents.

The study synthesises findings of the relevant technical studies and the core elements of GCC, State and Council policies to demonstrate that this Planning Proposal will deliver better outcomes than those in the original PRCUTS.

**Appendix 12 – Parramatta Road Urban Amenity Improvement Plan (Inner West Council Masterplan)**

This study provides detail on projects identified through the Parramatta Road Urban Amenity Improvement Plan for Leichhardt precinct including:

1. Public domain improvement to key north-south streets perpendicular to Parramatta Road: Rofe Street, Renwick Street, Norton Street, Balmain Road, Catherine Street and Crystal Street
2. New cycle connection along Dot Lane
3. Conversion of Petersham Street to a pocket park

**Additional justification of proposed amendments:**

• **Proposed R3 Medium Density Residential + RFBs**

Condition 1(e) of the Gateway determination requires consideration of the suitability of using the R1 General Residential and/or R4 High Density Residential zones, rather than the R3 Medium Density Residential zone, to remove the need to rely upon 'residential flat buildings' as an additional permitted use.

Council's position is to be consistent with PRCUTS which recommends the use of the R3 Medium Density Residential zoning for land within in the Planning Proposal area. The R3 zoning will allow for the delivery of a diversity of housing types consistent with the objectives of the zone in the IWLEP which are:

- i. To provide housing needs of the community within a medium density residential environment.
- ii. To provide a variety of housing types within a medium density residential environment.
- iii. To enable other land uses that provide facilities or services to meet the day to day needs of residents.
- iv. To provide housing that is compatible with the desired future character of the locality.
- v. To ensure that lots are of adequate size and shape to facilitate the appropriate balance between built form and high-quality landscape outcomes.
- vi. To protect and enhance the amenity of existing and future residents and the neighbourhood.

This position is also consistent with Council's Local Housing Strategy (LHS) which suggested implementation of the PRCUTS recommendations, subject to the outcomes of place-based studies. The LHS recommends 'medium rise' residential building typologies in PRCUTS precincts. Council's urban design studies also support the use of the R3 zone with residential flat buildings as an additional permitted use. Consideration was given to zoning these lands R1 General Residential or R4 High Density Residential, but this approach is not consistent with the PRCUTS, nor appropriate in the context.

The current zoning framework under the IWLEP 2022 is the result of the consolidation of three LEPs into one, while retaining all land use zones and the permissibility of land uses under the former instruments. The R4 High Density Residential zone is currently only used in the former Marrickville LGA and has been applied to both older walk-up flats and recent residential flat building developments with a wide range of FSRs and heights. Height controls for R4 sites vary from 14m up to 40m. To use the R4 zone, with the objective of providing housing within a 'high density residential environment', would be inconsistent with the desired future character and the scale of development to be facilitated under the

proposed controls. Proposed heights for the subject land range from 12m to 21m (3 to 6 storeys).

While the R1 General Residential zone has been used in the former Leichhardt and Marrickville LGAs to facilitate medium density development in the form of residential flat buildings, it is applied to all residential land in the former Leichhardt LGA (with the exception of part of the former Labelcraft/Kolotex site in Leichhardt which is zoned R3 and has been redeveloped with residential flat buildings). Consequently, the zone includes residential development of various scales. To continue this approach would be a missed opportunity to structure zones and objectives to reflect the desired future character. The current LEP Practice Note on zones confirms that the intent of the R1 zone is to provide for a variety of residential densities and housing types. It notes that if a particular type and consistent density of residential development is desired, a more tailored residential zone such as R2, R3 or R4 should be used.

Rather than use an inappropriate zone to achieve the desired outcome of permitting residential flat buildings, the R3 zone will be used, with residential flat buildings as an additional permitted use. It is envisaged that future reviews of the zoning framework under IWLEP 2022 will establish a more consistent approach where the permitted uses and scale of development more closely aligns with the chosen zone, objectives and the desired future character.

Note: The Planning Proposal's intention is to allow residential flat buildings as an APU for specific sites in the Planning Proposal area only. This does not affect existing R3 zoned sites outside the Planning Proposal area.

- **Kings Bay/ Croydon Opportunity Sites: Proposed RFBs as APUs on E3 Zoned Land**

Analysis was undertaken by Architectus (Appendix 2) to consider residential uses on certain existing E3 sites in the Kings Bay/ Croydon Precinct on Parramatta Road near Croydon Road, which are 800m away from the proposed Five Dock Metro Station. These are identified as 'opportunity sites' which would enable high-quality residential development in vicinity of the committed public transport infrastructure – this is consistent with PRCUTS vision and largely in response to the proposed transport and infrastructure changes by the NSW Government for Sydney Metro West.

PRCUTS was adopted in 2016 and is outdated. It has not been actively reviewed in response to these infrastructure changes. Through the place-based urban design work, this Planning Proposal intends to catalyse on the opportunity to provide more residential development close to public transport infrastructure to create more liveable, sustainable and walkable neighbourhoods.

Additionally, Council has undertaken economic impact assessment and feasibility testing (Appendix 4) to analyse the impacts of allowing residential uses on these key opportunity sites. The Economic assessment provides recommendations regarding ensuring that the ground floor of these sites continue to provide employment uses and that land use conflict plus urban hazards are appropriately managed. These recommendations have informed Council's draft LEP and DCP provisions.

**Q2. Is the planning proposal the best means of achieving the objectives or intended outcomes, or is there a better way?**

Yes. The wide range of NSW State Government and Council strategies and the Planning Proposal's supporting technical studies outlined above demonstrate how the PRCUTS

recommendations can be implemented to achieve better outcomes. This Planning Proposal is the best means of achieving the objectives and Intended Outcomes set out in Part 1 of the Planning Proposal.

**Section B – Relationship to the strategic planning framework**

**Q3. Will the planning proposal give effect to the objectives and actions of the applicable regional, or district plan or strategy (including any exhibited draft plans or strategies)?**

The Planning Proposal gives effect to the NSW Government’s Greater Sydney Region Plan 2018, Eastern City District Plan 2018 and Parramatta Road Corridor Urban Transformation Strategy 2016.

**1. Greater Sydney Region Plan 2018**

The Greater Sydney Region Plan (GSRP) is the Greater Cities Commission’s (GCC) (formerly Greater Sydney Commission) vision for a Greater Sydney of three cities, where most residents live within 30 minutes of their jobs and services. It sets a 40-year vision and establishes a 20-year plan to manage growth and change for a Greater Sydney, informing district and local plans and the assessment of planning proposals.

The GSRP contains 10 directions and 40 objectives to guide future growth, covering infrastructure, housing, economic development, and sustainability.

Table 1313 – Council’s consideration of Greater Sydney Region Plan (GSRP) Objectives

Objectives	Consideration
<b>Direction 1: A city supported by infrastructure</b>	
Objective 1: Infrastructure supports the three cities	<p><b><i>“Planning decisions need to support new infrastructure in each city – including cultural, education, health, community and water infrastructure – to fairly balance population growth with infrastructure investment. Decisions are required to equitably enhance local opportunities, inclusion and connection to services.” (GSRP p35)</i></b></p> <p>The GSRP recommended “Potential indicator” for achieving this objective for each of the three cities is <b><i>“Increased 30-minute access to a metropolitan centre/cluster.”</i></b> (GSRP p22)</p> <p>This 30-minute city access principle has informed this Planning Proposal as the affected area covers significant parts of the Parramatta Road Corridor in the Eastern Harbour City.</p> <p>30-minute access requires suitable public transport infrastructure to ensure that new development has desired levels of accessibility. This indicator is explained further as <b><i>“Providing adequate infrastructure to support population growth is essential to creating strong communities.”</i></b> (GSRP p32)</p>

Objectives	Consideration
	<p>The residential and employment population growth which can be facilitated by this Planning Proposal must accordingly be supported by adequate infrastructure, especially public transport infrastructure given the travel time nature of this Objective's indicator. Parramatta Road is a heavily congested corridor which requires substantial improvements including mass transit and traffic calming prior to the Corridor being burdened by additional growth.</p> <p>This cannot be achieved by the Planning Proposal alone and, therefore, support is sought from the NSW State Government to provide the necessary transport infrastructure to support growth in this corridor.</p> <p>In the interim, the Planning Proposal includes provisions relating to consideration of future transport infrastructure when designing new development and outcomes of a precinct-wide traffic and transport study.</p>
<p>Objective 2: Infrastructure aligns with forecast growth – growth infrastructure compact</p>	<p>The GSRP points out that areas forecast to experience significant residential and employment growth will require new and/or enhanced local and regional infrastructure to support growth (GSRP p36).</p> <p>This Planning Proposal supports this principle and is based on the following GSRP concept: <b><i>“Effectively aligning infrastructure with growth requires a methodical and sequenced approach to development.”</i></b> This requires a whole-of-government approach and a place-based understanding of sequencing of infrastructure delivery. This enables planning to support infrastructure alignment with areas of growth and transformation before additional areas are rezoned and ready for development. This new approach supports the appropriate growth and infrastructure being provided at the right time” (GSRP p39).</p> <p>PRCUTS identifies substantial development growth opportunities that can only be realised if they are aligned with substantial public transport infrastructure improvements. To achieve this <b>“methodical and sequenced approach”</b>, Council's Planning Proposal can potentially deliver 1516 new dwellings as outlined previously in Section 2a Staging Plan.</p> <p>The proposed phasing of growth would ensure that growth in the corridor is commensurate with the provision of local and State infrastructure.</p>

Objectives	Consideration
	<p>Council's Local Infrastructure Contributions Plan 2023 provides a framework for collecting local contributions to support population growth in the Inner West. The growth anticipated by this Planning Proposal is encapsulated by the works schedule contained in the Contributions Plan.</p> <p>The Planning Proposal also includes a provision to ensure development contributes to State/ Regional public infrastructure.</p>
<p>Objective 3: Infrastructure adapts to meet future needs</p>	<p>The Planning Proposal has been prepared within the context of what an LEP and its associated DCP can achieve in respect of this objective. This is particularly the case in terms of the following GSRP elaboration of what the Objective intends to achieve – <i>“Flexible designs and agreements for shared use provide new opportunities for social infrastructure including sportsgrounds, performing arts centres and gallery spaces, meeting rooms, schools and community facilities as well as car sharing and carpooling initiatives that can be adapted for use by a cross-section of the community. As technologies evolve, a precinct-based approach to community and public infrastructure, including car parking, needs to be taken to maximise adaptability, access and utilisation.”</i> (GSRP p40).</p> <p>The Proposal locates new development close to these types of facilities. Implementation of this Objective will, however, be dependent on collaboration with major stakeholders such as the NSW Department of Education, Catholic Education and private schools. Gateway Determination condition (2) requires consultation with public authorities including Department of Education.</p>
<p>Objective 4: Infrastructure use is optimised</p>	<p>The Planning Proposal reflects this Objective and in particular how <i>“This can be achieved by exploring opportunities to:</i></p> <ul style="list-style-type: none"> <li data-bbox="815 1487 1294 1563"><i>i. adopt new technologies such as smart traffic management systems and real-time energy and water metering systems</i></li> <li data-bbox="815 1570 1294 1646"><i>ii. use land more efficiently by co-locating services, or by allocating road space to support increased mass transit services</i></li> <li data-bbox="815 1653 1294 1711"><i>iii. change user behaviours by flexible pricing and other policies</i></li> </ul>

Objectives	Consideration
	<p data-bbox="807 371 1294 454"><i>iv. develop and implement travel plans to encourage the use of sustainable transport choices."</i></p> <p data-bbox="791 468 1294 573">The Planning Proposal includes maximum car parking rates and end of trip facilities requirements which would encourage use of sustainable transport modes.</p> <p data-bbox="791 586 1294 799">This first stage of Implementation of PRCUTS is focused around existing public transport infrastructure, such as Lewisham light rail station, Taverners Hill light rail station, bus network along Norton Street and future Metro station in Five Dock near Croydon so that the future developments can optimise on these public transport stops.</p>

**Direction 2: A collaborative city**

<p data-bbox="355 896 775 972">Objective 5: Benefits of growth realised by collaboration of governments, community and business</p>	<p data-bbox="791 896 1294 1108">This objective is primarily concerned with designated Collaboration Areas. The only such area in the Inner West part of the PRCUTS is Camperdown Ultimo, which is not covered by this Planning Proposal. The GSRP recommended "Potential indicator" for this objective is <i>"Increased use of public resources such as open space and community facilities"</i>.</p> <p data-bbox="791 1122 1294 1435">This is not directly related to the Planning Proposal and cannot be achieved through the LEP. It requires collaboration with State Government and Department of Education to expand the use of its existing open spaces to the community. The schools in proximity to the Planning Proposal area include Leichhardt Public School, Taverners Hill Infants School, Kegworth Public School and Fort Street High School. It is Council's intention to work with the State Government to increase the use of existing public resources.</p> <p data-bbox="791 1449 1294 1639">Council's Planning Proposal includes rezoning of the existing car park on 2 Hay Street, Leichhardt to new RE1 Public Recreation space. This site is currently owned by TfNSW. This rezoning is consistent with PRCUTS vision and is considered a better use of land which would benefit the surrounding community.</p> <p data-bbox="791 1675 1294 1753">Further, the Gateway Determination condition (2) requires consultation with numerous public authorities during consultation including</p>
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Objectives	Consideration
	Department of Education, Transport for NSW etc.
<b>Direction 3: A city for people</b>	
Objective 6: Services and infrastructure meet communities' changing needs	<p>The Planning Proposal and its associated urban design study support the following Objective 6 tenets <i>“Improved health, public transport and accessibility outcomes can be achieved through the provision of schools, recreation, transport, arts and cultural, community and health facilities in walkable, mixed-use places co-located with social infrastructure and local services.”</i></p> <p>It very much reflects the 30-minute city concept and endeavours to help the area evolve towards a 15-minute city.</p> <p>In this context, the Proposal seeks to locate new residential development close to existing and proposed transport, schools, recreational, arts and cultural, community.</p> <p>It does rely on public transport to be enhanced significantly on Parramatta Road to ensure that new residents can access facilities/ jobs that cannot be walked to in 30 minutes.</p> <p>A good mass transit service on Parramatta Road is necessary to meet Objective 6 to ensure <i>“Good accessibility to local services for young people, older people and culturally and linguistically diverse communities”</i>. The GSRP stresses that this <i>“is an important way of enabling people to age within their community. This contributes to improved mental and physical health and wellbeing outcomes.”</i></p> <p>Council looks forward to working with the State Government on achieving this Objective within the context defined as <i>“Delivery of the necessary facilities and services to meet people’s changing needs requires integrated planning and collaboration amongst a broad range of stakeholders. This includes considering the provision of services for both existing and planned new communities as well as the principles of intergenerational equity.”</i></p> <p>This co-operation should follow the GSRP principle <i>“Joint and shared use of facilities is encouraged to make school assets available to the community outside school hours and to give schools access to community facilities.”</i></p>

Objectives	Consideration
	<p>In addition, the GSRP chapter on Objective 6 points out that <i>“The needs of children and young people go beyond education facilities. With families increasingly living in higher density areas, there needs to be greater importance placed on how open spaces, cultural facilities and the public realm are planned, designed and managed to include children and young people”</i>. Some of these needs will only be able to be met if Parramatta Road has a high-quality mass transit system.</p>
<p>Objective 7: Communities are healthy, resilient and socially connected</p>	<p>Similar to Objective 6, the Planning Proposal aims to implement Objective 7 in partnership with State Government. This is particularly relevant to PRCUTS’ vision of transformation and revitalisation of Parramatta Road which is dependent on the provision of improved public transport.</p> <p>Efforts are also required from State Government to reduce traffic congestion on Parramatta Road, in particular east of the Hawthorne Canal, so that it can transform into a vibrant street that people can walk alongside comfortably. This is also required by PRCUTS.</p> <p>These public transport and traffic measures are necessary to accommodate the GSRP aim: <i>“Mixed-use neighbourhoods close to centres and public transport improve the opportunity for people to walk and cycle to schools, local shops and services. Enhancing the safety, convenience and accessibility of walking and cycling trips has many benefits including healthier people, more successful businesses and centres and reduced traffic congestion.</i></p> <p><i>Strategy 7.1</i></p> <p><i>Deliver healthy, safe and inclusive places for people of all ages and abilities that support active, resilient and socially connected communities by:</i></p> <ul style="list-style-type: none"> <li><i>i. providing walkable places at a human scale with active street life</i></li> <li><i>ii. prioritising opportunities for people to walk, cycle and use public transport</i></li> <li><i>iii. co-locating schools, health, aged care, sporting and cultural facilities</i></li> <li><i>iv. promoting local access to healthy fresh food and supporting local fresh food production.”</i></li> </ul>

Objectives	Consideration
	<p>To enable a healthy, resilient and socially connected built environment, this Planning Proposal includes provisions for new through-site links, improved public domain for walking and cycling, active streets and buildings that minimise energy and water consumption. In addition, it has refined PRCUTS design recommendations to provide walkable places at human scale in cognition of the local area.</p>
<p>Objective 8: Greater Sydney's communities are culturally rich with diverse neighbourhoods</p>	<p>Inner West Council has a rich cultural and linguistic diversity, which is reflected in a strong programme of services, policies, festivals, projects and engagement with all of its communities. This approach was followed in the community engagement process undertaken during the preparation of the Council's LSPS, Local Housing Strategy, Integrated Transport Strategy and Employment and Retail Lands Strategy. This Planning Proposal is based on these strategies and will be subject to further community engagement of this nature when it is exhibited.</p>
<p>Objective 9: Greater Sydney celebrates the arts and supports creative industries and innovation</p>	<p>Strategy 9.1 expands this Objective in several ways, but in particular sets the following target: <i>“Facilitate opportunities for creative and artistic expression and participation, wherever feasible with a minimum regulatory burden, including:</i></p> <ul style="list-style-type: none"> <li data-bbox="820 1137 1302 1193"><i>i. arts enterprises and facilities and creative industries</i></li> <li data-bbox="820 1193 1302 1227"><i>ii. interim and temporary uses</i></li> <li data-bbox="820 1227 1302 1283"><i>iii. appropriate development of the night-time economy.”</i></li> </ul> <p>Inner West Council is renowned for the support it provides to its creative and artistic community. This philosophy embraces an approach to urban planning that works to protect affordable employment floorspace which this community needs and proactive policies that take up the GSRP Strategy 9.1 opportunities outlined above. The “Open and Creative Inner West” Planning Proposal is a prime example.</p> <p>It proposes new complying development provisions to simplify approval processes for small-scale cultural uses. The Proposal aims to simplify the regulatory regime in the Inner West to encourage a diverse vibrant and safe night-time economy while striking a balance that will protect residential amenity.</p>

Objectives	Consideration
	<p>This Planning Proposal includes provisions to provide active street frontages along key streets. In conjunction with Council’s Open and Creative Inner West, the Planning Proposal will help achieve this Objective.</p> <p>Plans are also underway to consult with the community in Leichhardt to designate Leichhardt as a Special Entertainment Precinct. This would mean that a plan is in place to manage entertainment sound. This will require new buildings to be designed to be soundproof from existing and future entertainment sound in the area.</p> <p>In addition, Council’s LSPS and EaRLS have numerous policies designed to support the area’s creative community and businesses.</p>
<p><b>Direction 4: Housing the city</b></p>	
<p>Objective 10: Greater housing supply</p>	<p>This Planning Proposal is based on Council’s and GCC endorsed LHS and an in-depth investigation of opportunities for new residential development identified in PRCUTS.</p> <p>In response to Action 4 of this Objective, this Planning Proposal seeks to address DPE’s 6-10-year shortfall of up to 1600 dwellings in the Inner West.</p> <p>The Planning Proposal also re-distributes where and how much new development will be in these three precincts in the first stage of PRCUTS implementation in response to detailed urban design studies and current policy context. Below is a breakdown of proposed dwelling growth in individual precincts in this Planning Proposal:</p> <ul style="list-style-type: none"> <li>i. Leichhardt – 707 new dwellings</li> <li>ii. Taverners Hill – 393 new dwellings</li> <li>iii. Kings Bay/ Croydon – 416 new dwellings</li> <li>iv. Total – 1516 new dwellings</li> </ul> <p>The proposed phasing is based on levels of existing/proposed transport infrastructure in response to GSRP Objectives 1-4 as discussed previously.</p> <p>The supporting studies with this Planning Proposal and PRCUTS have identified opportunities for additional growth beyond this Planning Proposal area which will be delivered through future LEP amendments.</p>

Objectives	Consideration
Objective 11: Housing is more diverse and affordable	<p>The supporting urban design study recommends a range of building typologies which will encourage diversity in dwelling types.</p> <p>This Planning Proposal includes provisions to implement the Affordable Housing Contribution Scheme (AHCS) in the Leichhardt precinct under <i>State Environmental Planning Policy (Housing) 2021</i> (Housing SEPP).</p> <p>The Scheme recommends 2% of Residential Strata in Leichhardt precinct be provided as Affordable Housing Contributions. GSRP p. 70 notes that it is viable for new developments to provide 5-10% of new residential floorspace as affordable rental housing.</p> <p>DPE’s viability tool indicates that an AHCS would not be viable for Taverners Hill and Kings Bay/ Croydon precincts. As such, the AHCS is only proposed to apply in the Leichhardt precinct.</p>

**Direction 5: A city of great places**

Objective 12: Great places that bring people together	<p>The essence of this objective is set out in its Strategy 12.1 <i>“Using a place-based and collaborative approach throughout planning, design, development and management, deliver great places by:</i></p> <ul style="list-style-type: none"> <li data-bbox="815 1099 1294 1182"><i>i. prioritising a people-friendly public realm and open spaces as a central organising design principle</i></li> <li data-bbox="815 1184 1294 1267"><i>ii. recognising and balancing the dual function of streets as places for people and movement</i></li> <li data-bbox="815 1270 1294 1384"><i>iii. providing fine grain urban form, diverse land use mix, high amenity and walkability in and within a 10-minute walk of centres</i></li> <li data-bbox="815 1386 1294 1469"><i>iv. integrating social infrastructure to support social connections and provide a community hub</i></li> <li data-bbox="815 1471 1294 1525"><i>v. recognising and celebrating the character of a place and its people.”</i></li> </ul> <p>These principles underpin this Planning Proposal and have also led to refinement of PRCUTS recommendations to achieve local place-based outcomes.</p> <p>Delivering Parramatta Road as a great place requires a whole-of-government approach which prioritises people friendly public-realm and recognises the importance of this road as a street for people. Substantial improvements</p>
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Objectives	Consideration
<p>Objective 13: Environmental heritage is identified, conserved and enhanced</p>	<p>would be required to relieve traffic congestion and provide mass-transit public transport. Council looks forward to working with the NSW Government to achieve this vision.</p> <p>The Planning Proposal has been informed by a detailed heritage study by Hector Abrahams (Appendix 3). The brief for the study included all properties identified in the PRCUTS Fine Grain Study to be assessed for heritage significance, as well as the Parramatta Road/Norton Street Heritage Study completed by NBR Architecture for Leichhardt Council in 2016.</p> <p>The Abrahams study recommends 24 new Heritage Items and 1 new Archaeological Site, as well as an extension of the Excelsior Subdivision Heritage Conservation Area (HCA) to include three additional properties, and a small new HCA along Barker Street, Lewisham in the Taverners Hill Precinct. Heritage inventory sheets have been prepared for the properties and conservation areas recommended for listing.</p> <p>While not recommended for heritage listing by the Hector Abrahams Study, a review of Lewisham Hotel was prepared by GML Heritage Pty Ltd. The review seeks to list Lewisham Hotel as a local item, noting historical, aesthetic and social significance. Refer to Appendix 3.</p> <p>The urban design study took the findings of the heritage study and the existing environmental heritage into account in its analysis of the area and subsequent recommendations. This approach reflects the importance of integrating new development with the heritage of the area to create “distinctive local places” while managing the cumulative impact of new development on heritage and character.</p>
<p><b>Direction 6: A well-connected city</b></p>	
<p>Objective 14: <i>A Metropolis of Three Cities</i> – integrated land use and transport creates walkable and 30-minute cities</p>	<p>This GSRP Objective has three core elements:</p> <p><i>“Strategy 14.1</i> <i>Integrate land use and transport plans to deliver the 30-minute city.</i></p> <p><i>Strategy 14.2</i> <i>Investigate, plan and protect future transport and infrastructure corridors.</i></p> <p><i>Strategy 14.3</i></p>

Objectives	Consideration
	<p><i>Support innovative approaches to the operation of business, educational and institutional establishments to improve the performance of the transport network.</i></p> <p>Overall, this objective explicitly helps implement the State Government’s Greater Sydney Future Transport Strategy 2056 city-shaping network vision.</p> <p>“City-shaping corridors” are one of the major policy planks in this vision. These are major trunk corridors with services and infrastructure, such as mass transit, express public transport services and the strategic road network, that will provide turn up-and-go access across the metropolitan region and to each of the three cities.</p> <p>The Planning Proposal area is part of the most important “city-shaping corridor” in the metropolitan area. It includes Parramatta Road itself with the Inner West railway line to the south, proposed Metro West to the north and the light rail line to the west of Taverners Hill Precinct.</p> <p>This context, Objective 14 of the GSRP and the 30-minute city access principle underpin the Objectives of this Planning Proposal.</p> <p>The GSRP reinforces how crucial it is that Planning Proposals which would facilitate increased population in the areas they cover must have the right infrastructure in place first. <i>“Providing adequate infrastructure to support population growth is essential to creating strong communities.”</i> (GSRP p32)</p> <p>Consequently the 30-minute access benchmark requires suitable public transport infrastructure to ensure that new development will have that level of accessibility.</p> <p>The residential and employment population growth that can be facilitated by this Planning Proposal must accordingly be supported by adequate infrastructure especially public transport infrastructure.</p> <p>The implementation of PRCUTS requires State Government investment for enhanced public transport along Parramatta Road in advance of any substantial new development over and above that can already be provided under existing LEP controls.</p>

Objectives	Consideration
Objective 15: The Eastern, GOP and Western Economic Corridors are better connected and more competitive	N/A
Objective 16: Freight and logistics network is competitive and efficient	Parramatta Road Corridor is an important freight and movement corridor. Council's supporting DCP includes controls to facilitate free movement of freight (including any loading docks and servicing) by encouraging access to employment uses on Parramatta Road from the rear.
Objective 17: Regional connectivity is enhanced	N/A
<b>Direction 7: Jobs and skills for the city</b>	
Objective 18: Harbour CBD is stronger and more competitive	<p>The Planning Proposal deals with an important section of the "city-shaping corridor" that provides critical connections to the Harbour CBD and in particular to the Tech Central Precinct (previously Camperdown Ultimo Collaboration Area) with its concentration of health and educational institutions.</p> <p>Strategy 18.1 requires "<i>prioritising of public transport projects to Harbour CBD to improve business-to-business connections and support the 30-minute City</i>". Parramatta Road public transport solution will significantly increase and improve accessibility to and from the Harbour CBD and thus contribute to delivering Strategy 18.1.</p> <p>Strategy 18.2 requires "<i>Infrastructure investments, particularly those focused on the access to the transport network, which enhance cycling connectivity within 5 kilometres of strategic centres or 10 kilometres of the Harbour CBD</i>". Most of the Planning Proposal area except for Kings Bay/ Croydon is within 10km of Harbour CBD and therefore, requires significant infrastructure investment to strengthen the connections with the Harbour CBD.</p>
Objective 19: Greater Parramatta is stronger and better connected	N/A
Objective 20: Western Sydney Airport and Badgerys Creek Aerotropolis are economic catalysts for Western Parkland City	N/A
Objective 21: Internationally competitive health, education, research and innovation precincts	Strategy 21.1 " <i>Develop and implement land use and infrastructure plans for health and education precincts that:</i>

Objectives	Consideration
	<ul style="list-style-type: none"> <li>• <i>create the conditions for the continued co-location of health and education facilities, and services to support the precinct and growth of the precincts have high levels of accessibility</i></li> <li>• <i>attract associated businesses, industries and commercialisation of research</i></li> <li>• <i>facilitate housing opportunities for students and workers within 30 minutes of the precinct.</i></li> </ul> <p>The Planning Proposal covers an area adjacent to the Camperdown health and education precinct Collaboration Area (now called Tech Central). Efforts are required to enhance accessibility to the Camperdown precinct by providing Parramatta Road public transport improvements plus enhancing the cycling connectivity.</p> <p>The Planning Proposal also includes provisions for affordable housing in Leichhardt which is within 30 minutes of the Camperdown health and education precinct Collaboration Area.</p>
Objective 22: Investment and business activity in centres	This Planning Proposal will encourage new residential and business developments which will in turn strengthen the local centres in the area.
Objective 23: Industrial and urban services land is planned, retained and managed	<p>This Planning Proposal area does not include any industrial lands and therefore, the ethos of Objective 23 is complemented.</p> <p>Two E3 zoned sites have been investigated for residential uses in the Kings Bay/Croydon precinct. Appropriate provisions have been incorporated in this Planning Proposal and supporting DCP amendments to ensure that the site continues to provide employment uses on ground floor and does to result in adverse impacts on the ongoing employment role of the precinct.</p>
Objective 24: Economic sectors are targeted for success	The Planning Proposal seeks to facilitate urban support services in Kings Bay/ Croydon Precinct. Council's supporting DCP includes provisions to ensure ground floor non-residential floorspace is flexible and provides for the likely spatial and functional requirements of urban support services.
<b>Direction 8: A city in its landscape</b>	
Objective 25: The coast and waterways are protected and healthier	Council is committed to protecting and making its coastline and waterways healthier.

Objectives	Consideration
	<p>This is reflected in its LSPS Planning Priority 4: <i>Inner West is a water sensitive city with clean waterways</i></p> <p>Objectives:</p> <ul style="list-style-type: none"> <li>i. <i>Development on all private and public land incorporates best practice Water Sensitive Urban Design (WSUD)</i></li> <li>ii. <i>Reduced reliance on Sydney’s drinking water supply</i></li> <li>iii. <i>Water quality is enhanced, and natural waterways are clean and healthy</i></li> </ul> <p>Council’s draft DCP amendments have incorporated requirements regarding stormwater management measures including provision of water sensitive urban design.</p>
Objective 26: A cool and green parkland city in the South Creek corridor	N/A
Objective 27: Biodiversity is protected, urban bushland and remnant vegetation is enhanced	Council’s LSPS Planning Priority 3 “ <i>A diverse and increasing urban forest that connects habitats of flora and fauna</i> ” aligns with this objective and has informed this Planning Proposal.
Objective 28: Scenic and cultural landscapes are protected	N/A
Objective 29: Environmental, social and economic values in rural areas are protected and enhanced	N/A
Objective 30: Urban tree canopy cover is increased	<p>The Planning Proposal includes site-specific tree canopy and deep soil targets for certain zones. These targets have been adopted directly from the NSW Government Architect’s Greener Neighbourhoods Guide which represents best practice targets for urban greening. The proposed LEP amendments would help meet the objectives of Strategy 30.1 by mandating increased tree canopy cover in the private domain. The DCP includes broader landscaping objectives and controls applicable to the public and private domain to support this outcome.</p>
Objective 31: Public open space is accessible, protected and enhanced	<p>Planning Proposal includes provisions to create new through-site links which would enhance access to public spaces. The Planning Proposal also seeks to create new public open spaces in the Leichhardt precinct:</p> <ul style="list-style-type: none"> <li>• 2 Hay Street Leichhardt – existing car park which is recommended to be</li> </ul>

Objectives	Consideration
	<p>rezoned to RE1 Recreational Open Space.</p> <ul style="list-style-type: none"> <li>Norton Plaza – new civic open space/ plaza fronting Norton Street is proposed as part of redevelopment of the opportunity site.</li> </ul> <p>These measures would contribute towards achieving Strategy 31.1 which seeks to maximise the use of existing open space and protect, enhance and expand public open space. These are also consistent with PRCUTS.</p> <p>In addition, Council's Recreation Needs Study has identified gaps in the existing open space and recreational infrastructure and any opportunities for improvements. This study informed the development of Council's Local Infrastructure Contributions Plan that came into effect on 20 February 2023.</p>
<p>Objective 32: The Green Grid links parks, open spaces, bushland and walking and cycling paths</p>	<p>Strategy 32.1 <i>“Progressively refine the detailed design and delivery of:</i></p> <ol style="list-style-type: none"> <li><i>Greater Sydney Green Grid priority corridors</i></li> <li><i>opportunities for connections that form the long-term vision of the network</i></li> <li><i>walking and cycling links for transport as well as leisure and recreational trips”.</i></li> </ol> <p>As above Objective indicates, the Planning Proposal and supporting DCPs include provisions for new open space, new through-site links and setbacks from key roads to provide walking and cycling infrastructure. These would connect the network of existing and proposed open spaces, footpaths and cycleways. Council's draft Blue and Green Strategy expands further on these walking and cycling links.</p>
<p><b>Direction 9: An efficient city</b></p>	
<p>Objective 33: A low-carbon city contributes to net-zero emissions by 2050 and mitigates climate change</p>	<p>This Objective suggests that areas undergoing transformative change can pursue comprehensive approaches to delivering greater reductions in greenhouse gas emissions than those possible elsewhere. It lists examples of greater use of public transport, lower parking rates and improvements to building efficiency as examples of such approaches.</p> <p>This Planning Proposal is informed by Inner West LSPS Planning Priority 1 <i>“Adapt to climate change”</i>, and Planning Priority 2 <i>“Inner West is a zero emissions community”</i> demonstrating alignment with this Direction and PRCUTS’</p>

Objectives	Consideration
	<p>aspirations. It includes provisions for new developments relying on bonus incentives to provide:</p> <ul style="list-style-type: none"> <li>i. high performing buildings with increased water and energy performance targets</li> <li>ii. car parking rates</li> <li>iii. worker facilities/ end of trip facilities</li> <li>iv. urban heat mitigation requirements including minimum targets for deep soil planting and tree canopy cover.</li> </ul>
<p>Objective 34: Energy and water flows are captured, used and re-used</p>	<p>Although this Planning Proposal area does not fall within this Objective's particular spatial focus of designated Planned Precincts, Growth Area, Collaboration Areas and State Significant Precincts, the Planning Proposal is informed by Inner West LSPS Planning Priority 2 "<i>Inner West is a zero emissions community</i>" and Planning Priority 4 "<i>Inner West is a water sensitive city with clean waterways</i>".</p> <p>The Planning Proposed includes enhanced requirements to facilitate the delivery of high-performance buildings for new developments relying on FSR and HOB incentives. The performance targets will encourage new developments to integrate solar PV and recycled water systems into building design so that energy and water flows are captured, used and reused.</p> <p>The proposed energy and water targets are generally based on PRCUTS. However, PRCUTS targets are outdated given these were released in 2016 and latest policy work done by DPE on the Sustainable Buildings SEPP.</p> <p>Consequently, this proposal includes slightly advanced performance targets as relevant in the context of state policy updates and where it can be demonstrated that better outcomes would be achieved beyond PRCUTS. Further justification is provided at Appendix 9.</p>
<p>Objective 35: More waste is re-used and recycled to support the development of a circular economy</p>	<p>Council's LSPS Planning Priority 5 is that "<i>Inner West becomes a zero-waste community</i>".</p> <p>This Priority runs in tandem with Council's Zero Waste Strategy with targets such as "<i>Reduce waste landfilled per capita by 50% by 2036.</i>"</p> <p>Council's future consolidated Inner West DCP will include measures to ensure new developments accommodate reuse, waste and</p>

Objectives	Consideration
	<p>recycling on site unless a long-term enforceable alternative can be demonstrated. New developments will also be required to provide shared spaces for temporary storage and sharing for large bulky items and space for communal gardens which include potential sites for compost bins and/or worm farms.</p>
<p><b>Direction 10: A resilient city</b></p>	
<p>Objective 36: People and places adapt to climate change and future shocks and stresses</p>	<p>The Planning Proposal is based on the LSPS's Planning Priority 1 <i>"Adapt to climate change"</i> with its objective of making the <i>"Inner West resilient to the impacts of climate change, urban and natural Hazards"</i>.</p> <p>The Planning Proposal and supporting DCPs will achieve this Objective through provisions to mitigate the effects of climate change and urban heat island by:</p> <ul style="list-style-type: none"> <li>• introducing energy and water performance targets that exceed minimum compliance standards for residential accommodation, office, shopping centres, hotels, serviced apartment developments that seek to utilise the FSR and HOB incentives</li> <li>• incorporating green infrastructure and features in private developments (green roofs and walls, deep soil gardens and landscaping)</li> <li>• requiring developments to meet specific tree canopy and deep soil targets in certain land use zones.</li> </ul>
<p>Objective 37: Exposure to natural and urban hazards is reduced</p>	<p>The Planning Proposal is informed by several studies including flooding, contamination and aircraft noise. These studies underpin how potential intensification of development in the Planning Proposal areas which are vulnerable to hazards such as flooding, noise and pollution will be limited and managed.</p>
<p>Objective 38: Heatwaves and extreme heat are managed</p>	<p>The Planning Proposal is based on the LSPS action to <i>"Mitigate the impacts of Urban Heat Island Effect in both the private and public domain"</i>. It includes measures such as incorporation of green infrastructure and proposed site-specific tree canopy cover targets which will help improve liveability by cooling the city and protecting people and communities from heat stress.</p>

Objectives	Consideration
<b>Direction 11: Implementation</b>	
Objective 39: A collaborative approach to city planning	<p>This Planning Proposal aims to promote orderly development that aligns with the objectives of Local, District and Regional planning frameworks.</p> <p>The Planning Proposal is based on Council's LSPS which was subject of extensive and in-depth community and stakeholder engagement.</p> <p>The Proposal itself has been prepared in close collaboration with DPE and a wide range of Council stakeholders.</p> <p>Preliminary engagement has taken place with:</p> <ul style="list-style-type: none"> <li>• Department of Planning and Environment</li> <li>• Sydney Airport and Commonwealth Department of Infrastructure (see further discussion under Local Planning Direction 5.3 – Development Near Regulated Airports and Defence Airfields for further details).</li> </ul> <p>Jemena regarding the gas pipeline in vicinity of Taverners Hill Precinct (see the below section under Q11).</p> <p>The Planning Proposal will be subject to further extensive collaboration and engagement with the community and stakeholders during its public exhibition.</p>
Objective 40: Plans refined by monitoring and reporting	<p>This cannot be achieved by the Planning Proposal alone and requires collaboration with all levels government to ensure that the objectives are being met.</p> <p>This is particularly relevant in the case of this Planning Proposal which is prepared by Council to facilitate implementation of the State-Government prepared and endorsed PRCUTS.</p> <p>Council looks forward to working with the NSW Government to ensure that PRCUTS' vision and objectives can be realised. Outcomes of this Planning Proposal will be reviewed and monitored to inform Council's medium to long term Implementation Plan and future planning proposals for the Corridor.</p>

**2. Eastern City District Plan 2018**

The Eastern City District Plan is a 20-year plan to manage growth in the context of economic, social and environmental matters along the way to achieve the 40-year vision for Greater Sydney. The District Plan has 22 planning priorities which planning authorities must give effect to in strategic planning and preparing planning proposals.

Table 1414 – Council’s consideration of Eastern City District Plan Objectives

Planning Priorities	Consideration
<b>Direction 1: A city supported by infrastructure</b>	
Planning Priority E1: Planning for a city supported by infrastructure	See responses to GSRP Objectives 1 – 4 above.
<b>Direction 2: A collaborative city</b>	
Planning Priority E2: Working through collaboration	See response to GSRP Objectives 5 and 39 above.
<b>Direction 3: A city for people</b>	
Planning Priority E3: Providing services and social infrastructure to meet people’s changing needs	See response to GSRP Objective 6 above.
Planning Priority E4: Fostering healthy, creative, culturally rich and socially connected communities	See response to GSRP Objectives 7 – 9 above.
<b>Direction 4: Housing the city</b>	
Planning Priority E5: Providing housing supply, choice and affordability with access to jobs, services and public transport	See response to GSRP Objectives 10 -11 above.
<b>Direction 5: A city of great places</b>	
Planning Priority E6: Creating and renewing great places and local centres, and respecting the district’s heritage	See response to GSRP Objective 12 – 13 above.
<b>Direction 6: A well-connected city</b>	
Planning Priority E10: Delivering integrated land use and transport planning and a 30-minute city	See response to GSRP Objective 14 above.
<b>Direction 7: Jobs and skills for the city</b>	
Planning Priority E7: Growing a stronger and more competitive Harbour CBD	See response to GSRP Objective 18 above.
Planning Priority E8: Growing and investing in health and education precincts and the Innovation Corridor	See response to GSRP Objective 21 above.
Planning Priority E9: Growing international trade gateways	N/A
Planning Priority E11: Growing investment, business opportunities and jobs in strategic centres	See response to GSRP Objective 22 above.
Planning Priority E12: Retaining and managing industrial and urban services land	See response to GSRP Objective 23 above.

Planning Priorities	Consideration
Planning Priority E13: Supporting growth of targeted industry sectors	See response to GSRP Objective 24 above.
<b>Direction 8: A city in its landscape</b>	
Planning Priority E14: Protecting and improving the health and enjoyment of Sydney Harbour and the District's waterways	See response to GSRP Objective 25 above.
Planning Priority E15: Protecting and enhancing bushland and biodiversity	See response to GSRP Objective 27 above.
Planning Priority E16: Protecting and enhancing scenic and cultural landscapes	N/A
Planning Priority E17: Increasing urban tree canopy cover and delivering Green Grid connections	See response to GSRP Objectives 30 and 32 above.
Planning Priority E18: Delivering high quality open space	See response to GSRP Objective 31 above.
<b>Direction 9: An efficient city</b>	
Planning Priority E19: Reducing carbon emissions and managing energy, water and waste efficiently	See response to GSRP Objective 33 -35 above.
<b>Direction 10: A resilient city</b>	
Planning Priority E20: Adapting to the impacts of urban and natural hazards and climate change	See response to GSRP Objective 36 -38 above.
<b>Direction 11: Implementation</b>	
Planning Priority E21: Preparing LSPSs informed by local strategic planning	Inner West LSPS adopted by Council and approved by GSC in March 2020.
Planning Priority E21: Monitoring and reporting on the delivery of the Plan	See response to GSRP Objective 40 above.

### 3. NSW Housing Strategy 2021-2022 Action Plan

The NSW Housing Strategy is a 20-year plan to manage and support the delivery of housing in NSW. *Housing 2041* sets long-term vision and objectives, addressing housing solutions holistically. It looks at population patterns, economic and environmental effects and social trends. The foundations for a longer-term vision include: supply, diversity, affordability and resilience. The 2021-22 Action Plan has set five priorities to address the vision which planning authorities must give effect to in the strategic planning framework and when preparing planning proposals.

Table 15 - Council's consideration of NSW Housing Strategy 2021-2022 Action Plan

Priority Area	Consistency
<p><b>Priority Area 1</b> – Enabling access to and promoting the use of data and evidence-based decision-making</p>	<p>The Planning Proposal outcomes in relation to delivery of dwellings will be monitored by Council.</p>
<p><b>Priority Area 2</b> – Providing planning, regulation and guidelines to support the NSW Government housing objectives</p> <p><i>2.2.2 Deliver the Council Accelerated Assessment Program along with the Public Spaces Legacy Program, for councils to accelerate planning assessments to help stimulate local and regional economic activity. Participating councils who are able to commit to assessment timeframe improvements will also have access to funding for public and open space</i></p>	<p>The Planning Proposal has been prepared to address Council's submission on the Public Spaces Legacy Program.</p>
<p><b>Priority Area 3</b> – Maximising the impact of NSW Government-owned land, investment or assets and government-led development projects or funding to achieve the housing vision</p>	<p>N/A</p>
<p><b>Priority Area 4</b> – Establishing a research agenda that invests in best practice and new ways of building and living</p>	<p>This Planning Proposal is invested in best practice and delivering new ways of building and living.</p>
<p><b>Priority Area 5</b> – Working with local governments and communities to achieve the NSW Government housing objectives</p>	<p>This Planning Proposal puts forward rezonings that will facilitate 1516 new dwellings. This will make a substantial contribution towards DPE's identified short to medium term (2022 – 2026) shortfall of up to 1600 dwellings in the Inner West.</p>

**4. Parramatta Road Corridor Urban Transformation Strategy**

The PRCUTS was released in 2016 by UrbanGrowth NSW with a vision, land use and transport principles to accommodate 27,000 new homes and 50,000 new jobs in a range of industries along the Corridor over the next 30 years. The Strategy is given statutory force by way of a Local Planning Direction under Section 9.1 of the Environmental Planning and Assessment Act 1979 (EP&A Act 1979). Consistency of this Planning Proposal against the PRCUTS Policy Framework is discussed in Table 16 below. A further detailed consistency assessment in respect of the PRCUTS Implementation Toolkit including Planning and Design Guidelines is provided in Appendix .

PRCUTS is supported by an implementation toolkit to assist councils and other stakeholders by guiding where and when rezoning should occur, and what infrastructure is required to support land use changes. The relevant aspects of the implementation toolkit are discussed below.

**Consistency with the PRCUTS Policy Framework (Strategy Report 2016)**

The Planning Proposal is consistent with the Strategy's Principles and Strategic Actions in the following way:

*Table 16 – Consistency with the PRCUTS Principles and Strategic Actions*

Strategic Action	Consideration
<b>Principle 1: Housing choice and affordability</b>	
Review, update or prepare a new Local Housing Strategy that implements the PRCUTS's Principles and Strategic Actions, taking into account changed economic and demographic characteristics, new transport opportunities and population projections.	Council's LHS, which reviews and implements PRCUTS' Principles and Strategic Actions, was adopted by Council on 31 March 2020.  It recommends that PRCUTS be refined to align with changed economic and demographic projections. It also identifies flaws in the proposed PRCUTS dwellings and employment yield for Leichhardt and Taverners Hill Precincts.  DPE has approved the LHS subject to a range of conditions including submitting this Planning Proposal for a Gateway determination and the preparation of an Affordable Housing Contributions Scheme (AHCS). Council has completed a draft AHCS, and this is incorporated in the Planning Proposal in Section 4 Part 2.10.
Provide 'diverse housing' for both purchase and rental markets that satisfies the objectives and Design Criteria of the Apartment Design Guide, that may include: <ul style="list-style-type: none"> <li>• lower cost market housing for rent or purchase, including new generation</li> </ul>	The Planning Proposal includes provisions to provide 'diverse housing' opportunities including affordable housing for low and very low-income households under the Housing SEPP. This would apply to Leichhardt Precinct only as the DPE viability tool indicated that affordable housing contributions are not viable in Taverners Hill and Kings Bay/Croydon Precincts.  In addition, the supporting urban design studies recommend a range of building typologies which will

Strategic Action	Consideration
<p>boarding houses with high quality shared spaces</p> <ul style="list-style-type: none"> <li>• moderately priced housing that is affordable to purchase for households earning up to \$150,000 or 80-190% of the median income</li> <li>• rental properties with long-term tenures and optional extensions in place – housing that uses design innovations, resulting in new products such as decoupled/optional car parking, which are suited to essential service workers, young ‘city makers’ early in their careers looking for ‘starter homes’, families with children, and downsizers/seniors</li> <li>• student accommodation</li> <li>• aged-care housing</li> <li>• housing that promotes innovation in other ways across type, tenure, construction</li> <li>• methodology or other mechanisms to make such housing more attainable to diversity of income groups.</li> </ul>	<p>encourage a diversity in dwelling types. This will also be achieved through the existing and future DCPs.</p>
<p>Establish a mix of dwelling sizes, including studios, one bedroom and three-bedroom dwellings to be delivered in residential, mixed use and shop-top developments that cater to the future population profile of the Precincts and Frame Areas, having regard to any recommendations of the Local Housing Strategy, the requirements of State Environmental Planning Policy No 65 – Design Quality of Residential Apartment</p>	<p>This will be achieved through the existing and future DCPs.</p>

Strategic Action	Consideration
Development, and the Apartment Design Guide.	
Explore incentives such as value sharing where rezoning is necessary to achieve renewal of private sites to capture a proportion of the increased land value to fund affordable, diverse and social housing projects.	The Planning Proposal includes provisions for value sharing through affordable housing contributions in the Leichhardt Precinct. This is outlined in Section 4 Part 2.10.
<b>Principle 2: Affordable Housing</b>	
Provide a minimum of 5% of new housing as Affordable Housing, or in-line with Government policy of the day.	A rate of 2% of residential strata development is proposed in the draft Affordable Housing Contributions Scheme (see Appendix 8) for the Leichhardt Precinct. This is in line with testing undertaken using DPE's Viability Assessment tool.
Amend the underlying Local Environmental Plan(s) to insert Affordable Housing principles.	Affordable Housing principles are already an aim of the Inner West LEP 2022.
Amend State Environmental Planning Policy No 70 – Affordable Housing (Revised Scheme) to identify that there is a need for affordable housing in all local government areas (LGAs) in the Corridor.	SEPP 70 was amended by DPE to introduce AHCS provisions in the Inner West area. It has since been superseded by the Housing SEPP.
Prepare model 'development consent' conditions for inclusion into future planning proposals/rezonings to enable the levying of monetary contributions that can be used to fund Affordable Housing.	A model 'development consent' condition has been prepared and is provided at Appendix 8. This condition will be used in conjunction with a Standard LEP clause to enable the levying of affordable housing contributions in accordance with an Affordable Housing Contributions Scheme for development in the Leichhardt Precinct.  As discussed previously, DPE's viability tool indicates that affordable housing contributions are only viable for Leichhardt precinct in this Planning Proposal.
Investigate planning provisions and mechanisms to deliver more Affordable Housing within the Precincts. These could include density bonuses or offsets, decoupled parking, relaxation of development contributions, and mechanisms to streamline and expedite assessment and approvals processes for Affordable Housing projects.	The Planning Proposal includes provisions for value sharing through affordable housing contributions in the Leichhardt Precinct and state infrastructure contributions in all precincts.
<b>Principle 2: Diverse and resilient economy</b>	

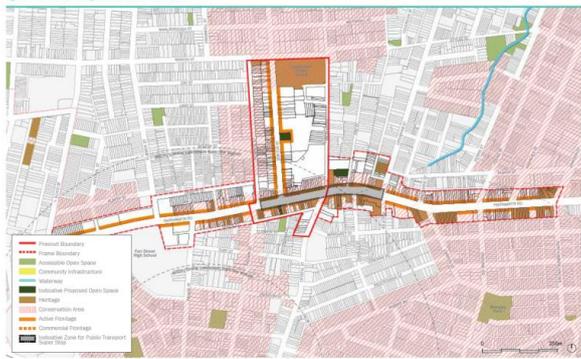
Strategic Action	Consideration
<p>Update Local Environmental Plan(s) to permit a wider range of employment uses, consistent with the recommended land uses, heights and densities identified in the Parramatta Road Corridor Planning and Design Guidelines. This includes:</p> <ul style="list-style-type: none"> <li>tailoring commercial and business zones to provide greater flexibility and opportunity for the establishment of new business models particularly for small to medium business enterprises</li> <li>facilitating increased densities to encourage the co-location of multiple uses in one building, including industrial functions, where appropriate.</li> </ul>	<p>The Planning Proposal will provide more jobs and business opportunities as it seeks to increase employment floorspace in the Leichhardt and Kings Bay/Croydon precincts.</p> <p>This Planning Proposal would deliver up to 1944 new jobs, mainly in Leichhardt and Kings Bay/Croydon Precincts. The Planning Proposal identifies two opportunity sites in Kings Bay/Croydon Precinct for new residential uses with employment uses (E3 Productivity Support) on the ground floor which will facilitate co-location of multiple uses.</p> <p>This Planning Proposal excludes all previously zoned industrial (IN2) lands. At the time of preparation of this proposal, DPE was reviewing all employment zones which have now come into effect (April 2023). Future planning proposals in the PRCUTS area will review these lands to address this principle.</p>
<p>Implement the built form controls identified in the Parramatta Road Corridor Planning and Design Guidelines to encourage new typologies that overcome these challenges and facilitate evolving and innovative employment uses.</p>	<p>Council has considered the built form controls from the PRCUTS Planning and Design Guidelines in the supporting DCPs to facilitate evolving and innovative employment uses.</p> <p>Council's urban design study has proposed high floor to floor heights for employment uses to support this objective for E1 zoned lands in Leichhardt. This approach has resulted in refinements to PRCUTS height, FSR and setback recommendations.</p>
<p>Actively explore and promote the use of the adaptable building design to enable a range of uses over time, and likely transitions in consumer preferences, transport options and travel patterns.</p>	<p>Council's urban design study has considered the provision of large, flexible floorplates and increased floor to ceiling heights as a key aspect of adaptable building design.</p>
<p>Develop planning controls that accommodate new models of large retail stores, in developments with multiple uses, in suitable parts of the Corridor, such as Auburn, Ashfield and Taverners Hill.</p>	<p>There are no large retail stores in the Planning Proposal area except at Norton Plaza which has been explored as an Opportunity Site.</p> <p>In the Taverners Hill precinct, the Planning Proposal only pursues change in residential areas and therefore this action is not relevant.</p>

Strategic Action	Consideration
Investigate the possible elevation of employment clusters or hubs in the Corridor to be recognised as Specialised Centres in A Plan for Growing Sydney and District Plans. Possible centres for consideration include Auburn as an employment hub and Camperdown as a new strategic centre.	N/A
Promote contemporary models of retail infill development, including multi-storey supermarkets and car showrooms that can offer more appropriate development outcomes within an established urban environment.	The proposed built form of employment zoned land in Kings Bay/Croydon will encourage contemporary models of retail infill.
<b>Principle 3: Accessible and Connected</b>	
Implement the Sydney CBD to Parramatta Strategic Transport Plan.	This is with TfNSW for implementation.
Amend the State Environmental Planning Policy (Infrastructure) 2007 to identify Parramatta Road between Burwood and the Sydney CBD as a strategic corridor, inserting provisions that require planning proposals and development applications along the Corridor to be referred to TfNSW for comment, particularly at and around future superstop locations.	As above.
Apply the road planning framework to guide the planning, development, management and operation of the Parramatta Road Corridor road network according to 'movement-place' principles.	PRCUTS 'movement-place' framework has been considered in developing this proposal. The Planning Proposal anticipates that TfNSW, DPE and Council will work together to make Parramatta Road a 'vibrant street' east of Hawthorne Canal as envisaged in PRCUTS.
<b>Principle 4: Vibrant communities and places</b>	
Deliver each Precinct along the Corridor as a '15-minute neighbourhood' through land use changes that implement the following principles:	These principles are embedded in the Planning Proposal and its associated urban design study.

Strategic Action	Consideration
<ul style="list-style-type: none"> <li>• improved walkability, cycling and safety to support healthier communities</li> <li>• improved housing choice and diversity – increased usability of, and access to, safe open spaces</li> <li>• improved local economic opportunities – adequate local services and infrastructure</li> <li>• access to public transport.</li> </ul>	
<p>Implement planning provisions to deliver active frontages in and around appropriate locations as illustrated on the Precinct Plans within the Parramatta Road Corridor Planning and Design Guidelines.</p>	<p>The Planning Proposal and supporting DCPs include active frontage provisions to implement this principle.</p>
<p>Strategically rezone parts of the Corridor (or where appropriate land outside the Corridor) for social infrastructure purposes in line with the Precinct Plans within the Parramatta Road Corridor Planning and Design Guidelines, Implementation Plan 2016-2023 and Infrastructure Schedule.</p>	<p>The areas and rezonings dealt with in the Planning Proposal have been carefully selected on the basis of a range of State government and Council policies and strategic studies to align with the Precinct Plans in the Parramatta Road Corridor Planning and Design Guidelines, Implementation Plan 2016-2023 and 2021 Update, and Infrastructure Schedule.</p> <p>The PRCUTS Infrastructure Schedule identifies a range of infrastructure and indicative timeframes for each Precinct. This Schedule, together with Council's own needs studies (community assets, recreational and traffic and transport), informed the Inner West Local Infrastructure Contributions Plan 2023.</p>
<p>Implement development controls that incentivise the delivery of social infrastructure, such as floor space bonuses, and discounting or excluding floor space provided as social infrastructure.</p>	<p>The Planning Proposal includes provisions for the delivery of affordable housing through State Environmental Planning Policy (Housing) 2021.</p> <p>Council's urban design study establishes maximum FSRs which can be supported based on urban design and built form testing. These are put forward in the Planning Proposal as floor space incentives.</p>
<p>Explore new models to design, finance and deliver education and health community assets in partnership with local councils,</p>	<p>While the delivery of education and health facilities are the responsibility of the State government, Council will work collaboratively where opportunities present.</p>

Strategic Action	Consideration
<p>government agencies and the private sector.</p> <p>As a first preference and where appropriate, optimise or embellish existing assets through solutions such as:</p> <ul style="list-style-type: none"> <li>• increasing the size, amenity and functionality of existing facilities to expand existing capacity</li> <li>• renewing existing assets to provide contemporary spaces or installing additional features so facilities can become multi-purpose and cater to different groups</li> <li>• upgrading features within existing facilities so they can accommodate a greater capacity</li> <li>• developing partnerships with other community infrastructure providers, including private or other government agencies, such as the Department of Education, or Local Health Districts, to enable the shared use of facilities</li> <li>• incentivising the private sector to deliver community infrastructure.</li> </ul>	<p>In developing the Works Schedule for the Inner West Local Contributions Plan 2023, the optimisation and embellishment of existing assets was considered as the first preference, where appropriate.</p>
<p>Implement development controls that encourage the adaptive reuse of heritage items in the Corridor such as additional permitted uses, heritage incentive schemes, Section 94 exemptions, and accelerated or prioritised planning processes for development that appropriately preserves, maintains and</p>	<p>Within the Planning Proposal area, there are existing and proposed heritage items and heritage conservation areas (HCAs) that will be eligible to access FSR and height incentives, thus facilitating adaptive reuse. Notwithstanding, development will need to demonstrate an appropriate heritage response that satisfies the heritage conservation objectives of the LEP.</p>

Strategic Action	Consideration
utilises these community assets.	
Implement transferable development rights for significant heritage conservation and development projects, where appropriate.	The Planning Proposal does not seek to implement a transferrable development rights scheme. As noted above, heritage items and properties within HCAs will be eligible to access FSR and height incentives, however development will need to demonstrate an appropriate heritage response that satisfies the heritage conservation objectives of the LEP.
Review and modernise the heritage listings concurrently with rezoning proposals, with a stronger focus on proactive heritage identification and preservation.	<p>This Planning Proposal, informed by the heritage study undertaken by Hector Abraham Architects, seeks to list 24 new heritage items, create a new heritage conservation area in Lewisham and extend the existing Excelsior Heritage Conservation Area in Leichhardt.</p> <p>The Planning Proposal includes a site which is outside the PRCUTS Leichhardt Precinct boundary i.e., 38-40 Renwick Street, Leichhardt. This was identified as an item for investigation in PRCUTS Fine Grain Study. Council's Heritage Study has confirmed that the site has merit to be heritage-listed and therefore it forms part of this Planning Proposal.</p>
Drawing on the Parramatta Road Corridor Planning and Design Guidelines, identify neighbourhoods and streetscapes through future rezoning processes, where existing character and amenity should be retained and should not be subject to renewal.	The supporting urban design study identifies parts of neighbourhoods in Taverners Hill precinct (suburbs of Leichhardt and Lewisham) where the existing built forms should be retained because of the contributions they make to the character, amenity, and streetscapes.
Prepare and implement a design excellence strategy.	<p>The Planning Proposal uses the incentives mechanism outlined in Section 4 Part 2.5 as a proactive means of achieving design excellence instead of a design excellence strategy.</p> <p>This mechanism will ensure that additional uplift, using bonus FSR and height incentives, is only granted when a high-quality built form design (in terms of massing, scale, modulation and materials) is demonstrated. Furthermore, the IWLEP 2022 design excellence clause (which applies to buildings of 14m or more) will continue to apply to the relevant sites at the development application stage.</p>
Incorporate the range of design approaches and measures identified in the Parramatta Road Corridor Design Guidelines to attenuate the effects of noise and air pollution.	Council's supporting DCPs include built form design measures to mitigate road noise and air quality impacts. These include measures identified in the Parramatta Road Corridor Design Guidelines.

Strategic Action	Consideration
<p>Use the development typology examples in the Parramatta Road Corridor Planning and Design Guidelines to inform future development controls.</p>	<p>PRCUTS building typologies were considered in Council's urban design study.</p>
<p><b>Principle 5: Green spaces and links</b></p>	
<p>Strategically rezone parts of the Corridor for open space purposes, with a view to allocating land to create a high-quality interconnected network of publicly accessible open space throughout the Corridor.</p>	<p>The Planning Proposal includes rezoning of 2 Hay Street, Leichhardt to RE1 Public Recreation in line with PRCUTS Planning &amp; Design Guidelines Leichhardt Structure Plan (Figure 11.9 p.231).</p> <p>There are inconsistencies within PRCUTS regarding the rezoning approach for this site. Notwithstanding, the vision for the site as new open space is clearly outlined in PRCUTS Planning &amp; Design Guidelines Figure 11.13 shown below.</p>  <p><i>Figure 7 – PRCUTS Planning &amp; Design Guidelines Leichhardt Interface map</i></p> <p>This Planning Proposal through rezoning of 2 Hay Street, Leichhardt to RE1 Public Recreation will significantly contribute towards the east-west strategic link via Dot Lane between Hay Street and Norton Street.</p> <p>The Planning Proposal also includes provisions for new through-site link/ public open space as part of the redevelopment of Norton Street opportunity site to improve accessibility and permeability. These have also been identified in the supporting DCP.</p>
<p>Provide a diverse range of connected high quality open space and public domain area to each Precinct in accordance with the Precinct Plans that ensures:</p> <ul style="list-style-type: none"> <li>local parks within 400m safe walking distance of</li> </ul>	<p>As outlined above, new open spaces and through-site links have been identified to help achieve this Principle.</p> <p>Projects completed under the PRCUTS UAIP have enhanced the public domain of north-south oriented streets that are perpendicular to Parramatta Road, including the creation of a pocket park at Petersham Street.</p>

Strategic Action	Consideration
<p>at least 95% of all dwellings</p> <ul style="list-style-type: none"> <li>• additional small local parks or urban spaces within 200m of activity centres and higher density residential areas</li> <li>• active open space within 1km of 95% of all dwellings</li> <li>• linear parks and trails linked to waterways, vegetation corridors and road reserves within 1 km of 95% of all dwellings.</li> </ul>	<p>See the Consistency with the Parramatta Road Corridor UAIP sub-section of the PRCUTS Implementation Tool Kit section below for additional details.</p> <p>The site-specific provision for Norton Street Opportunity Site (see Section 4 Part 2.11) also requires the provision of a substantial public domain area as part of future redevelopment of this site. This would involve a 25m wide plaza opening on to Norton Street, leading to an 18m wide landscaped through-site link from Norton Street to Balmain Road.</p>
<p>Implement building setbacks as identified on the Precinct Plans within the Parramatta Road Corridor Planning and Design Guidelines.</p>	<p>The PRC P&amp;DG setbacks have been refined through built form testing in Council's urban design study and will be implemented through the supporting draft DCPs.</p>
<p><b>Principle 6: Sustainability and resilience</b></p>	
<p>Commence the amendment of State Environmental Planning Policy (Sustainability Building Index: BASIX) 2004 to increase the water and energy targets as identified within the Parramatta Road Corridor Planning and Design Guidelines.</p>	<p>Sustainable Buildings SEPP 2022 came into force on 29 August 2022 and will commence on 1 October 2023. This repeals the BASIX SEPP and includes a suite of changes to increase sustainability performance standards for residential and non-residential developments. Notwithstanding, these increased standards do not exactly align with the performance standards required by PRC P&amp;DG.</p> <p>Further, the Sustainable Buildings SEPP does not include updates to energy performance standards for residential developments under 6 storeys, water performance standards or specific targets for non-residential development types and thresholds as described in PRC P&amp;DG (such as office, shopping centres, hotels).</p> <p>Both BASIX SEPP 2004 and Sustainable Buildings SEPP 2022 allow for increased sustainability requirements through an incentives FSR/HOB mechanism.</p> <p>This Planning Proposal provides FSR and height incentives to encourage new developments to meet, or where relevant, exceed the PRCUTS Sustainability and Resilience Requirements. Refer to Appendix 9 for further justification.</p>
<p>Implement comprehensive built form strategies for building efficiency, renewable energy, strategic parking, public</p>	<p>The Planning Proposal intends to achieve the PRCUTS Sustainability and Resilience Principle through key interventions for developments utilising the FSR and HOB incentives:</p>

Strategic Action	Consideration
<p>domain, and sustainable infrastructure to target the long-term achievement of:</p> <ul style="list-style-type: none"> <li>• 20% reduction in greenhouse gas emissions</li> <li>• renewable energy installation</li> <li>• 30% reduction in peak electricity demand</li> <li>• 30% reduction in water consumption</li> <li>• &gt;15% of water delivered by non-potable sources, including rainwater</li> <li>• or recycled water</li> <li>• 30% reduction in car use – 10-15% car share take-up rate.</li> </ul>	<ul style="list-style-type: none"> <li>• High performing buildings requirements</li> <li>• Car parking rates (incl. introducing maximum parking rates and encouraging unbundled car parking and carshare schemes)</li> </ul> <p>Refer to the proposed provisions outlined in Section 2.7 and 2.9.</p> <p>The draft DCPs include supporting controls which will contribute towards achieving the PRCUTS targets outlined in this Strategic Action.</p> <p>The Sustainability Study and Precinct-wide Traffic and Transport Study at Appendix and Appendix 10 respectively provide the justification for the proposed sustainability and sustainable transport controls.</p>
<b>Principle 7: Delivery</b>	
<p>Implement the Implementation Plan 2016 – 2023.</p>	<p>This Planning Proposal commences the execution of PRCUTS Implementation Plan 2016 – 2023. The Stage 1 Planning Proposal area is different to the Implementation Plan's 2016-2023 Area. This is justified as demonstrated in the Better Outcomes Study in Appendix 11.</p>
<p>Establish a robust funding mechanism to apply to new rezoning/development proposals that will fund the local and regional infrastructure demands required to service the future population growth in the renewed Corridor.</p>	<p>The Planning Proposal includes provisions to make adequate contributions towards State infrastructure to align with the additional growth capacity it proposes.</p> <p>The Inner West Local Infrastructure Contributions Plan 2023 provides a robust funding mechanism to deliver local infrastructure. The Plan was informed by a suite of needs studies, including:</p> <ul style="list-style-type: none"> <li>• Inner West Recreation Needs Study Update 2021</li> <li>• Inner West Community Asset Needs Study 2021</li> <li>• Inner West Traffic and Transport Needs Study 2021</li> <li>• Cardno's Precinct-wide Traffic and Transport Study 2022</li> </ul>
<p>Advise and assist councils in the revision of local contributions plans to address funding of local infrastructure and services in the Corridor.</p>	<p>Council looks forward to working with the State Government to address funding requirements for local infrastructure and services in the Corridor. Especially, the commitment that any Housing &amp; Productivity funds collected from this Planning Proposal area will be reinvested in this area to address funding gap of local and state infrastructure and services.</p>

**Consistency with Precinct Key Actions**

*Table 17 - Consistency with the Precinct Key Actions*

<b>Leichhardt Precinct</b>	
<b>Key Action</b>	<b>Consideration</b>
<b>Land uses</b>	
Create a truly mixed-use Precinct focused around Norton Street	The proposed employment and residential population growth in Leichhardt will contribute towards achieving this vision.
Encourage appropriately scaled residential development to attract and retain people in the core of the Precinct	The urban design study has reviewed the PRCUTS design recommendations with respect to this principle. Consequently, variations are sought to the PRCUTS recommendations to ensure that the precinct provides appropriately scaled development for optimal urban design outcomes.
<b>Transport and movement</b>	
Capitalise on the improved, high-capacity public transport connections along Parramatta Road to the Sydney CBD	<p>These improvements have not taken place yet.</p> <p>PRCUTS stresses the importance of implementing substantial improvements to public transport infrastructure to accommodate the population and employment growth it recommends.</p> <p>The Planning Proposal is crafted to deliver that growth. It can only do so if these improvements are delivered so that there is higher-capacity public transport in place along Parramatta Road to accommodate a substantial proportion of the trips generated by the growth in the Inner West part of the Corridor and in other areas of the Corridor to the west, beyond the Inner West LGA.</p> <p>Gateway determination condition 1(q) required the Planning Proposal to delete the two proposed transport infrastructure provisions in relation to NSW Government commitment to introduce an on-street rapid system and future transport infrastructure. Whilst these provisions have been deleted from the proposal, Council will continue to advocate NSW Government to provide improved, high-capacity public transport connections along Parramatta Road to Sydney CBD as per this Strategic Action.</p>
Formalise Parramatta Road as a Vibrant Street and Norton Street as a Place for People	The NSW Government will need to deliver this PRCUTS vision for Parramatta Road east of the Hawthorne Canal. This will go hand in hand with the public transport improvements along Parramatta Road required by PRCUTS.
<b>Place-making</b>	

Capitalise on new transport connections to rebadge and create a new Norton Street identity	<p>This will be subject to the NSW Government's delivery of a high-capacity public transport service along Parramatta Road.</p> <p>The Planning Proposal proposes additional development capacity on the east side of Norton Street. This, in conjunction with the improved public transport and public domain improvements, will help create a reinvigorated identity for Norton Street.</p>
Provide a 'Gateway' to Norton Street at Parramatta Road that is mirrored at Marion Street	<p>This will be implemented through Council's Public Domain Strategy.</p> <p>The Parramatta Road Urban Amenity Improvement Program (PRUAIP) initiated this work with Chiaroscuro, a new gateway light work commissioned for Norton Street.</p>
Identify short to medium-term opportunities for new public domain and spaces	<p>Council is on track to deliver the Parramatta Road Urban Amenity Improvement Plan (UAIP) (Appendix ) which identifies short to medium term opportunities for new public domain and spaces in Leichhardt precinct.</p>
Enhance side streets, including Thornley Street, Hay Street, Charles Street and Railway Street, as public spaces	<p>As above.</p>
Improve pedestrian conditions along Parramatta Road through traffic light phasing, reduced speed limits, increased crossing points, enhanced footpath design, and tree planting in side streets as well as along Parramatta Road	<p>These are dependent on the implementation of the recommendations in DPE/Council's precinct-wide Transport Study and will be delivered through a combination of State and Local Infrastructure Contributions funded projects. Council's implementation plan for the precinct-wide Transport Study recommendations is included in Appendix 10, Table 1.</p>
<b>Open space, linkages, and connections</b>	
Leverage new development to provide new open space and high quality and active public domains	<p>Provision of new plaza/open space and through-site link has been included in the Planning Proposal for the redevelopment of Norton Plaza Opportunity Site.</p>
Improve north-south movements, especially along Norton Street/Crystal Street, including a major crossing of Parramatta Road	<p>These are State-Classified Roads and improvement works are to be delivered through a combination of State and Local Infrastructure Contributions funded projects.</p>
Provide new links and a network of pedestrian and cycling connections to increase circulation and permeability in the Precinct	<p>This is being implemented through the Parramatta Road Urban Amenity Improvement Plan (UAIP).</p> <p>The Planning Proposal and associated draft DCPs identify new links and a network of active transport connections.</p>

<p>Create the Dot Lane cycle link and a new cycle link from Tebbutt Street to the GreenWay along Kegworth Street</p>	<p>The Dot Lane Cycle link will be created through the Parramatta Road UAIP and future redevelopment of sites along this link in Stage 1 Leichhardt area. This is included in the supporting draft DCP.</p> <p>Kegworth Street is not suitable for the provision of a cycle link. There is already a cycle link between Flood Street and the light rail underpass to the Greenway.</p>
<p><b>Taverners Hill Precinct</b></p>	
<p><b>Land uses</b></p>	
<p>Maintain an employment focus on both sides of Parramatta Road, Tebbutt Street, Upward Street and George Street</p>	<p>Employment lands are not in the scope of this Planning Proposal as discussed previously.</p>
<p>Retain the existing low-density character across the remainder of the Precinct</p>	<p>The urban design study has reviewed the PRCUTS design recommendations in respect of this principle. The study recommends that parts of the precinct should retain the existing low-density and fine grain character.</p>
<p>Encourage appropriately scaled residential development in select locations to attract and retain people in the core of the Precinct</p>	<p>The Planning Proposal strategically targets specific blocks in Taverners Hill to encourage appropriately scaled development in selected locations. These include:</p> <ul style="list-style-type: none"> <li>i. the southern part of Lewisham close to the railway station</li> <li>ii. the south-western corner of Leichhardt close to Taverners Hill Light Rail Station, around Kegworth, Hathern, Beeson and Tebbutt Streets.</li> </ul>
<p><b>Transport and movement</b></p>	
<p>Capitalise on the improved, high-capacity public transport connections along Parramatta Road to the Sydney CBD</p>	<p>Subject to the NSW Government's commitment to deliver a high-capacity public transport connection along Parramatta Road as discussed previously.</p>
<p>Capitalise on the good access to three major transport modes: light rail, heavy rail, and frequent buses along Parramatta Road</p>	<p>As above, existing and future public transport accessibility have been informed Council's staged implementation of PRCUTS.</p>
<p>Formalise Parramatta Road east of Hawthorne Canal as a vibrant Street</p>	<p>Efforts are required from NSW Government to deliver this in conjunction with the proposed public transport improvements along Parramatta Road.</p>
<p><b>Place-making</b></p>	

Establish a new high-amenity neighbourhood hub along Tebbutt Street/Upward Street that benefits from the Kolotex/Labelcraft site redevelopment	This area is outside the scope of the current Planning Proposal.
Create an activity node around the Taverners Hill Light Rail stop	As above
Create a new link between Upward Street and George Street	This has already been delivered through the Kolotex and Labelcraft redevelopment.
<b>Open space, linkages, and connections</b>	
Leverage new development to provide new open space and high-quality and active public domains	The Planning Proposal and associated urban design study identify design recommendations to provide a high quality, active public domain.
Complete missing links along the GreenWay, including under Parramatta Road and Longport Street	These works are being undertaken as part of Council's Greenway Masterplan. Longport Street is outside the Taverners Hill precinct boundary.
Capitalise on the proximity to light rail by providing increased connectivity for pedestrians and cyclists where possible	Subject to the NSW Government's commitment to deliver a high-capacity public transport connection.  This Planning Proposal strategically targets sites which are close to the existing light rail and heavy rail stations to capitalise on existing public transport accessibility.
Provide a new east-west link along Nestor Lane to connect Carrington Street, Old Canterbury Road and Brown Street to improve access to the Greenway.	This would be impractical as the western end of such a link would encounter a steep embankment at Brown Street and access to the Greenway on the west side of Brown Street is blocked by the Light Rail line, its western embankment and the Hawthorne Canal. Consequently, this PRCUTS proposal will not be pursued by Council.
Create a new east-west cycle link along the extended Nestor Lane	Same as above
Connect Upward Street, George Street and Flood Street as a new pedestrian and cycle link	Proposed links between Tebbutt Street and Upward Street; and George Street and Flood Street have been investigated as part of the urban design study.  The study recommends that the proposed connection between Tebbutt Street and Upward Street could be relocated to a more southerly alignment which can also accommodate an overland flowpath with a new stormwater connection/culvert underground to enhance flood management.  Appropriate mechanism to implement this requires further investigation and will be

	dealt with as part of a future Planning Proposal in Taverners Hill precinct.
Prioritise Tebbutt Street and Flood Street as key pedestrian routes	This requires footpath enhancement along Tebbutt and Flood Streets which will be funded through Council's Local Infrastructure Contributions Plan.
<b>Kings Bay/Croydon Precinct</b>	
<b>Land uses</b>	
Facilitate the mixed uses of land on both sides of Parramatta Road between Regatta Road and Scott Street	N/A – in Canada Bay LGA
Develop a new local village around Spencer Street	N/A
Encourage medium and high-density residential development north of Parramatta Road, with limited, appropriately scaled residential development south of Parramatta Road	<p>The urban design study prepared in support of the Planning Proposal has reviewed the PRCUTS design recommendations with respect to this principle.</p> <p>New opportunities for medium and high-density residential development along Croydon Road and Parramatta Road have been investigated to take into account the proximity of the future Five Dock Sydney Metro West station. This Metro had not been proposed when PRCUTS was adopted.</p> <p>Medium density residential development is also encouraged in the residential area to the south of Parramatta Road along Dalmar Street.</p>
<b>Transport and movement</b>	
Capitalise on reliable, frequent rapid transit to service the new population	<p>Subject to the NSW Government's commitment to deliver a high-capacity public transport connection along Parramatta Road.</p> <p>The Planning Proposal and associated urban design study have considered opportunities for increased densities to capitalise on the future improvements to public transport along Parramatta Road and Sydney Metro West.</p> <p>The Five Dock Sydney Metro West station will be approximately 800m – 1km from the proposed opportunity sites along Croydon Road and Parramatta Road. PRCUTS recommendations for Kings Bay/ Croydon have been refined to capitalise on the development opportunities that will arise from this future Metro Station.</p>

Reinforce active transport links to Croydon Station	Active transport links to Croydon Station and the Sydney Metro West station at Five Dock will be delivered via State and Local Infrastructure Contributions funded projects
Benefit from the potential decrease in traffic volumes on Queens Road because of WestConnex	N/A
Recognise Parramatta Road will continue to have a movement function	This has been reinforced through the retention of business uses along Parramatta Road which will continue to be serviced via Parramatta Road and rear laneways, where possible.
Formalise Spencer Street as a Place for People	N/A
Formalise Parramatta Road west of Walker Street and east of Short Street as a Vibrant Street	N/A
Reduce residential car parking rates to decrease car dependency, increase use of public transport and improve traffic conditions	The Planning Proposal includes provisions to decrease car dependency in line with PRCUTS recommendations.
<b>Place-making</b>	
Create a new fine-grain road network and a mixture of uses and activities anchored on the Queens Road, Spencer Street and William Street intersections	N/A
Establish a new, high-amenity neighbourhood hub, focussed around Spencer Street, with low traffic	N/A
Retain and build on existing lifestyle/recreation businesses in and around the Precinct	N/A
Facilitate site amalgamation in appropriate locations to provide opportunities for redevelopment	Site amalgamation provisions have been included in the Planning Proposal to provide opportunities for redevelopment and preclude site-isolation.  Site amalgamation guidelines have also been included in the draft DCPs. These are based on the outcomes of urban design and built form testing.
<b>Open space, linkages, and connections</b>	
Leverage development to provide new open space, a north-south plaza between Spencer Street and Queens Road, and green corridors between Parramatta Road and the foreshore	N/A

Create mid-block, through-site links and prioritised pedestrian connections	Desired through-site links have been identified in the draft DCPs to link into Canada Bay Council's active transport network.
Deliver new cycle links along Short Street, Grogan Street, Acton Street, William Street and through Barnwell Park Golf Course and Walker Street	N/A
Construct the new, separated, regional cycle path along Queens Road, connecting Concord in the west to Iron Cove in the east	N/A

**PRCUTS Implementation Tool Kit**

PRCUTS Implementation Toolkit has been given statutory force through the associated s9.1 Local Planning Direction and must be considered by Councils and stakeholders when making land use decisions. The toolkit includes:

1. Planning and Design Guidelines
2. Implementation Plan 2016-2023
3. Implementation Plan 2021
4. Urban Amenity Improvement Plan
5. Infrastructure Schedule

**Consistency with Parramatta Road Corridor Planning and Design Guidelines (PRC P&DG)**

The Planning and Design Guidelines have been developed to inform future controls in local environment plans and development control plans and should be considered when the Strategy is being implemented through rezoning proposals.

The Planning Proposal is generally in line with the vision and objectives of PRC P&DG. Some of the requirements in the P&DG relating to character and built form principles, when tested at local scale, have resulted in refinement of its land-use, building height and density recommendations. This is justified through detailed site-scale testing undertaken as part of the Urban Design Study at Appendix 2.

Some refinements are also required to fix numerous discrepancies in the PRC P&DG relating to mismatch of height and density recommendations, and mismatch of maps/diagrams with the corresponding text. There are also flaws in its proposed growth projections which have been confirmed and agreed with DPE as part of the work undertaken by Council for the Parramatta Road Corridor Precinct-wide Traffic and Transport Study.

*Appendix* provides a detailed consideration of this Planning Proposal against the Planning and Design Guidelines. Any inconsistencies are justified in *Appendix* and the supporting Urban Design Study at *Appendix 2*.

**Consistency with Parramatta Road Corridor Implementation Plan 2016 – 2023**

This Implementation Plan establishes a sequencing strategy identifying areas of the Parramatta Road corridor to be redeveloped prior to or after 2023. Part of the Planning Proposal area in Taverners Hill and Kings Bay/ Croydon precincts are outside the 2016-2023 Implementation area. This Planning Proposal is inconsistent with this Implementation Plan 2016-2023.

Notwithstanding the Implementation Plan 2016-2023 has been superseded by DPE's more recent Implementation Plan Update 2021 which is discussed in the below section.

Under s9.1 Local Planning Direction 1.5 for implementation of PRCUTS, planning proposals in the Corridor can be supported if:

- i. The proposal is consistent with the Implementation Plan 2016 – 2023 (November 2016) and the Parramatta Road Corridor Urban Transformation Implementation Update 2021, as applicable; or
- ii. The proposal is consistent with the 'Out of Sequence Checklist' in the Implementation Plan 2016 – 2023; or
- iii. The proposal can demonstrate through a justification study that it will deliver better outcomes than those identified in the *Parramatta Road Corridor Urban Transformation Strategy 2016* and the *Parramatta Road Corridor Implementation Plan 2016-2023*.

This proposal relies on (iii) above to demonstrate its strategic merit. *Appendix 11 – Parramatta Road Corridor Implementation Stage 1 - Justification Study* provides a detailed consideration of applicable Regional, State and Council policies and brings together the suite of technical studies which have been prepared to inform this Planning Proposal.

This study demonstrates that ongoing GCC, State Government and Council policy development for the Parramatta Road Corridor Precincts have reflected and improved on the original PRCUTS vision and objectives to deliver better outcomes. The Planning Proposal demonstrates that this arc of policy development is embodied in the proposition it puts forward. Consequently, the consistency test (b) under Local Planning Direction 1.5 is fully satisfied.

This in turn means that the Proposal can be inconsistent with the Implementation Plan and does not need to meet the Out of Sequence test, but still be consistent with the applicable Local Planning Direction.

**Consistency with Parramatta Road Corridor Implementation Plan Update 2021**

This document was released in June 2021 and provides updated implementation actions to suit the contemporary planning and policy context, supplementing the Implementation Plan 2016-2023.

The Planning Proposal is consistent with the PRCUTS Implementation Update 2021, which outlines 6 new and amended implementation actions for consideration:

*Table 18 - Consistency with Implementation Plan Update 2021*

New and amended implementation actions	Consistency
<p><b>Timing of release</b></p> <p>The timing of release considerations is replaced with:</p> <ol style="list-style-type: none"> <li>1. From the date of the Implementation Update, the progression of planning proposals in the 2016-2023 Release Area, or planning proposals for whole precincts can progress to a Gateway determination notwithstanding the status of any precinct wide traffic study. Planning proposals on individual sites</li> </ol>	<p>Land in the Leichhardt Precinct that is affected by this Planning Proposal is primarily within the 2016-2023 Release Area, with the exception of the Hay St carpark site and items proposed for heritage listing.</p> <p>Land in the Taverners Hill Precinct is both within the 2016-2023 Release Area (Tebbutt St properties) and within the 2021 Release area of whole precinct (solid red boundary).</p>

<p>and in Frame Areas can still be considered for progression using part 5(a) or 5(b) of the Direction (now referred to as (a) and (b) under 'Consistency')</p>	<p>In the Kings Bay/Croydon Precinct, the affected land is outside the 2016-2023 Release Area, both within the Precinct boundary (entirety of Precinct that is located in the Inner West LGA) and the Frame Area.</p> <p>In addition, proposed heritage items and heritage conservation areas are both within the Release Areas and outside of the precincts.</p> <p>As outlined in this action, the Local Planning Direction allows proposals that are inconsistent with the timing of release consideration to progress where they have been justified by a study demonstrating the delivery of better outcomes than PRCUTS. This is provided as Appendix 11.</p>
<p><b>Public transport</b></p> <p>The following considerations are added:</p> <ol style="list-style-type: none"> <li>1. Planning proposals must have regard to any relevant published plans by Transport for NSW for improved public transport in the corridor. DPE may require a planning proposal be amended to align with a plan published by Transport for NSW.</li> <li>2. DPE may also require a planning proposal to be amended to align with a future planning, transport or infrastructure plan developed by councils and NSW Government in response to Sydney Metro West.</li> </ol>	<p>Transport for NSW's Future Transport Strategy 2056 provides strategic directions for future planning, investment, delivery and operations.</p> <p>This Planning Proposal complements the strategic directions relating to improved connectivity, accessibility, reduced environmental impact and supporting growth through smarter planning. Specific priorities of the Strategic Direction <i>P1 Supporting growth through smarter planning</i> will be implemented through this Planning Proposal:</p> <ul style="list-style-type: none"> <li>• Support growth around public transport</li> <li>• Ensure public transport is available on day one</li> <li>• Improve parking provision and management</li> </ul> <p>There are no specific actions in the Future Transport Strategy 2056 relating to Parramatta Road. There is reference to rapid bus network as non-operational infrastructure and services including committed, funded and visionary projects.</p> <p>This Planning Proposal fully supports and implements PRCUTS vision of a revitalised Parramatta Road Corridor, which is dependent on the provision of improved public transport (on-street rapid transit system) and reduced reliance on private cars.</p>

	<p>The Planning Proposal also responds to the NSW Government's investment in Sydney Metro West by strategically selecting opportunity sites in Kings Bay/Croydon precinct for residential uses to capitalise on the proximity to the future Five Dock) Metro Station to create more liveable, sustainable, and walkable neighbourhoods.</p>
<p><b>Active transport</b></p> <p>The following considerations are added:</p> <ol style="list-style-type: none"> <li>1. Planning proposals must have regard to any relevant published plans by Transport for NSW or Council endorsed local plans for active transport. DPE may require a proposal to be amended to align with these plans.</li> <li>2. DPE may also require a planning proposal to be supported by additional or alternative active transport solutions if the proposed rezoning is inconsistent with the PRCUTS Planning and Design Guideline.</li> </ol>	<p>Improving active transport infrastructure and use is a priority of the Planning Proposal. This is supported by the NSW Government's Future Transport Strategy, Council's Integrated Transport Strategy and the findings of the precinct-wide traffic and transport studies. Council's response to the Transport Study's active transport actions is provided at Table 1, Appendix 10.</p>
<p><b>Open Space</b></p> <p>The following considerations are added:</p> <ol style="list-style-type: none"> <li>1. Planning proposals must have regard to any relevant open space plans published by NSW Government or endorsed by council. DPE may require a planning proposal to be amended to align with these plans.</li> </ol>	<p>NSW Government's Public Open Space Strategy 2022 provides a framework for implementing policies across Government and contribute to a coordinated approach to public open space planning and delivery.</p> <p>The Planning Proposal seeks to create new public open spaces in the Leichhardt precinct:</p> <ul style="list-style-type: none"> <li>• 2 Hay Street Leichhardt – existing car park which is recommended to be rezoned to RE1 Recreational Open Space.</li> <li>• Norton Plaza – new civic open space/ plaza fronting Norton Street proposed as part of redevelopment of the opportunity site.</li> </ul> <p>The Planning Proposal also includes provisions to create new through-site links which will enhance access to public spaces. These measures would contribute towards achieving the objectives in the NSW Government's Public Open Space Strategy.</p> <p>Further, Council's Recreation Needs Study has identified gaps in the existing open space and recreational infrastructure and</p>

	<p>any opportunities for improvements. This study informed the development of Council's Local Infrastructure Contributions Plan that came into effect on 20 February 2023 and will fund the recreational infrastructure in the area.</p>
<p><b>Road improvements and upgrades</b></p> <p>The road improvements and upgrades considerations are replaced with:</p> <ol style="list-style-type: none"> <li>1. Planning proposals must have regard to the necessary road improvements and upgrades identified in completed precinct-wide traffic studies. DPE may require a planning proposal to be amended to address recommendations of completed traffic studies, including but not limited to setbacks to support active, public or private transport improvements, or controls to manage traffic and parking impacts.</li> <li>2. If Gateway is granted prior to the completion of a precinct-wide traffic study, DPE may impose a condition requiring the planning proposal to be updated prior to finalisation to address the recommendations of the completed traffic study.</li> <li>3. No planning proposal is to be finalised until the relevant precinct-wide traffic study is complete or alternate traffic study approved by the Minister for Planning and Public Spaces or his delegate.</li> </ol>	<p>Council and DPE jointly commissioned a Precinct-wide Traffic and Transport Study which was undertaken between 2018 and 2022. It focussed primarily on the IWC Precincts of Camperdown, Leichhardt and Taverners Hill.</p> <p>Furthermore, IWC and DPE commissioned localised traffic analysis for the Inner West section of Kings Bay/ Croydon Precinct. This report finalised in April 2022. This was in addition to the precinct-wide traffic study for the Kings Bay/Croydon Precinct prepared for Canada Bay Council.</p> <p>This Planning Proposal is informed by the recommendations in the precinct-wide traffic study particularly as relevant to:</p> <ul style="list-style-type: none"> <li>• increasing mode share to sustainable transport</li> <li>• reducing private car dependency</li> <li>• optimising existing transport infrastructure.</li> </ul> <p>The above, alongside Council's work on the implementation of this traffic study is discussed in Appendix 10.</p>
<p><b>Funding framework or satisfactory arrangements</b></p> <p>The funding framework or satisfactory arrangements considerations are replaced with:</p> <ol style="list-style-type: none"> <li>1. DPE may impose a Gateway condition or otherwise amend a planning proposal to address State infrastructure requirements.</li> </ol>	<p>The Planning Proposal seeks to introduce a clause requiring satisfactory arrangements to be made for the provision of State public infrastructure prior to the granting of consent for development in the three Precincts.</p>

**Consistency with Parramatta Road Corridor Urban Amenity Improvement Plan**

The Parramatta Road Urban Amenity Improvement Program (UAIP) projects are self-contained and deliverable in the short-term and will provide a better, more liveable environment while building momentum for more ambitious changes and projects involving the transformation of Parramatta Road itself as well as public spaces adjacent to it.

The proposed improvements include three categories of projects:

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- i. Streetscape upgrades including tree planting, multi-purpose lighting, new pavements and north-south pedestrian and cycle crossings.
- ii. Creation of new or improved open spaces, urban plazas and town squares
- iii. New walking and cycling links to key transport nodes and open spaces which connect to strategic regional and local networks.

The projects and descriptions were developed by Urban Growth NSW through an iterative process with councils along the corridor. The projects aim to build on the existing strengths of the neighbourhoods and reflect their sense of place and character.

*Appendix* details projects identified through the UAIP for Leichhardt and Camperdown Precincts. The relevant projects for Leichhardt Precinct, which have largely been completed, include:

- Public domain improvement to key north-south streets perpendicular to Parramatta Road: Rofe Street, Renwick Street, Norton Street, Balmain Road, Catherine Street and Crystal Street
- New cycle connection along Dot Lane
- Conversion of Petersham Street to a pocket park

See Figure 8 8 below which identifies the location of abovementioned projects.



Figure 8 8 – Location of Leichhardt precinct UAIP works

**Consistency with Parramatta Road Corridor Infrastructure Schedule**

A prioritised and costed list of future infrastructure including open space, transport, traffic community, health and education facilities is required to support the long-term growth in the Corridor. Parramatta Road Corridor Infrastructure Schedule provides a preliminary list of

works and associated costing which need to be undertaken by State and local Government to support the growth in this Corridor.

This Infrastructure Schedule was prepared in 2016 and is out of date with respect to its proposed costs. It also has some gaps with respect to the list of infrastructure it identifies. The Infrastructure Schedule was reviewed in the preparation of the Inner West Local Contributions Plan 2023, which came into effect on 20 February 2023. This included incorporating items identified in the PRCUTS into the works schedule of Council's Contributions Plan, together with updated costings.

The Parramatta Road Corridor Infrastructure Schedule also relies on state/ regional infrastructure contributions being made by new development within the Corridor. The Planning Proposal includes provisions to require satisfactory regional infrastructure contributions prior to any consent being granted.

#### **Consistency with Parramatta Road Corridor Precinct-wide Traffic and Transport Study (IWC/DPE)**

IWC and DPE jointly commissioned Cardno consultants to prepare a transport study analysing the transport network implications of proposals contained in the Parramatta Road Corridor, in combination with the numerous adjacent infrastructure projects, including WestConnex and Sydney Metro West.

This Transport study was commenced in 2018 and finalised in March 2022 through a series of iterations over the four years. This study focused primarily on the IWC precincts of Camperdown, Leichhardt and Taverners Hill. The report includes four sub-reports:

- i. *Context Report* which provides a comprehensive background analysis of the land use and transport in the area to inform future recommendations.
- ii. *Future Modelling Report* which investigates the traffic network along the Parramatta Road Corridor including an AIMSUM hybrid traffic simulation model.
- iii. *Parking Policy and Rate Review* which provides car parking recommendations in the key precincts to achieve sustainable mode share.
- iv. *Transport Plan* which sets the vision for Parramatta Road Corridor and provides set of recommendations including the Implementation Plan.

In addition, Cardno (now Stantec) was commissioned by IWC and DPE to undertake further localised traffic analysis for Inner West section of Kings Bay/Croydon Precinct. This report, finalised in April 2022, concurs with the utilisation of proposed maximum PRCUTS car parking rates for uplift in the IWC component of the Kings Bay/ Croydon Precinct. Note: Canada Bay Council have also prepared a precinct-wide traffic study for the Kings Bay/Croydon precinct which provides the contextual overview of the wider precinct.

Cardno generally used the PRCUTS' principles and its assumptions to set the framework for the Transport Plan's recommendations. The Transport Plan in its final set of recommendations adopts many aspects of the PRCUTS, including its vision:

*"Incremental renewal of the Corridor will occur over the long term to deliver a high quality, multi-use Corridor with improved transport choices, better amenity, and balanced growth of housing and jobs."*

In order to achieve this vision, it will be essential to implement a comprehensive, integrated approach to transport, public domain, land use and urban design. An important first step in this process is the establishment of measures which will encourage a mode shift away from private car use and so creating a more "liveable" environment in the Corridor.

The modelling in this report tests a scenario to achieve a minimum mode shift in private car use, from 79% in 2019 to 71% in 2036 (a 10% reduction of existing car use). Noting that this mode shift is required purely to achieve acceptable movement along the corridor, to achieve the desired level of liveability.

PRCUTS proposes a 30% mode shift away from private car use, as an average improvement across the three Inner West Precincts. Consequently, this Planning Proposal's key mode shift target is to reduce private car use by a minimum of 10%, and up to 30% between 2019 and 2036.

Based on experience in Australia and around the world, it is considered that sustainably based Corridor activation leads to more liveable environments in inner city areas. This requires inclusion of significantly enhanced public transport.

This traffic report also identifies numerous road network capacity increases / road widenings as part of the modelling scenarios. These road widenings are not supported by Council officers if related to increasing road capacity to accommodate additional car movements, as it directly conflicts with Council's numerous objectives relating to increasing liveability and sustainability.

Notwithstanding, there are other strategies and actions recommended in the Transport Plan which are strongly supported by Council, including, but not limited to, proposals such as:

- i. Implement an enhanced public transport solution in the corridor which achieves the following outcomes:
  - Safety;
  - Place-making benefits;
  - High capacity;
  - High frequency;
  - Reliability;
  - Travel time efficiency; and
  - Streetscape improvements.
- ii. Develop a travel behavioural change plan.
- iii. Initiate public domain improvements along Parramatta Road and on adjacent side streets.
- iv. Provide gateway treatments at key intersections.
- v. Construct missing links in the walking and cycling network, including completing a cycle route immediately to the north of, and parallel to, Parramatta Road.
- vi. Install continuous footpath treatments across the mouth of numerous streets in the Corridor.
- vii. **Work with the State Government to provide an enhanced local bus network and increased frequency of services along Parramatta Road.**
- viii. Examine reasons for crash clusters at specific intersections.
- ix. Rationalise parking controls, both kerbside and on-street, including the introduction of a maximum rate for on-site provision and the use of unbundled parking where practical.
- x. Develop controls to better accommodate the needs of freight and servicing within the Corridor.

Further details of Council's implementation of the Transport Study's recommended actions are included in Appendix 10, Table 1.

This Planning Proposal has used many of the Transport Plan's strategies and actions to cater for increased population and residents, while enabling the creation of a more liveable environment. **Intrinsically this means that private car dependence must be reduced in**

**relation to both local journeys and travel along the Corridor, to and from points beyond the Inner West LGA.**

The Kings Bay/Croydon Traffic report reaffirms the need to introduce a mass transit service on Parramatta Road, utilising dedicated right-of-way lanes and establishing a superstop in the Kings Bay/Croydon precinct. Furthermore, it recommends investigation of a new turn-up-and-go bus route between the future Five Dock Metro station and Croydon station to provide an adequate connection between two key public transport nodes.

Car parking rates justification

To achieve the PRCUTS vision, the proposal encourages people to **reduce car use and dependence and lessen the impact on local roads**. The transport study includes detailed analysis in relation to proposed car parking rates as included in Part C *Parking Policy and Rate Review* of the Appendix 10.

This Planning Proposal proposes lower maximum car parking rates specified in PRCUTS over the slightly higher alternate rates specified by Cardno. This is because the PRCUTS rates are based on a vision of high-quality public transport along the corridor, consistent with Council's objectives of this Planning Proposal. The Cardno rates are slightly higher as they assume no substantial change to the quality of public transport in the foreseeable future. As this Planning Proposal is focused around existing public transport catchments which are generally well-served, it is recommended that the lower PRCUTS parking rates be pursued to achieve the level of mode share required in this Corridor.

Maximum rates, rather than fixed or minimum rates, are recommended in both Cardno's report and PRCUTS and are being pursued through this Planning Proposal. This allows for flexibility, where developers can provide an appropriate level of parking where there is demand but are not required to provide more parking than is needed. Overprovision of parking can have significant negative impacts on design outcomes and the affordability of housing and workplaces.

Note: Resident parking permits will not be issued to occupants of these new developments. This will not impact existing entitlements for these permits.

This Planning Proposal sets maximum car parking rates based on PRCUTS maximum rates for the majority of land uses except restaurants and bulky goods. For these uses, parking rates slightly higher than PRCUTS are recommended, based on Cardno's alternate rates. This approach is necessary to address the specific needs of these specialist uses.

Use of maximum parking rates is also supported by the NSW Government's *Future Transport Strategy 2056* Action to 'Reduce recommended parking rates, improve guidance, and amend regulations to accelerate the uptake of car sharing and e-bikes'. Maximum rates are increasingly being used in cities around the world and are now used in major Sydney growth areas within North Sydney, Parramatta and the City of Sydney.

The Planning Proposal proposes to encourage unbundled, decoupled and shared car parking arrangements to improve the degree to which available parking space meets demand. Management regimes such as pricing and resident parking schemes can optimise turnover in centres and protect existing residents from increased parking demand on streets created by new development.

These car parking provisions aimed at minimising private car dependency are key to creating sustainable, liveable and affordable neighbourhoods in the Inner West.

Implementation Plan

The Transport Plan prepared by Cardno includes an Implementation Plan which provides a framework to inform implementation of key actions proposed in the Plan. This framework has eight broad categories, identifies the lead agency responsible for implementing the actions and provides an indicative timeframe for implementation. See below:

Item	Description	Action	Lead	Timeframe
1	Parking controls	Council to refine and implement parking recommendations through its Development Control Plan aligned with planning proposal to implement PRCUTS	Council and DPIE	Short term
2	Local road improvements *	Council to refine recommended local road improvements through local infrastructure planning and detailed design	Council and DPIE	Short to medium term
3	State road improvements *	TfNSW to refine recommended state road works through TfNSW's road network planning and detailed design	TfNSW	Short to medium term
4	Public Transport improvements	TfNSW implement enhanced public transport solutions through service planning and project business Cases	TfNSW	Short to medium term
5	Local bicycle network and public domain improvements	Council to refine and implement cycle path recommendations through it's capital works and local contributions plans. Opportunities exist for other funding mechanisms.	Council and DPIE	Short to medium term
6	Bicycle network improvements on state roads	TfNSW to implement bicycle network improvements on state roads through infrastructure planning and detailed design and subject to comprehensive Council and community consultation.	TfNSW	Short to medium term
7	Footpath improvements on local roads	Council to refine and implement pedestrian improvements to local roads as part of it's capital works and local contributions plans	Council and DPIE	Short to medium term
8	Footpath improvements on state roads	TfNSW to implement place based recommendation for state roads as part of project business cases	TfNSW	Short to medium term

Figure 9.9 – Implementation Plan

In summary, Council's primary responsibilities include the implementation of new parking controls, as well as improvements to:

- local roads;
- local active transport network; and
- areas of public domain.

Council will also continue discussions with the State Government in relation to public road re-configuration and public transport improvements, as well as enhancement of the regional bicycle network.

While the Future Modelling Report included a series of site-specific measures to increase road capacity on Parramatta Road, the Transport Plan does not commit to these measures, stating that:

*'... the interventions tested are predominantly based on a traffic operation improvement perspective and may not reflect the shared nature of the different urban environments. Changes to intersections need to be considered not just in terms of the vehicle movements but also active transport, public transport and place outcomes. The funding mechanisms for road upgrades / infrastructure contributions will need to be agreed by Council and DPE.'*

This Planning Proposal has been prepared based on the assumption that with increasing population and employment, it will be essential to significantly improve public transport in the

Corridor, and further, that any measures to increase traffic flow on Parramatta Road should only be introduced if they benefit active and public transport.

This position is also reflected in the actions recommended in the Transport Plan and the Kings Bay/Croydon Transport Assessment, particularly noting the absence of recommendations for traffic capacity increases. Council's response against each action is included in Table 1 of Appendix 10.

In moving forward, it will be essential for Council, DPE and TfNSW to work together to:

- i. Prioritise the actions recommended in the Transport Plan;
- ii. Examine funding sources for the actions;
- iii. Create a formal agreement on processes to implement the Plan's actions;
- iv. Establish and agreed set of criteria linking population and employment growth with public transport improvements.

Council looks forward to continue working with the NSW Government to introduce both local and regional measures to facilitate the realisation of PRCUTS vision of a healthier, more sustainable and more liveable Corridor.

#### Consistency with PRCUTS Reference Reports

- i. **Fine Grain Study** - The relevant principles have been discussed in the Planning and Design Guidelines section above.
- ii. **Social Infrastructure Analysis Report** - Social Infrastructure impacts have been considered when drafting the provisions of the Planning Proposal. Refer to Q9 under the Strategic Merit Assessment Test in the Planning Proposal.
- iii. **Sustainability Implementation Plan** - The Planning Proposal is consistent with the objectives of this Plan and intends to deliver better outcomes than those envisaged in this Plan.

Council commissioned a Sustainability Study as required by the Gateway determination conditions 1 (h), (i) and 3 (d). This included a comprehensive review of the PRCUTS performance targets against the current minimum compliance standards (Sustainable Buildings SEPP 2022) and current best planning policy practice. The proposed performance targets are provided in Part 4 Section 2.7 of the Planning Proposal. Refer to the Sustainability Study provided at Appendix for supporting information justifying the proposed building performance standards.

- iv. **Economic Analysis Report** - The Planning Proposal is underpinned by Council's detailed economic analysis including Employment and Retail Lands Strategy and Feasibility Study. Overall, this Planning Proposal will have positive economic outcomes as it has the potential to generate 1944 new jobs.

#### Q4. Is the planning proposal consistent with a council LSPS that has been endorsed by the Planning Secretary or GCC, or another endorsed local strategy or strategic plan?

The Planning Proposal represents a logical progression in implementing Council's LSPS and several other Council strategies. An overview of these strategies, and how the Planning Proposal responds is outlined below. These Strategies can be found [here](#).

#### 5. Our Inner West 2036 - Council's Community Strategic Plan

The *Inner West 2036 Community Strategic Plan* (CSP) was adopted in June 2022. The Plan seeks to create a sustainable, progressive, networked, vibrant and creative future for the Inner

West LGA. The plan reflects the values of the Inner West community and is the foundation of all decision-making, actions taken and management of resources.

The CSP identifies five strategic directions to guide planning in the Inner West:

1. An ecologically sustainable Inner West
2. Liveable, connected neighbourhoods and transport
3. Creative communities and a strong economy
4. Healthy, resilient and caring communities
5. Progressive, responsive and effective civic leadership

This Planning Proposal will assist in achieving these strategic directions by:

- Providing a clear and consistent planning framework, and process, that respects the distinct character of the Inner West.
- Maintaining existing mechanisms to support the preservation of new and existing heritage items, as well as maintaining the character of heritage conservation areas.
- Managing the intensification of residential and commercial uses through design guidelines and controls that ensure new development provides an appropriate response to the existing environment.
- Retaining employment and industrial lands as recommended in Council's *Employment and Retail Lands Strategy*.
- Revitalising areas of Norton Street, Tebbutt Street and Parramatta Road through appropriate intensification of residential and commercial uses, active transport links and the delivery of public places and open space.
- Balancing growth and development in the Inner West with adequate provision of infrastructure.

## 6. Inner West Local Strategic Planning Statement – Our Place Inner West

The Inner West LSPS came into effect on 31 March 2020. The LSPS outlines a long-term strategic vision to guide land use planning, housing and infrastructure delivery in the Inner West until 2036. The 20-year vision reflects the values of the Inner West community.

LSPS's Vision:

*The Inner West is a place that supports a high quality of life; embraces diversity and adapts to the population needs of the future; and our neighbourhoods are ecologically sustainable, economically productive and our communities are connected to one another through space, social engagement, and transport.*

The LSPS is structured around the same five CSP strategic directions with the addition of sustainable transport. To achieve the vision and these six strategic directions, the LSPS contains 14 priorities.

The most relevant LSPS priorities relating to this Planning Proposal are outlined below:

- i. *Planning Priority 2.3:* Update planning controls to improve the overall environmental performance of new buildings and precincts. This includes Parramatta Road Corridor.
- ii. *Planning Priority 13.6:* Implement the finalised housing, employment and transport strategies, and the Parramatta Road Corridor Transport Study, and prepare urban design / place based/open space studies to inform planning proposals to implement the Parramatta Road Corridor Urban Transformation Strategy: Implementation Plan

2016-2023 and Urban Amenity Improvement Plan, subject to the provision of public mass transit being provided on dedicated lanes on Parramatta Road.

- iii. *Planning Priority 13.7:* Collaborate with Parramatta Road Corridor councils to ensure planning for Parramatta Road is integrated across LGA boundaries.
- iv. *Planning Priority 13.7:* Prepare Parramatta Road Corridor local contributions plan to address funding of local infrastructure and services in the Corridor.
- v. *Planning Priority 13.9:* Seek a variation under the Section 9.1 Direction for the *Parramatta Road Corridor Urban Transformation Strategy* to retain the existing industrial land within the corridor and undertake further investigations to identify any additional variations required for the existing employment lands within the corridor.

As detailed previously, this Planning Proposal's provisions are underpinned by the above-mentioned LSPS Planning Priorities alongside its generic priorities.

The LSPS also identifies inconsistencies in the PRCUTS projected dwelling and job numbers. In response, the Council and DPE re-analysed growth projections and further investigations were undertaken during preparation of the *Inner West LHS* and *Parramatta Road Corridor Precinct-wide Traffic and Transport Study*.

This Planning Proposal represents a place-based approach through urban design analysis to identify locations for new residential and employment opportunities while delivering best practice urban design and infrastructure outcomes. Subject to the provision of mass-transit along Parramatta Road, the Planning Proposal will increase residential and commercial floor space aligned to the LSPS and DPEs targets across the Inner West in the short to medium term.

**7. Inner West Local Housing Strategy**

The LHS was adopted by Council in March 2020. It was endorsed by DPE on 8 July 2021, subject to a number of conditions as discussed below.

The LHS anticipates a total of 2,204 dwellings across the Leichhardt, Taverners Hill and Kings Bay/ Croydon precincts. This Planning Proposal anticipates 1516 dwellings in this first stage of implementation, within parts of the precincts. Stage 2 will provide for additional dwellings to meet or exceed the LHS projections.

*Table 19 Consistency with relevant LHS principles*

LHS Principle	Consideration
1. Ensure the cultural significance of landscapes, sites, waterways, customs and traditions that Aboriginal communities wish to conserve are protected and maintained in housing development.	Protection of riparian areas, new links to waterways, and tree canopy targets inform the supporting draft DCPs.
2. Accommodate housing growth through a range of sensitive infill compatible with heritage values and local character – enabling areas to evolve with respect over time.	The urban design study takes into account the heritage values and local character of the areas. The supporting draft DCP amendments outline the desired future character for these areas.

LHS Principle	Consideration
3. Provide for a diverse mix of housing typologies, sizes and tenures that cater to the needs of people at all stages of their lives.	The urban design study recommends a range of building typologies which will encourage diversity in dwelling types.
4. Start to close the gap between housing need and provision for very low, low and moderate income households.	The Planning Proposal includes provisions to implement an Affordable Housing Contributions Scheme under the Housing SEPP.
5. Locate the majority of new housing opportunities in areas that are within a 10-minute walk of centres, transport and services, supporting their vibrancy and aligning with infrastructure provision and growth.	The proposed new housing is within a 10-minute walk of (current or future) rail or light rail stations or a centre well serviced by transport services.
6. Design quality housing to maximise amenity, safety and security for residents and provide a positive contribution to its neighbourhood.	Amenity and character were thoroughly considered as part of the Urban Design Study. Consequently, changes have been recommended through this Planning Proposal to PRCUTS recommendations for density, building height, setbacks and built form. Whilst this Planning Proposal is inconsistent with PRCUTS in places, it evidently brings forward better design recommendations which will provide positive contributions to neighbourhoods in recognition of this LHS principle.
7. Homes are designed to be environmentally sustainable, supporting Council's aim of zero net carbon emissions by 2050, water sensitivity, increasing biodiversity and zero waste.	The Planning Proposal intends to achieve improved sustainability outcomes through high performance buildings, reduced car parking and green infrastructure provision.  Council's proposed DCP amendments include provisions to improve water management, increase biodiversity and work towards zero waste in the Inner West.

The LHS's precinct by precinct findings for Parramatta Road Corridor are provided as follows:

- **Leichhardt Precinct:**

LHS Strategy identifies the potential for an additional 1,629 dwellings in the Leichhardt Precinct, being the core and frame areas under PRCUTS. Dwelling types are anticipated as shop-top housing or residential flat buildings. Through place-based investigation, this Planning Proposal has identified sites that can intensify residential development by approximately 707 new dwellings in this first stage of the implementation of PRCUTS.

In addition, the Leichhardt Precinct provides opportunity for the delivery of affordable housing through the Housing SEPP as detailed in the draft Inner West Affordable Housing Contributions Scheme (Appendix 8).

Consideration of specific LHS actions relating to Leichhardt precinct is discussed in the following table.

Table 20 Consistency with relevant LHS Leichhardt Actions

Key LHS action	Consideration
Prepare a place-based study to test and confirm potential additional yield analysis.	Completed. Refer to Appendix 2.
Continue to prepare the relevant studies to support a Planning Proposal for the locality including traffic and transport modelling and a social infrastructure assessment.	Completed. Refer to <b>Error! Reference source not found.</b> In addition, Council has recently completed its social infrastructure review for the whole of the local government area which has been used to inform the Inner West Local Infrastructure Contributions Plan 2023.
Include the key dependency of improved mass transit on Parramatta Road (committed under Future Transport 2056 in the 0–10-year timeframe).	This formed part of Council's original proposal which was submitted to DPE for Gateway Determination. However, Gateway determination condition 1(q) required the Planning Proposal to delete the two proposed transport infrastructure provisions in relation to NSW Government commitment to introduce an on-street rapid system and future transport infrastructure. Whilst these provisions have been deleted from the proposal, Council will continue to advocate NSW Government to provide improved mass transit along Parramatta Road as per this Action.
Develop a local contributions plan and incorporate the precinct in an Affordable Housing Target Scheme as required by SEPP 70 (now Housing SEPP).	Completed. Refer to Appendix 8.
Assess any Council-owned land that may be suitable affordable housing donor locations to potentially increase the contribution versus dwelling yield outcome in the Affordable Housing Target Scheme.	This Planning Proposal does not include any Council owned sites which have the potential to contribute towards additional affordable housing. Council owed sites will be investigated in Stage 2 of PRCUTS implementation.
Assess areas or sites that require heritage protection whilst allowing growth to occur.	Completed. Refer to Appendix 3.
Assess the impact of noise attenuation requirements in ANEF 20 to 25 affected	Completed. Refer to Appendix 7.

Key LHS action	Consideration
land in relation to internal amenity, environmental performance of buildings and architectural outcome to determine whether residential intensification in this location is acceptable.	

• **Taverners Hill Precinct:**

The LHS reinforces Council's aim to protect employment land in the Taverners Hill Precinct. This Planning Proposal excludes industrial and employment lands as discussed previously. It is focused around specific residential areas of the Precinct and provides opportunities for an additional 393 dwellings of the 456 identified in the Local Housing Strategy. The predominant building typology proposed is low to medium-rise residential flat buildings.

Consideration of specific LHS actions relating to Taverners Hill precinct is discussed in the below table.

Table 21 Consistency with relevant LHS Taverners Hill Actions

Key LHS action	Consideration
Prepare a place-based study to test and confirm potential additional yield analysis.	Completed. Refer to Appendix 2.
Continue to prepare the relevant studies to support a Planning Proposal for the locality including traffic and transport modelling and a social infrastructure assessment.	Completed. Refer to <b>Error! Reference source not found.</b> 10. In addition, Council has recently completed its social infrastructure review for the whole of the local government area which has been used to inform the Inner West Local Infrastructure Contributions Plan 2023.
Include the key dependency of improved mass transit on Parramatta Road (committed under Future Transport 2056 in the 0–10-year timeframe).	This formed part of Council's original proposal which was submitted to DPE for Gateway determination. However, Gateway determination condition 1(q) required the Planning Proposal to delete the two proposed transport infrastructure provisions in relation to NSW Government commitment to introduce an on-street rapid system and future transport infrastructure.  Whilst these provisions have been deleted from the proposal, Council will continue to advocate NSW Government to provide improved mass transit along Parramatta Road as per this Action.

Key LHS action	Consideration
Develop a local contributions plan and incorporate the precinct in an Affordable Housing Target Scheme as required by SEPP 70).	Completed. Refer to Appendix 8. DPE's viability tool indicates that AHCS will not be feasible for the proposed uplift in Taverners Hill precinct. Substantially higher FSRs and building heights are required to facilitate affordable housing in this area. This is primarily due to land values in the area and costs of potential amalgamations.
Assess areas or sites that require heritage protection whilst allowing growth to occur.	Completed. Refer to Appendix 3.

• **Kings Bay/ Croydon/ Croydon Precinct:**

The LHS proposes that the low-density residential area along Dalmar Street be rezoned to R3 Medium Density Residential. This Planning Proposal supports this outcome.

Urban design testing also recommends this approach to provide an appropriate transition to the proposed built form on the E3 Productivity Support land to the north, fronting Parramatta Road. This change, along with the addition of shop top housing on two Opportunity Sites, in response to the proposed Sydney Metro West station in Five Dock, will provide approximately 416 new dwellings in the Inner West part of Kings Bay/ Croydon. LHS had envisaged a lower yield at about 56 dwellings.

Council's Urban Design Study demonstrates that Kings Bay/ Croydon has potential to accommodate more growth in the existing residential area alongside the proposed Opportunity Sites. This is generally considered to be a good outcome as it is aligned with committed public transport infrastructure, with Five Dock Metro Station to be within an 800m walking catchment of this Precinct.

Consideration of specific LHS actions relating to Kings Bay/ Croydon/ Croydon precinct is discussed in the below table.

Table 22 Consistency with relevant LHS Kings Bay/ Croydon/ Croydon Actions

Key LHS action	Consideration
Prepare a place-based study to test and confirm potential additional yield analysis.	Completed. Refer to Appendix 2.
Work with Canada Bay Council to prepare the relevant studies to support a Planning Proposal for the locality including traffic and transport modelling and a social infrastructure assessment.	Both Canada Bay Council Traffic and Transport Study for Kings Bay Precinct 2022 and Kings Bay Precinct Inner West Council Traffic Impact Assessment 2022 ( <b>Error! Reference source not found.</b> 10) and associated outcomes have informed this Planning Proposal.

Key LHS action	Consideration
	<p>In addition, Council has recently completed its social infrastructure review for the whole of the local government area which has been used to inform the Inner West Local Infrastructure Contributions Plan 2023.</p>
<p>Include the key dependency of improved mass transit on Parramatta Road (committed under Future Transport 2056 in the 0–10-year timeframe).</p>	<p>This formed part of Council's original proposal which was submitted to DPE for Gateway determination. However, Gateway determination condition 1(q) required the Planning Proposal to delete the two proposed transport infrastructure provisions in relation to NSW Government commitment to introduce an on-street rapid system and future transport infrastructure.</p> <p>Whilst these provisions have been deleted from the proposal, Council will continue to advocate NSW Government to provide improved mass transit along Parramatta Road as per this Action.</p>

In addition to the matters addressed in the LHS, DPE's letter of endorsement included the following conditions that are relevant to this Planning Proposal:

- Condition 1:** Within four (4) months of Council being notified of the LHS approval, Council is to prepare an updated and prioritised Implementation and Delivery Plan that clearly articulates the actions, roles and responsibilities, sequencing and timing to facilitate housing supply, diversity and affordability between 2022 and 2026 and beyond. The Plan should be prepared in consultation with DPIE, TfNSW and Infrastructure NSW, to ensure any critical interdependencies are satisfactorily resolved.

**Council's consideration:** Council submitted its LHS Implementation Plan to DPE in December 2021 which addresses the above condition. This action has now been completed and requires Council to progress this Planning Proposal to address short to medium term projected housing shortfall in the Inner West.

- Condition 2:** Council is to submit to DPIE for Gateway determination its planning proposal for the areas of the PRCUTS (Leichhardt, Taverners Hill, and Part of Kings Bay/ Croydon) 3 months (excluding the Christmas and New Year period) from the time when there is finalisation and provision of the results of the Department's associated transport modelling to Council. This planning proposal is to be submitted to the Department for finalisation 6 months after Gateway Determination is issued. This approach is necessary for council to achieve its 6–10-year housing targets.

**Council's consideration:** Council is committed to pursuing this Planning Proposal for areas of the PRCUTS (Leichhardt, Taverners Hill, and Part of Kings Bay/ Croydon) to address the 6–10-year dwelling shortfall identified by DPE. Due to circumstances beyond Council's control and delays associated with the completion of supporting technical studies to address the Gateway conditions, the Planning Proposal has been delayed to be publicly

exhibited for community feedback. A new timeline to proceed with this Planning Proposal is outlined in Part 6 below.

3. **Condition 7:** Council is to prepare an Affordable Housing Contributions Scheme that commits Council to examining the feasibility of levying affordable housing contributions for any new planning proposals that would result in development uplift or an increase in land value. This will ensure that planning proposals give effect to the District Plan, Action 17 to prepare an Affordable Housing Contributions Scheme.

**Council's consideration:** This Planning Proposal includes provisions to implement the draft AHCS in the Leichhardt precinct under the Housing SEPP. Site-specific testing of Kings Bay/ Croydon and Taverners Hill precincts using DPE's Affordable Housing Viability Tool indicates that AHCS would not be financially viable for these precincts.

4. **Condition 8:** Council's LHS is identified as being inconsistent with PRCUTS as it proposes an alternative approach to industrial land identified for land use change. To support a strategic led approach under the Eastern City District Plan and the PRCUTS, land use change for industrial land identified in the PRCUTS is to be implemented and is to prevail to the extent of any inconsistency with the approach to industrial land identified in Council's Local Housing Strategy.

**Council's consideration:** This Planning Proposal seeks staged implementation of the PRCUTS. Stage 1 Implementation Area as detailed in this Planning Proposal does not include industrial zoned land. This will be dealt through future planning proposal in these precincts.

5. **Condition 9:** The Section 9.1 Local Planning Direction – 1.5 Parramatta Road Corridor Urban Transformation Strategy (as amended and current) is to prevail to the extent of any inconsistency with the approach identified in relation to PRCUTS within Council's Local Housing Strategy.

**Council's consideration:** This Planning Proposal is fully consistent with the Section 9.1 Local Planning Direction – 1.5 Parramatta Road Corridor Urban Transformation Strategy as also discussed in the following section.

## 8. Employment and Retail Lands Strategy

The Inner West *Employment and Retail Lands Strategy* (EaRLS), came into effect in September 2020. It aims to facilitate the management of Inner West employment lands and commercial centres, prioritising actions for productive commercial and industrial land uses to facilitate job growth and thriving economy. The EaRLS is yet to be adopted by DPE.

EaRLS sets four principles to guide planning for the future of the Inner West employment and retail lands.

- i. *Principle 1:* Centres are distinctive and productive: Development in centres will prioritise employment and will be complemented by quality public space.
- ii. *Principle 2:* Industrial and urban services lands are protected and managed: Employment lands will be retained and managed so that industries have confidence to locate and expand.
- iii. *Principle 3:* Spaces for business are suitable and viable: A pipeline of new, well-located suitable employment floor space will be delivered in employment corridors and key precincts.

- iv. *Principle 4:* The planning framework is clear: The planning framework supports local business and minimises land use conflict.

The Planning Proposal is consistent with these principles, specifically in Leichhardt local centre and along areas of Parramatta Road included in this Planning Proposal.

EaRLS recommends several specific actions relevant to the PRCUTS area as part of this Planning Proposal. The following table demonstrates how the Planning Proposal is consistent with these actions.

Table 23 Consistency with relevant EaRLS Actions

EaRLS Action	EaRLS Recommendations	Study Response
<b>Leichhardt Precinct</b>		
<p><b>Action 1.2.2:</b> Protect and increase non-residential floor space in centres by implementing minimum non-residential FSR provisions and initiating planning responses prescribed in Table 5.</p>	<p>Establish a minimum FSR control for non-residential uses, to require ground floor retail and first floor business and office space in Norton Street. The former Leichhardt Council urban design and heritage studies provide a starting point for developing LEP and DCP controls.</p>	<p>Establishment of minimum non-residential FSRs were investigated as part of the Economic Assessment (Appendix 4) undertaken by SGS. This study recommends not to proceed with minimum non-residential FSRs for the Leichhardt precinct as there appears to be limited market demand for large first floor commercial spaces.</p>
<p><b>Action 3.1.5:</b> Explore options with TfNSW to improve pedestrian and business amenity along Parramatta Road east of Taverners Hill Light Rail stop, including reducing the speed limit and traffic calming.</p>	<p>Council should explore options with TfNSW to increase pedestrian and business amenity along the corridor. This may include reducing the speed limit on Parramatta Road and introduce calming measures to improve amenity and vehicular access to businesses along Norton Street and Parramatta Road.</p>	<p>This will be pursued through the Implementation Plan set out in the <i>Appendix 10 – Parramatta Road Corridor Precinct-wide Traffic and Transport Study and its Implementation (Camperdown, Leichhardt, Taverners Hill and Kings Bay/ Croydon precincts)</i>.</p>
<p><b>Strategy 1.7: Establish a targeted planning response to support employment growth in key centres</b></p> <p><b>Action 1.7.3:</b> Prepare a place-based study to review planning controls for the overall Leichhardt Precinct in the context of studies</p>	<p>Review planning controls for the overall Leichhardt Precinct in the context of studies undertaken (PRCUTS and Former Leichhardt Urban Design and Heritage studies, 2016) with the aim of delivering positive urban design outcomes for the centre.</p>	<p>The place-based Urban Design Studies (Appendix 2) undertaken by Architectus were used to inform the proposed planning controls for Leichhardt precinct.</p> <p>As discussed above, non-residential requirements were investigated as part of</p>

EaRLS Action	EaRLS Study Recommendations	Response
<p>undertaken for PRCUTS, with the aim of delivering positive urban design outcomes for the centre. Undertake a feasibility study that determines the tipping point that makes a mixed-use development viable.</p> <p><b>Action 1.7.6:</b> Review planning controls, including zoning along Norton Street north of Allen Street to enable evolution of the area into an office/professional service, mixed use precinct with improved development and urban design outcomes.</p>	<p>Undertake a feasibility study that determines the tipping point that makes a mixed-use development viable.</p> <p>Review built form outcomes resulting from development approvals along Norton Street and Parramatta Road to understand issues that need to be overcome when developing new LEP and DCP provisions for the Leichhardt precinct.</p> <p>Review the Leichhardt DCP Precinct Parking Strategy in relation to Norton Street Core to identify options for reducing or eliminating off-street parking requirements and delivery arrangements for properties without secondary vehicular access.</p>	<p>the Economic Assessment (Appendix 4).</p> <p>Recent development approvals along Norton Street and Parramatta Road were reviewed as part of the SGS's Study.</p> <p>The Planning Proposal and supporting draft DCPs include provisions to reduce parking and minimise traffic generation and vehicular crossovers on Parramatta Road and Norton Street.</p> <p>This Planning Proposal seeks to implement the findings of these studies, satisfying the requirement of this action and demonstrating consistency with EaRLS. This follow-on work associated with urban design and feasibility testing has resulted in refinements of PRCUTS recommendations to deliver better outcomes through this Planning Proposal.</p>
<p><b>Action 2.21:</b> In the context of the capacity assessment identified in the Employment and Retail Lands supporting study, adopt the policy position to – use the outcomes of EaRLS to demonstrate that retention of industrial land within the Parramatta Road Corridor will deliver better outcomes than those recommended in the PRCUTS and PRCUTS Implementation Strategy (2016).</p>	<p>PRCUTS recommended planning controls which would result in loss of flexible and affordable employment floorspace are not supported. Consider adoption of the above alternate scenario with the goal of retaining industrial and high order business zones to encourage employment, lower-value retail and urban services over higher-value centre uses.</p>	<p>This Planning Proposal excludes existing industrial/employment lands in the Parramatta Road Corridor.</p> <p>At the time of preparation of this proposal, DPE was reviewing all employment zones which have now come into effect since April 2023. Future planning proposals in PRCUTS area will review these lands to address this principle.</p>
<p><b>Strategy 1.8:</b> Support a vibrant night-time economy.</p>	<p>Support the growth of the nighttime economy of Norton Street by:</p>	<p>Council is pursuing this through a separate planning</p>

EaRLS Action	EaRLS Study Recommendations	Response
	<ul style="list-style-type: none"> <li>- Reviewing planning controls to enable late night trading and encouraging the establishment of small bars, restaurants, and entertainment uses.</li> <li>- Encourage the growth of markets and street festivals that bring people into the area.</li> <li>- Actively marketing and promoting the centre as places to establish businesses.</li> </ul>	proposal for 'Open and Creative Inner West'.
<p><b>Action 3.1.7:</b> Develop an innovative, planning response for the Leichhardt and Camperdown Frame areas to support renewal of employment floor space and achieve positive urban design outcomes. Refer to Section 9.3.3.3 and 9.3.4.3 of the Study for detail. Carefully consider the impact of redevelopment on floor space affordability for businesses, especially in the creative and wedding services sectors.</p>	<p>Revisions to controls along the Leichhardt Frame Area Precinct should carefully consider:</p> <ul style="list-style-type: none"> <li>- Built form requirements such as doorway widths, loading, parking and minimum floor to ceiling heights for ground floor tenancies following consultation with industry</li> <li>- The impact of redevelopment on rental affordability for business</li> <li>- The quantity and usability of the employment floorspace likely to result from future redevelopment.</li> </ul>	Most of the Leichhardt Frame Area is excluded from this Planning Proposal and will become part of a future LEP amendment.
<b>Taverners Hill Precinct</b>		
N/A	N/A	N/A - This Planning Proposal excludes the employment areas in Taverners Hill and only deals with Residential zones.
<b>Kings Bay/ Croydon</b>		
<p><b>Action 3.1.6:</b> Undertake detailed precinct planning for Kings Bay/ Croydon and Taverners Hill to improve urban design outcomes and resolve site-</p>	<p>Prior to any changes to planning controls, undertake coordinated and detailed precinct planning for Kings Bay/ Croydon to resolve site specific constraints which</p>	Coordinated and detailed precinct planning for Kings Bay/ Croydon was undertaken as part of the urban design studies in Appendix 2.

EaRLS Action	EaRLS Recommendations Study	Response
specific/feasibility constraints to support redevelopment for employment uses.	may limit uptake of higher floorspace controls. This may include: <ul style="list-style-type: none"> <li>- Reviewing height and FSR controls, green edge setbacks and lot consolidation required for redevelopment</li> <li>- Engaging with landowners, Burwood Council and State agencies to arrange for side and rear access to reduce vehicle crossovers on Parramatta Road</li> <li>- Considering expansion of the E3 Productivity Support zone in consultation with affected landowners</li> <li>- Undertaking feasibility testing, to determine the viability of redevelopment for employment uses.</li> </ul>	Two opportunity sites to the east of the Kings Bay/ Croydon frame area that are suitable to provide residential uses due to their strategic location in proximity to the future Five Dock Metro station which is within 800m walking catchment.  The Planning Proposal subsequently includes provisions to retain the E3 zone and provide ground floor uses on these two sites, with residential flat buildings as an additional permitted use on the upper levels.  Specific recommendations for the E3 Productivity Support lands were also investigated as part of the Economic Assessment (Appendix 4) undertaken by SGS Economics and Planning.  EaRLS Study recommendations regarding engagement with adjacent councils and landowners will be undertaken as part of the statutory LEP consultation process.

Furthermore, Council commissioned an economic feasibility study. Key actions resulting from the study are discussed below.

**Parramatta Road/ Norton Street Economic Testing**

SGS Economics and Planning and Savills were appointed by Inner West Council to analyse the financial feasibility of development along Parramatta Road/ Norton Street in Leichhardt precinct and provide recommendations to incorporate minimum non-residential uses in future developments. The scope of this project included:

- i. An explanation of the existing market profile, supply and demand for land uses in the Leichhardt Precinct and along Parramatta Road
- ii. Determining project internal rate of return (IRR) with base planning controls that currently apply as well as proposed planning controls

- iii. Determine whether increasing the FSR control along Parramatta Road can deliver a feasible IRR, and if so, what is the FSR tipping point to enable this
- iv. Determine the tipping point for financial feasibility in Leichhardt as the mix between residential and non-residential floorspace is changed
- v. Consider varying assumptions to the feasibility modelling such as parking, access, costs, sustainability requirements and the need to amalgamate sites
- vi. Provide best practice examples of planning mechanisms to encourage site amalgamation as part of mixed-use development, and recommend planning controls to encourage site amalgamation
- vii. Make recommendations on the design of mixed-use development and stratum titling to ensure floor space is suitable to accommodate non-residential uses that are in highest demand in Leichhardt

Savills undertook feasibility modelling with Estate Master using high level assumptions with the aim of informing strategic planning for Parramatta Road. SGS/Savills market analysis and feasibility methodology is detailed in the Appendix 4.

Table 24 – Key recommendations from the Parramatta Road/ Norton Street economic assessment study

Study recommendation	Study rationale	Planning consideration	Proposal
Active uses on the ground floor be required along the length of Norton Street.	<p>Non-residential FSRs between 0.6:1 and 0.7:1 are generally feasible in the Leichhardt precinct where total FSRs are proposed at 1.9:1 and 3:1, respectively.</p> <p>This was confirmed through modelling 'tipping point' analyses to determine development feasibility for a number of test sites throughout the Leichhardt precinct.</p> <p>Depending on the site size, a non-residential FSR range between 0.3:1 and 0.5:1 can be secured on the ground floor.</p>	Active street frontages are required as per the active frontages map at Appendix 1. These provisions are supported by proposed controls in the draft DCPs.	
Stratum subdivision to separate ground non-residential floor space from residential floor space.	<p>Commercial tenants and owners do not want heavy involvement in strata committees, and disputes can arise from commercial and residential owners due to the variety of possible land uses. This can restrict the range of possible uses.</p> <p>Stratum subdivision can limit potential for disagreements.</p>	Supporting draft DCP provisions require stratum subdivision to separate ground non-residential floor space from residential floor space.	

Study recommendation	Study rationale	Planning consideration	Proposal
	<p>Separate strata committees are established for residential and non-residential portions of a building. Stratum subdivision is likely to be most appropriate where there are multiple employment generating premises within the same complex.</p>		
<p>Specific design controls limiting poor design outcomes on narrow and small sites.</p>	<p>Identify poor design outcomes on narrow or small sites and create design controls in the DCP to prevent these outcomes.</p> <p>These controls could be outcome based e.g., basement entrances should not dominate the street-front. Or specify quantified standards e.g., a maximum proportion of the street frontage can be occupied by a basement entrance.</p>	<p>These have been included in the accompanying draft DCP amendments.</p> <p>Additional local provisions are also proposed to ensure developments relying on incentives achieve an appropriate development pattern and high-quality built form in terms of bulk, massing, height, separation, setbacks, amenity and modulation.</p>	
<p>Implement a sliding scale FSR to encourage site amalgamation.</p>	<p>For multi-storey apartment development, larger sites are more likely to result in better urban design outcomes. As it allows greater design flexibility and facilitates basement entrances that do not overwhelm a frontage.</p> <p>Implementing a sliding scale FSR that would allow larger FSRs on larger sites will incentivise site amalgamation, possibly leading to better design outcomes (especially if minimum onsite car parking is pursued for narrow sites).</p>	<p>Site amalgamation is required by the Planning Proposal provisions and supporting draft DCP amendments.</p> <p>A sliding scale FSR was not selected as height and FSR incentives have been utilised to achieve several other positive design outcomes. Conflating these with a sliding scale provision tied to site width or size would be overly complicated.</p>	
<p>Service the Leichhardt Precinct with mass transit.</p>	<p>Ashfield, Burwood and Strathfield are the larger</p>	<p>Gateway determination condition 1(q) required the</p>	

Study recommendation	Study rationale	Planning Proposal consideration
	<p>Inner West office/commercial locations. They have a greater concentration of retail and services than Leichhardt and so remain more competitive as a business location.</p> <p>With mass transit Leichhardt would have a clear advantage as a location for population serving businesses over other nearby areas.</p> <p>Transit would need to be significant enough to enough to increase competitiveness against other nearby centres with heavy rail.</p>	<p>Planning Proposal to delete the two proposed transport infrastructure provisions in relation to NSW Government commitment to introduce an on-street rapid system and future transport infrastructure.</p>
<p>Lower car parking requirements and encourage decoupled car parking.</p>	<p>Leichhardt DCP 2013 currently requires one space per one bedroom unit, two per two and above bedroom units.</p> <p>Narrow sites make basement car parks difficult. Facilitating development without basement car parking is critical for multi-storey residential apartment development to occur on Norton Street and Parramatta Road.</p> <p>Lowering the DCP requirements for narrow sites and providing long term car parking in nearby facilities will address this.</p>	<p>Car parking requirements are proposed at a maximum rate and below the current minimum rate required under the Leichhardt DCP 2013.</p> <p>This will permit narrow and smaller sites where basement parking is not feasible to not provide car parking.</p> <p>Further details are provided under the car parking provisions above in section 2.9.</p>
<p>Facilitate through site links by:</p> <ul style="list-style-type: none"> <li>• Allowing additional height and floor space</li> </ul>	<p>Providing an open to the sky through site link across private land heavily impacts on the feasibility of development.</p>	<p>The Planning Proposal requires a through-site link between Balmain Road and Norton Street through the Norton Plaza Opportunity</p>

Study recommendation	Study rationale	Planning consideration
<p>when TSLs are provided on site</p> <ul style="list-style-type: none"> <li>Design controls in the DCP</li> </ul>	<p>To maintain feasibility and incentivise through site links where desired, additional HOB and FSR allowances should be allocated to sites through an additional local provision in the LEP when through site links are provided.</p> <p>Design controls should also be contained within the DCP to ensure the links are designed in a manner that is satisfactory to Council and provides a high-quality urban design outcome.</p>	<p>site. Supporting draft DCP also requires the below desired through-site links:</p> <ul style="list-style-type: none"> <li>East-West link along Dot Lane and its extension from Balmain Road to Norton Street (to the south of Italian forum)</li> <li>East-West link as extension of McDonald Street between Balmain Road and Norton Street (to the south of Leichhardt Public School)</li> </ul> <p>FSR and HOB incentives are proposed to facilitate this link in line with the urban design study recommendations.</p> <p>Further details are provided under Opportunity sites in section 2.11 of this Planning Proposal.</p> <p>Design controls for the proposed though-site link are provided in the supporting DCP.</p>

Economic assessment was not required for Taverners Hill as this Planning Proposal only deals with residential zoned land in Taverners Hill Precinct.

Council also got expert economic advice from SGS (Appendix 4) for the two Opportunity sites in Kings Bay/ Croydon/ Croydon Precinct where residential uses are being introduced to the existing E3 Productivity Support zoned sites. This report considered the impacts of adding residential uses on these sites and provided design guidance to manage urban hazards. These recommendations are discussed in the below table.

Table 25 – Key recommendations from Kings Bay/ Croydon/ Croydon Opportunity Sites report

Study recommendation	Study rationale	Council response
<b>Kings Bay/ Croydon/ Croydon Opportunity Sites</b>		
<p>Non-residential FSR of 1:1 be required for opportunity sites, with the remaining 1.4:1 FSR being residential.</p>	<p>Requiring 1:1 of the FSR be for non-residential uses is generally considered to be feasible. This would deliver a notable increase in the employment capacity of the sites.</p>	<p>Retaining employment floorspace on the ground floor is included in this Planning Proposal. This is in response to SGS's advice that there is excess capacity across business and enterprise sites. Retaining employment opportunities on</p>

	<p>Currently there is excess capacity across business and enterprise sites, however it remains largely unfeasible for redevelopment to generate additional employment floorspace.</p> <p>The inclusion of mixed-use zoning with residential above will trigger redevelopment, leading to an increase in employment floorspace.</p>	<p>the whole of the ground floor would ensure sufficient employment capacity into the future.</p> <p>Setting a FSR minimum for non-residential floorspace is not proposed.</p>
<p>Urban hazards from Parramatta Road on residential development are minimised.</p>	<p>Parramatta Road has low amenity for residential apartments due to high levels of noise and pollution.</p> <p>It's recommended that separation from Parramatta Road is the most appropriate way to mitigate this problem.</p> <p>This could either be achieved through horizontal separation (setting back) of residential floorspace or vertical separation. Double height ground floor spaces could achieve this.</p>	<p>Urban hazards including noise and pollution will be dealt through the proposed controls in this Planning Proposal and supporting DCP, as well as the requirements under the SEPP (Transport and Infrastructure) 2021.</p>
<p>Ensure ground floor employment space are flexible, have double height ceilings, are suitable for a range of uses, have large floorplates and adequate vehicle access.</p>	<p>Such provisions are required to facilitate showroom or light urban services uses. Small loading docks would likely be required, with vehicle access provided from a lane or side street off Parramatta Road.</p>	<p>Supporting draft DCP controls are proposed to ensure that this recommendation is achieved.</p>
<p>Design requirements for through-site links.</p>	<p>Public benefits should be secured to make surrounding public domain compatible with the scale of development.</p> <p>Provision of through site links are generally considered to be feasible.</p>	<p>Requirements for desired through-site links are proposed in the draft DCP controls such as:</p> <ul style="list-style-type: none"> <li>- North-South link between Parramatta Road and Dalmar Street as an extension of Hammond Avenue</li> </ul>

- North – South link between Parramatta Road and Dalmar Street as extension of Burn Street.

These links are required to increase permeability of the precinct and break down large blocks which currently have no mid-block crossings.

### 9. Going Places: Integrated Transport Strategy

Inner West Council's Integrated Transport Strategy (ITS), *Our Place Inner West Going Places*, was adopted in March 2020. It builds on the plans, studies and projects of the three former Councils (Leichhardt, Ashfield and Marrickville), NSW Government land use plans and transport strategies and Council's CSP to identify transport needs, opportunities and projects for the future.

ITS outlines seven principles which aim to address the transport challenges facing the Inner West as it moves towards a transport future focussed on active and sustainable modes of transport.

- *Principle 1:* Plan land use to support active and sustainable transport for reduced travel times and distances.
- *Principle 2:* Improve safety, personal security, and provide equitable access for full community participation.
- *Principle 3:* Prioritise people in centres and main streets and revitalise key roads.
- *Principle 4:* Commit to active transport infrastructure, services and programs.
- *Principle 5:* Encourage shift to public transport and shared transport from private vehicles by providing attractive alternatives and reduce the impact of congestion and parking.
- *Principle 6:* Manage a freight and goods delivery network to enhance efficiency and Inner West liveability.
- *Principle 7:* Harness technology to improve information, safety, travel choices and environmental outcomes.

This Planning Proposal responds to the ITS and seeks to implement a number of key actions including:

- Revise the planning instruments to require major new developments to provide through-site links for public access on foot and bicycle.
- Review parking requirements in areas within close proximity to high frequency public transport.
- Develop a Section 7.11/7.12 Development Contributions Plan to support more active transport links.
- Ensure minimum bicycle parking requirements are contained within the consolidated DCP for residential and employment uses.
- Inclusion of controls to require 'end of trip' facilities in commercial and industrial developments of a certain size in the supporting DCP.
- Investigate opportunities to encourage and provide de-coupled parking to support new developments.

- Require major new developments to include electric vehicle charging facilities.

**10. Council's Affordable Housing Policy**

A revised Affordable Housing Policy was adopted by Council on 10 May 2022. This policy sets out requirements for Affordable housing in the Inner West. It outlines that *'planning proposals and development applications that fall within precincts identified by an AHCS must deliver affordable housing in accordance with the affordable housing contribution rate prescribed by that AHCS / Inner West Local Environmental Plan.'*

*The feasibility of imposing affordable housing contributions must be investigated in all planning proposals and rezoning requests that are located in areas not covered by an existing AHCS. Where feasibility can be demonstrated, an AHCS must form part of any planning proposal submission. The scheme must be prepared in accordance with the NSW Government's Guideline for Developing an Affordable Housing Contribution Scheme.'*

This Planning Proposal seeks to introduce an AHCS in the Leichhardt Precinct and is supported by feasibility analysis using DPE's viability tool (Refer to Appendix 8). This tool demonstrates that a contribution of 2.2% is viable for affordable housing contributions in Leichhardt precinct and can be provided as per below:

- on-site: 2% of the residential strata area of the development must be provided as an affordable housing contribution
- monetary: 2% of the residential strata area of the development must be provided as an affordable housing contribution
- combined: 2% of the residential strata area of the development must be provided as an affordable housing contribution.

A development may provide affordable housing on-site, in another location within the Inner West Council LGA, or pay an equivalent monetary contribution to allow housing units to be built or purchased elsewhere in the Inner West LGA. Council prefers that contributions are provided in the form of dwellings within the development to which the contribution applies.

This tool when tested for sites in Taverners Hill and Kings Bay/ Croydon Precinct indicated that it would not be financially viable to impose affordable housing contributions in these Precincts.

**Q5. Is the planning proposal consistent with any other applicable State and regional studies or strategies?**

**Future Transport Strategy 2056**

Transport for NSW's Future Transport Strategy 2056 provides strategic directions for future planning, investment, delivery and operations.

This Planning Proposal complements the strategic directions relating to improved connectivity, accessibility, reduced environmental impact and supporting growth through smarter planning. Below specific priorities of the Strategic Direction *P1 Supporting growth through smarter planning* will be implemented through this Planning Proposal:

- Support growth around public transport
- Ensure public transport is available on day one
- Improve parking provision and management

There are no specific actions in the Future Transport Strategy 2056 relating to Parramatta Road. There is some reference of rapid bus network as Non-operational infrastructure and services including committed, funded and visionary projects.

This Planning Proposal fully supports and implements PRCUTS vision of a revitalised Parramatta Road Corridor, which is dependent on the provision of improved public transport (on-street rapid transit system) and reduced reliance on private cars.

**Public Open Space Strategy for NSW**

NSW Government’s Public Open Space Strategy 2022 provides a framework for implementing policies across Government and contribute to a more joined-up approach to public open space planning and delivery.

The Planning Proposal seeks to create new public open spaces in the Leichhardt precinct:

- 2 Hay Street Leichhardt – existing car park which is recommended to be rezoned to RE1 Recreational Open Space.
- Norton Plaza – new civic open space/ plaza fronting Norton Street proposed as part of redevelopment of the opportunity site.

The Planning Proposal also includes provisions to create new through-site links which will enhance access to public spaces. These measures would contribute towards achieving the objectives in the NSW Government’s Public Open Space Strategy.

Further, Council’s Recreation Needs Study has identified gaps in the existing open space and recreational infrastructure and any opportunities for improvements. This study informed the development of Council’s Local Infrastructure Contributions Plan that came into effect on 20 February 2023 and will fund the recreational infrastructure in the area.

**Net Zero Plan**

NSW Government’s Net Zero Plan Stage 1: 2020-2030 is the foundation for NSW’s action on climate change and goal to reach net zero emissions by 2050. It outlines the NSW Government’s approach to protect our future by growing the economy, creating jobs and reducing emissions over the next decade. The plan aims to strengthen the prosperity and quality of life of the people of NSW, while helping to achieve the State’s objective to deliver a 70% cut in emissions by 2035 compared to 2005 levels.

The plan supports a range of initiatives targeting energy, electric vehicles, hydrogen, primary industries, technology, built environment, carbon financing and organic waste.

Council is committed to becoming an ecologically sustainable Inner West. Council’s Community Strategic Plan 2022 includes a strategic direction that commits to becoming zero emissions, climate adapted and resilient Inner West by 2036. This aspiration aligns with the priorities of the NSW Government’s Net Zero Plan Stage 1: 2020-2030.

This Planning Proposal seeks to support net zero targets by setting increased energy and water performance targets for developments that seek to utilise the FSR and HOB incentives. The increased energy and water performance standards will contribute towards emission reduction of new and refurbished buildings in the Parramatta Road Corridor, as a pathway to net zero.

These provisions have been informed by the Sustainability Study provided at Appendix 9 which included a comprehensive review of sustainability provisions recommended in best practice planning policies, demonstrating consistency with the Net Zero Plan.

Council is also investigating options to incorporate the requirement for new developments to commit to the procurement of renewable energy over the duration of the building’s lifespan to ensure buildings continue to reduce their energy consumption and achieve net zero energy use. Further Council’s draft DCP includes supporting controls to achieve these increased targets and also sets requirements for electric charging infrastructure.

**State Environmental Planning Policies**

**Q6. Is the planning proposal consistent with applicable State Environmental Planning Policies?**

The consolidated SEPPs, which combine the former SEPPs into 11 SEPPs, commenced on 1 March 2022.

Consistency with the applicable SEPPs is discussed in the table below.

*Table 26 – Consistency with applicable SEPPs*

SEPP	Comment
Housing	<p><b>Former SEPP (Affordable Rental Housing) 2009, SEPP (Housing for Seniors and People with a Disability) 2004 SEPP Affordable Housing (Revised Schemes) - No 70.</b></p> <p>Consistent.</p> <p>This Planning Proposal does not contain provisions that contradict or hinder the application of this SEPP.</p> <p>A draft Affordable Housing Contributions Scheme (Appendix 8) is proposed for Leichhardt Precinct as included in this Planning Proposal.</p>
Transport Infrastructure and	<p><b>Former SEPP (Infrastructure) 2007 and SEPP (Educational Establishments and Childcare Facilities) 2017</b></p> <p>Consistent</p> <p>This Planning Proposal does not contain provisions that contradict or hinder the application of this SEPP.</p>
Primary Production	N/A
Biodiversity Conservation and	<p><b>Former SEPP Bushland in Urban Areas - No. 19</b></p> <p>Consistent.</p> <p>This Planning Proposal does not contain provisions that contradict or hinder the application of this SEPP.</p> <p><b>Former Sydney Regional Environmental Plan (Sydney Harbour Catchment) 2005</b></p> <p>Consistent.</p> <p>The area is within the Sydney Harbour Catchment. This planning proposal does not contain provisions that contradict the requirements of this SEPP. Should the proposed IWLEP</p>

SEPP	Comment
	<p>amendment proceed, future development must continue to comply with the requirements of this SEPP.</p> <p><b>Former SEPP (Vegetation in Non-Rural Areas) 2017</b></p> <p>Consistent.</p> <p>This Planning Proposal does not contain provisions that contradict or hinder the application of this SEPP</p>
Resilience and Hazards	<p><b>Former SEPP (Coastal Management) 2018</b></p> <p>Consistent</p> <p>This Planning Proposal does not contain provisions that contradict or hinder the application of this SEPP.</p> <p><b>Former SEPP Remediation of Land – No. 55</b></p> <p>Consistent.</p> <p>This Planning Proposal does not contain provisions that contradict or hinder the application of this SEPP.</p> <p>The Planning Proposal seeks to introduce new uses to a few sites which have some level of contamination. To address this, Council commissioned Preliminary Site Investigations (Appendix 6) which conclude that these sites can be made suitable for the proposed uses subject to appropriate site-by-site contamination management and/or remediation at the development application stage.</p> <p>The application of SEPP 55 at the DA stage will ensure that the sites are remediated before the land is used for the proposed purposes.</p> <p><b>Former SEPP Hazardous and Offensive Development – No. 33</b></p> <p>Consistent</p> <p>This Planning Proposal does not contain provisions that contradict or hinder the application of this SEPP.</p>
Building Sustainability Index: BASIX 2004 – No. 64/ Sustainable Buildings SEPP 2022	<p>Consistent.</p> <p>DPE have introduced the Sustainable Buildings SEPP 2022 which came into force on 29 August 2022 and will commence on 1 October 2023. Upon commencement, the Sustainable Buildings SEPP 2022 will repeal the Building Sustainability Index: BASIX SEPP 2004.</p> <p>Both BASIX SEPP 2004 and Sustainable Buildings SEPP 2022 allow for increased sustainability targets beyond the standard SEPP requirements through an incentives FSR/HOB mechanism.</p> <p>This Planning Proposal provides FSR and height incentives to encourage new developments to meet, or where relevant,</p>

SEPP	Comment
	<p>exceed the PRCUTS Sustainability and Resilience Requirements. Refer to Appendix 9 for further justification.</p> <p>Compliance with the Sustainable Buildings SEPP 2022 is to be demonstrated at the development application stage.</p>
<p>Design Quality of Residential Apartment Development – No. 65</p>	<p>Consistent.</p> <p>This Planning Proposal does not contain provisions that contradict or hinder the application of this SEPP. Compliance with the SEPP 65 is to be demonstrated at the development application stage.</p> <p>Design quality principles of SEPP 65 and the supplementary Apartment Design Guidelines (ADG) were considered as part of PRCUTS and supporting documents, <i>Planning and Design Guidelines</i> and <i>Fine Grain Study</i>.</p> <p>The proposed height and FSR controls and additional local provisions have been informed by these documents, and the Appendix 2 - Urban Design Studies, commissioned by Council which tested the PRCUTS recommendations for areas within the Leichhardt, Taverners Hill and Kings Bay/ Croydon precincts. The proposed height and FSR controls and additional local provisions have been informed by these documents, and the Appendix 2 - Urban Design Studies, commissioned by Council which tested the PRCUTS recommendations for areas within the Leichhardt, Taverners Hill and / Croydon precincts.</p> <p>The testing included consideration of ADG requirements and SEPP 65 principles. This included overshadowing testing that demonstrated adequate solar access is achievable for future public/ private domain and public open spaces with respect to the anticipated built form.</p> <p>The supporting DCP provisions will also guide new development by implementing associated design controls which will ensure that desired urban design and community outcomes are realised in the Corridor.</p> <p>Further, this Planning Proposal and draft DCPs were referred to Council’s Architectural Excellence and Design Review Panel (AEDRP) on 5 April 2022 in accordance with the requirements of Clause 15(1)(a) of the Environmental Planning and Assessment Regulation, 2021 (EPA Regulations). AEDRP are required to consider the draft DCPs with respect to the matters specified in Parts 1 and 2 of the Apartment Design Guide.</p> <p>AEDRP’s initial feedback has led to refinements of the draft DCPs. The revised draft DCPs will be exhibited with the Planning Proposal for community feedback.</p>
<p>Industry and Employment</p>	<p><b>Former SEPP Advertising and Signage – No. 64</b></p> <p>Consistent</p> <p>This Planning Proposal does not contain provisions that contradict or hinder the application of this SEPP.</p>

SEPP	Comment
Resources and Energy	N/A
Planning Systems	<b>Former SEPP (Concurrences and Consents) 2018 and SEPP (State and Regional Development) 2011</b> Consistent. This Planning Proposal does not contain provisions that contradict or hinder the application of this SEPP.
Exempt and Complying Development Codes	Consistent. This Planning Proposal does not contain provisions that contradict or hinder the application of this SEPP.
Precincts - Eastern Harbour City	N/A

### 11. Local Planning Directions

#### Q7. Is the planning proposal consistent with applicable Ministerial Directions (s.9.1 directions) or key government priority?

The Planning Proposal seeks to deliver on the key government priority relating to boosting housing supply and providing affordable housing in NSW. It will assist government in unlocking housing supply to meet the housing shortfall in short to medium term.

The Planning Proposal area has been identified to address the Department of Planning and Environment's (DPE) requirement that a short to medium term shortfall of up to 1600 dwellings in the Inner West be met. Further, Council has received a letter from DPE to urgently progress their work towards finalising the Parramatta Road Planning Proposal by the end of March 2024 to help unlock much needed housing opportunities as soon as possible.

The letter states that: *'This is the Inner West Council's only substantive planning proposal that unlocks significant housing supply. This is important given Council is not projected to 'meet its housing target of 5,000 dwellings between 2021 and 2026.*

*Governments at all levels have a shared responsibility to address the housing crisis and meet the goals of the National Housing Accord. That shared responsibility also requires a strong sense of urgency. This proposal offers the opportunity to deliver up to 1,700 dwellings and is an important first step for Council to unlock more housing supply.'*

Consistency with the applicable Local Planning Directions is discussed in the table below:

Table 27 – Consistency with applicable Local Planning Directions

Direction	Consistency/Comment
<b>Focus Area 1: Planning Systems</b>	
1.1 Implementation of Regional Plans	Consistent

Direction	Consistency/Comment
	<p>As detailed previously, this Planning Proposal is consistent with the vision, land use strategy, goals, directions and actions of Greater Sydney Region Plan – A Metropolis of Three Cities and the Eastern City District Plan.</p> <p>The Planning Proposal is to facilitate the implementation of the PRCUTS, which is approved by the Secretary of the DPE.</p>
<p>1.3 Approval and Referral Requirements</p>	<p>Consistent</p> <p>This Direction requires planning proposals to ensure LEP provisions encourage efficient and appropriate assessment of development.</p> <p>This Planning Proposal is consistent with this Direction. It does not seek to change existing requirements for concurrence, consultation or referral provisions and does not identify any developments as designated development.</p> <p>Standard consultation with public authorities such as DPE, TfNSW, NSW Health, Sydney Water, Sydney Airport etc. is required as part of the Gateway process. Any provisions for future concurrence or consultation with public authorities regarding the State infrastructure needs will be agreed with the appropriate Minister or public authority prior to undertaking community consultation in satisfaction of Schedule 1 of the EP&amp;A Act as required by 1(b) of the Direction.</p> <p>The Planning Proposal will require approval from DPE prior to any development consent being granted as it includes provisions to enter into satisfactory agreement with DPE with respect to regional infrastructure contributions. This is consistent with the Local Planning Direction 1.5 regarding implementation of the PRCUTS as discussed below.</p>
<p>1.4 Site Specific Provisions</p>	<p>Consistent</p> <p>The Planning Proposal does not seek to apply unnecessarily restrictive site-specific planning controls.</p> <p>The Planning Proposal provides optional FSR and HOB incentives above the existing LEP controls to encourage additional development through site-specific provisions which require addressing planning issues associated with the proposed amendments. This approach is necessary to ensure that development in the corridor occurs in line with the vision of PRCUTS.</p> <p>Any design outcomes included in the Planning Proposal or associated draft DCP amendments are to give effect to the PRCUTS planning and design guidelines or in certain instances, deliver even better outcomes, where supported by detailed site-testing. These requirements are not enforceable unless the development seeks to achieve additional uplift through the incentives mechanism, above the LEP base standards, in which case it should demonstrate at the DA stage that it has sufficient site-specific merit prior to any development consent being granted.</p>

Direction	Consistency/Comment
	The Planning Proposal is to facilitate the implementation of the PRCUTS, which is approved by the Secretary of the DPE.
<b>Focus Area 1: Planning Systems – Place-based</b>	
1.5 Parramatta Road Corridor Urban Transformation Strategy	This is the most relevant Local Planning Direction as the Planning Proposal has been prepared to implement this Direction. Consistency with each objective is discussed in detail below.
<i>The objectives of this Direction are to:</i>	
<p>(a) facilitate development within the Parramatta Road Corridor that is consistent with the Parramatta Road Corridor Urban Transformation Strategy (November 2016) and the Parramatta Road Corridor Implementation ToolKit</p>	<p>The Planning Proposal will facilitate development which is largely consistent with the vision of Parramatta Road Corridor Urban Transformation Strategy (November 2016). This Planning Proposal and its <i>Appendix 11</i> provide a detailed consistency check against the Strategy and supporting Implementation Toolkit documents including Planning and Design Guidelines and Implementation Plan 2016-2023 and Implementation Plan 2021.</p> <p>Variations are sought to the Strategy including its land-use, density and height recommendations in response to more recent Government policy and Council's recent evidence-based work to deliver precinct-wide urban design outcomes and community benefits.</p> <p>PRCUTS Stage 1 release areas have been investigated and refined to support the principle of land use and transport integration which is at the heart of this Strategy's vision and objectives.</p>
<p>(b) provide a diversity of jobs and housing to meet the needs of a broad cross-section of the community, and</p>	<p>The Planning Proposal would provide 1516 new dwellings and 1944 new jobs. It also seeks to mandate the provision of affordable housing in Leichhardt Precinct.</p>
<p>(c) guide the incremental transformation of the Parramatta Road Corridor in line with the delivery of necessary infrastructure.</p>	<p>The Strategy is underpinned by the objectives of aligning growth with the delivery of infrastructure. The Planning Proposal area is strategically selected as it is in close proximity of existing public transport stops which are well-served.</p> <p>The Planning Proposal includes provisions for developments to access additional development capacity by making adequate State/ Regional infrastructure contributions.</p> <p>The timely delivery of infrastructure with growth will also be addressed through collection of local infrastructure contributions via the Inner West Local Infrastructure Contributions Plan 2023. This Plan was adopted by Council February 2023 and has considered the level of local infrastructure required to support growth in Parramatta Road Corridor.</p> <p>The supporting precinct-wide traffic and transport study puts a strong case forward regarding provision of on-street rapid transit along Parramatta Road to service the future growth. Council will continue to advocate for NSW Government to provide adequate state infrastructure through collection of Housing and Productivity</p>

	<p>contributions. Given the level of infrastructure funding gap in this corridor, Council will continue to advocate that any Housing and Productivity contributions collected from this area be also reinvested into the area for provision of state infrastructure including:</p> <ul style="list-style-type: none"> <li>a) State and regional roads,</li> <li>b) bus interchanges and bus lanes,</li> <li>c) land required for regional open space,</li> <li>d) social infrastructure and facilities (such as schools, hospitals, emergency services and justice purposes),</li> <li>e) light rail infrastructure.</li> </ul>
<p><i>(1) A planning proposal that applies to land within the Parramatta Road Corridor must:</i></p>	
<p><i>(a) give effect to the objectives of this Direction</i></p>	<p>As above.</p>
<p><i>(b) be consistent with the Strategic Actions within the Parramatta Road Corridor Urban Transformation Strategy (November 2016),</i></p>	<p>The Planning Proposal is fully consistent with the PRCUTS Strategic Actions as discussed in this Planning Proposal and its Appendix 11.</p>
<p><i>(c) be consistent with the Parramatta Road Corridor Planning and Design Guidelines (November, 2016) and particularly the requirements set out in Section 3 Corridor-wide Guidelines and the relevant Precinct Guidelines,</i></p>	<p>The Planning Proposal is largely consistent with the Parramatta Road Corridor Planning and Design Guidelines as detailed in Appendix . Any variations sought to PRCUTS design recommendations are to deliver better urban design and place-based outcomes, aligned with the above Strategic Actions.</p>
<p><i>(d) be consistent with the staging and other identified thresholds for land use change identified in the Parramatta Road Corridor Implementation Plan 2016 – 2023 (November 2016), and the Parramatta Road Corridor Urban Transformation Implementation Update 2021, as applicable</i></p>	<p>The Planning Proposal is inconsistent with the staging identified in the Parramatta Road Corridor Implementation Plan 2016-2023. This is primarily to align growth with the delivery of infrastructure. This is justified by the study at Appendix 11.</p> <p>Notwithstanding, the Planning Proposal is consistent with the PRCUTS Implementation Update 2021, which outlines 6 new and amended implementation actions for consideration.</p> <ul style="list-style-type: none"> <li>1. Timing of release</li> <li>2. Public Transport</li> <li>3. Active Transport</li> <li>4. Road Improvements and upgrades</li> <li>5. Funding Framework or satisfactory arrangements</li> <li>6. Open Space</li> </ul>

	<p>The discussion within the Planning Proposal outlines consistency with the new and amended implementation actions.</p> <p>The Parramatta Road Corridor Implementation Update 2021 is a new addition to the PRCUTS suite, outlining updated implementation actions to suit the contemporary planning and policy context. Rather than updating the Implementation Plan 2016-2023, this document supplements it with additional actions. If there is conflict between the Implementation Plan 2016-2023 and the Implementation Update 2021, the latter applies.</p> <p>As stated above, the variation sought to the Implementation Plan regarding staging of release areas is to align with the provision of infrastructure and ensure the delivery of sound and measured outcomes.</p>
<p><i>(e) contain a requirement that development is not permitted until land is adequately serviced (or arrangements satisfactory to the relevant planning authority, or other appropriate authority, have been made to service it) consistent with the Parramatta Road Corridor Implementation Plan 2016 – 2023 (November, 2016),</i></p>	<p>The Planning Proposal requires that satisfactory arrangements be made for the provision of State public infrastructure before a consent is granted for development in these precincts.</p>
<p><i>(f) be consistent with the relevant District Plan.</i></p>	<p>The Planning Proposal is fully consistent with the Eastern City District Plan as discussed under Section B.</p>
<p><i>A planning proposal may be inconsistent with the terms of this Direction only if the relevant planning authority can satisfy the Secretary of the DPE (or an officer of the Department nominated by the Secretary) that the planning proposal is:</i></p>	
<p><i>(a) consistent with the Out of Sequence Checklist in the Parramatta Road Corridor Implementation Plan 2016 – 2023 (November 2016), or</i></p>	<p>N/A - The Proposal does not rely on the Out of Sequence Checklist for its justification.</p>
<p><i>(b) justified by a study (prepared in support of the planning proposal) that clearly demonstrates better outcomes are delivered than identified in the Parramatta Road</i></p>	<p>The Planning Proposal is justified by the Justification Study at Appendix which brings together relevant Government policies and supporting technical documents to demonstrate that better outcomes will be delivered than those identified in the Strategy and its Implementation Plan having regard to the vision and objectives.</p> <p>The variations to PRCUTS are necessary as the Strategy:</p>

<p><i>Corridor Urban Transformation Strategy (November, 2016) and Parramatta Road Corridor Implementation Plan 2016-2023 (November, 2016) having regard to the vision and objectives, or</i></p>	<ul style="list-style-type: none"> <li>• is out-of-date having been adopted in 2016 and any associated background studies to inform its preparation been completed between 2014-2016.</li> <li>• is high-level considering it spans 20km from Camperdown to Granville and lacks local place-based assessment.</li> <li>• has numerous discrepancies including significant underestimation of proposed number of dwellings and over-estimation of the number of jobs – this has been confirmed through the work completed by DPE and Council for Parramatta Road Corridor Precinct-wide Traffic and Transport Study.</li> </ul> <p>Thus, refinements are necessary to achieve PRCUTS’ original vision and objectives. <i>Appendix 11</i> provides detailed justification to fully satisfy the requirement of this Direction.</p>
<p><i>(c) of minor significance.</i></p>	<p>It is noted that the Planning Proposal’s inconsistency is not of minor insignificance and is therefore aimed to satisfy (b) above.</p>
<p><b>Focus Area 2: Design and Place – Not in effect.</b></p>	
<p><b>Focus Area 3: Biodiversity and Conservation</b></p>	
<p>3.1 Conservation Zones</p>	<p>Consistent</p> <p>This Direction requires that the environmental protection standards applying to the land not be reduced.</p> <p>Land on the southern side of Parramatta Road from Hawthorne Canal to Palace St is identified as “Biodiversity” on the IWLEP 2022 Natural Resource - Biodiversity Map. The Planning Proposal provisions do not change the protection for this area. It also does not alter the requirements to be considered in the assessment of a development application on land identified on the Biodiversity Map.</p>
<p>3.2 Heritage Conservation</p>	<p>Consistent</p> <p>The Planning Proposal has been informed by the following studies prepared by heritage experts in accordance with the NSW Heritage Office Guidelines:</p> <ol style="list-style-type: none"> <li>Heritage Assessment Study including Heritage Inventory Sheets prepared by Hector Abraham Architects for Inner West Council in 2021 and supplementary report by GML Heritage (<i>Appendix 3</i>).</li> <li>Parramatta Road Corridor Urban Transformation Corridor Strategy Fine Grain Study and its identification of potential heritage items.</li> <li>Parramatta Road/Norton Street Heritage Study completed by NBRS Architecture for Leichhardt Council in 2016.</li> </ol> <p>Amendments relating to heritage include listing 24 additional properties of heritage significance and 1 Archaeological Site, extending the Excelsior HCA to include 20-24 Norton Street, Leichhardt and creating a new HCA consisting of 9 properties for a set of workers cottages on Barker Street, Lewisham.</p>

<p>3.7 Public Bushland</p>	<p>Consistent</p> <p>Land to the south of Parramatta Road, between the Hawthorne Canal and Palace St, is identified as “Biodiversity” in the IWLEP 2022. The changes proposed by the Planning Proposal are unlikely to result in adverse effects on critical habitat for threatened species and ecological communities.</p> <p>The Planning proposal is therefore consistent with the objectives of this direction as it will not alter the extent of the biodiversity mapping.</p>
<p>3.10 Water Catchment Protection Area</p>	<p>Consistent.</p> <p>The Planning Proposal area is located within the Sydney Harbour Catchment area pursuant to the Biodiversity and Conservation SEPP 2021.</p> <p>However, the implementation area is not directly within or adjacent to the regulated catchment natural water body. Further, the implementation area is not in an identified wetland protection area or foreshore and waterway area.</p> <p>A Flood Impact Risk Assessment provided at Appendix 5 was undertaken to ensure that the proposed planning controls do not adversely impact the flood behaviour within the implementation area and the relevant catchment area. This outcome is consistent with the objectives of this direction.</p> <p>The proposed controls are therefore unlikely to have an adverse direct, indirect, or cumulative impact on water quality and flows of natural water bodies and the environment more generally.</p> <p>Site-specific development impacts will be assessed in accordance with the Biodiversity and Conservation SEPP 2021 at the development application stage.</p>

**Focus Area 4: Resilience and Hazards**

**4.1 Flooding**

The planning proposal incorporates provisions that align with the mentioned requirements, including the:

- NSW Flood Prone Land Policy;
- Floodplain Development Manual 2005 and 2023;
- Considering flooding in land use planning guideline 2021; and
- any adopted flood study and/or floodplain risk management plan approved by the relevant council.

The Planning Proposal proposes to rezone land located within the flood planning area that has already been developed, however, modelling has been undertaken to ensure that the change in development will not have significant flood impacts to other properties or result in an increased hazard to the existing and proposed developments. As such, the planning proposal does not include provisions that allows for an increase in development in floodway areas, development causing significant flood impacts to other properties, residential accommodation in high hazard areas, significant increase in development or dwelling density, development in areas where effective evacuation is not possible, development

without consent (excluding exempt development or agriculture), provisions leading to a significantly increased requirement for government spending on emergency management services and flood mitigation, or hazardous industries or storage establishments that cannot effectively contain hazardous materials during a flood event.

As such, the Planning Proposal does not include provisions that allows for an increase in development in floodway areas, development causing significant flood impacts to other properties, development where effective evacuation is not possible, or result in a significantly increased requirement for government spending on emergency management services and flood mitigation.

Council's existing DCP flood controls, which will continue to apply for this Planning Proposal area, require vulnerable developments like childcare, boarding houses, group homes, care facilities and seniors housing to account for the Probable Maximum Flood (PMF) event when determining appropriate flood levels and evacuation routes.

This Planning Proposal is inconsistent with the direction but meets the requirements of consistency clause (c) because whilst there is minor afflux occurring within road reserves due to the Planning Proposal, it is supported by a flood impact risk assessment (FIRA) (Appendix 5) accepted by the relevant planning authority. This FIRA is prepared in accordance with the principles of both the Floodplain Development Manual 2005 and 2023 and satisfies the requirements set by the relevant planning authority. Refer to Appendix 5 for detailed information.

**4.4 Remediation of Contaminated Land**

Consistent

The Planning Proposal seeks to introduce new uses to the below few sites which have been identified to have some level of contamination. To address this, Council commissioned Preliminary Site Investigations (Appendix 6) which conclude that these sites can be made suitable for the proposed uses subject to appropriate site-by-site contamination management and/or remediation at the development application stage.

The application of SEPP Resilience and Hazards 2021 (formerly SEPP 55) at the DA stage will ensure that the sites are remediated before the land is used for the proposed purposes.

The following sites which are likely to have some level of contamination:

**Leichhardt precinct:**

- 2-18 Crystal Street, Petersham

This site is currently zoned E1 Local Centre and occupied by a service station, vehicle sales or hire premises and vehicle repair workshops. The site is proposed to be rezoned to R3 Medium Density Residential with residential flat building as an additional permitted use. No other new sensitive uses are being introduced as a result of the proposed rezoning of this site from E1 Local Centre to R3 Residential Zoning. A number of sensitive uses such as centre-based childcare facilities, community facilities and education facilities are already permitted under the current E1 Local Centre Zoning.

**Kings Bay/ Croydon precinct:**

- 590 Parramatta Road, Croydon
- 596-598 Parramatta Road, Croydon
- 600 Parramatta Road, Croydon

- 604-610 Parramatta Road, Croydon
- 612-614 Parramatta Road, Croydon
- 616-618 Parramatta Road, Croydon
- 620-624 Parramatta Road, Croydon
- 210 Croydon Road, Croydon

These sites are currently zoned E3 Productivity Support and occupied by industrial/commercial uses, service stations, vehicle sales or hire premises, vehicle repair workshops, fast food outlets, among other uses. These properties are identified as Opportunity Sites in the Planning Proposal and residential flat buildings will be permissible as an APU above ground floor commercial uses. No other new sensitive uses are being introduced other than what are already permitted under the current zoning E3 Productivity Support.

In accordance with the Local Planning Directions 4.4 (2), Council has undertaken Preliminary Site Investigations (*Appendix 6*) including desktop analysis of all the above-mentioned properties where sensitive or potentially sensitive uses may occur. These investigations conclude that the sites can be made suitable for the proposed uses subject to appropriate site-by-site contamination management and/or remediation at the development application stage.

This approach is warranted by the existing SEPP framework for Stage 2 Detailed Site Investigations and Remediation Action Plans at the Development Application Stage.

<p>4.5 Acid Sulfate Soils</p>	<p>Inconsistent - but satisfies Consistency test Direction 4.5 Clause (b): minor significance.</p> <p>North of Parramatta Road in Taverners Hill and Leichhardt Precinct are subject to Class 5 Acid Sulfate Soils. Kings Bay/ Croydon Precinct is also subject to Class 5 Acid Sulfate Soils.</p> <p>While the provisions in this Planning Proposal may result in some intensification of land uses, this intensification is in part in response to the PRCUTS which is approved by the Secretary of DPE.</p> <p>Given the Acid Sulfate Soils provisions in the IWLEP2022, specific responses to acid sulfate soils can be addressed site by site through the development application process.</p>
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**Focus Area 5: Transport and Infrastructure**

<p>5.1 Integrating Land Use and Transport</p>	<p>Consistent</p> <p>The Planning Proposal meets all the objectives of this Direction.</p> <p>The Planning Proposal seeks to implement PRCUTS, which is an integrated land use planning and transport policy framework for the Parramatta Road Corridor, approved by the Secretary of DPE.</p>
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	<p>The recommendations of IWC/DPE Parramatta Road Corridor Precinct-wide Traffic and Transport Study (<b>Error! Reference source not found.</b>) including supporting modelling have also been considered in drafting this Planning Proposal's provisions as discussed in the previous sections.</p> <p>Preliminary consultation with TfNSW has been undertaken during the preparation of the precinct-wide traffic study and Council's LHS Implementation Plan. Further consultation with TfNSW and Sydney Metro will be undertaken post-Gateway through the public consultation process.</p>
<p>5.2 Reserving Land for Public Purposes</p>	<p>Consistent</p> <p>This Planning Proposal does not seek to create, alter or reduce reservations of land for public purposes.</p> <p>It does propose to rezone the existing TfNSW owned car park 2 Hay Street, Leichhardt (Lot A DP 348040) from E1 – Local Centre to RE1 – Public Recreation.</p> <p>This rezoning is required to deliver the PRCUTS Planning and Design Guideline's Open Space Requirement <i>"Provide a new public open space area in the eastern Frame Area that connects Hay Street, Dot Lane, and Balmain Road by repurposing existing at grade car parks."</i></p> <p>This land is not required to be reserved for public purposes as it is already in NSW Government's possession. This could be transferred or leased to Council for the use of open space.</p>

**5.3 Development Near Regulated Airports and Defence Airfields**

Consistent

Kings Bay/ Croydon and Taverners Hill Precincts are within Aircraft Noise Exposure Forecast (ANEF) 15-20 range; Leichhardt is affected by ANEF 15-20 (small portion), ANEF 20-25 and ANEF 25-30.

Under this Direction, planning proposals that rezone land for residential purposes or increase residential densities where the ANEF is between 20 and 25 must include a provision that interior noise levels will meet the Australian standard for aircraft noise intrusion (AS 2021).

The Direction is silent on rezoning or increased densities in the ANEF 25-30, but logically the requirements that apply for areas in the ANEF 20 –25 also apply to those in the ANEF 25 – 30.

Leichhardt Precinct is proposed to accommodate additional residential growth in the ANEF 25-30 contour. This proposal is informed by:

- An Aircraft Noise Impact Assessment (see *Appendix 7* and discussion below),
- The National Airports Safeguarding Framework (Guideline A) (November 2016) that details two scenarios for considering rezoning land for noise sensitive uses – see discussion below, and
- Consultation with Sydney Airport Corporation and Commonwealth Department of Infrastructure - see summary of comments and discussion below.

Aircraft Noise Impact Assessment

Council has undertaken Aircraft Noise Impact Assessment (Appendix 7) which intends to address this intensification in the context summarised below:

- The precinct is exposed to noise levels of between 74dB and 88dB from aircraft departures and 66-87dB from arrivals.
- Sydney Airport Masterplan 2039 identifies a projected reduction of aircraft noise levels through decommissioning old aircraft and a new generation of quieter aircraft.
- The typical noise level difference between ANEF 20-25 and ANEF 25-30 is only 2dB.
- This difference in noise level would not materially alter the building materials required to achieve compliance with internal design levels of AS 2021.
- Construction and design options are available that will allow development in the ANEF 25-30 areas to meet the internal noise level requirements of AS 2021.

The assessment concludes that land within the ANEF 25-30 contour is acceptable for increased residential densities and that any future residential development should be designed and constructed to comply with internal design levels of AS 2021.

National Airports Safeguarding Framework (Guideline A) (NASF Guideline)

The purpose of this guideline is to provide guidance to Commonwealth, State, Territory and Local Government decision makers to manage the impacts of noise around airports including assessing the suitability of development.

The NASF Guideline takes into consideration the ANEF contours, being the subject of the Local Planning Directions, and enhances guidance based on the N70 charts that aim to identify not only the noise level but the number of occurrences an area may exceed a specified level and then provides advice based on two rezoning scenarios:

1. Rezoning greenfield land – advises avoiding noise sensitive uses within the 20ANEF contour and where the N70 charts show the number of daily events exceed specified levels i.e. 20 or more daily events greater than 70dB(A).
2. Rezoning brownfield or existing urban land for noise sensitive use. In this scenario it advises, *“there is a need to balance the need to provide housing, economic growth and strategic planning outcomes against the operational needs of the airports. This approach may identify some adversely impacted parties and it can also identify where benefits outweigh the overall disadvantages”*.

Where the strategic merit of a proposal includes noise sensitive uses such as new housing in the Leichhardt Precinct, the NASF Guideline emphasises the need to incorporate measures to manage the implications and physically reduces noise impacts, such as those detailed above in the Impact Assessment Study. In some instances, potential redevelopment in the Leichhardt Precinct will result in areas already exposed to aircraft noise achieving a more desirable outcome through better design and construction responses; this is advocated as a positive outcome by the NASF Guideline. The Guideline also encourage disclosure and Council is considering expanding S10.7(5) details to ensure that future residents are made aware of these impact prior to purchase.

The Guideline clearly identifies it is not appropriate to allow development that would impact on operational safety of an airport.

The Planning Proposal area for Leichhardt Precinct is within the 80m to 100m Sydney Airport OLS and has a ground level of 28m to 36m. The maximum building height in parts of this area are proposed to be 30.5m. This will result in a maximum building height above ground level of approximately 60m to 64m, being well below the OSL in this location, and thereby, will not impact on the operational safety of Sydney Airport.

Consultation with Sydney Airport Corporation and Commonwealth Department of Infrastructure, Regional Infrastructure and Communications

Council has consulted with key stakeholders seeking preliminary comments on the Planning Proposal. A summary of their feedback includes:

The Commonwealth Government has advised:

*“As required by the National Safeguarding Framework Guideline A: Measures for Managing Impacts of Aircraft Noise Inner West Council should ensure any development is undertaken in a manner that physically reduces noise impacts (e.g. through appropriate construction techniques and adherence to Australian Standard AS2021) but also through a disclosure process that ensures future residents are aware of these impacts prior to purchase.”*

As detailed above, the Planning Proposal includes measures to reduce impacts and is giving consideration to disclosure mechanisms.

Sydney Airports Corporation has advised:

They provided initial comments during preparation of the Parramatta Road Corridor Urban Transformation Strategy and reiterate *‘aircraft noise would be a specific issue in the Leichhardt Precinct and that the precinct core area remain beneath one of Sydney Airport’s busiest flight paths’*. In addition, they state:

- *“The proposed locations of the residential dwellings located in the area within the 25-30 ANEF zone are said to be “unacceptable” under the relevant Australian Standard, AS2021:2015. Such dwellings could only become “acceptable” if noise control features were incorporated in each of the residential dwellings”, and*
- *Zoning for noise-sensitive development [should] be avoided where ultimate capacity for long range noise modelling [N70 charts] for the airport indicates 20 or more daily events great then 70dB(A).*

Council recognises the Leichhardt Precinct identified within the Planning Proposal as well as the broader, predominantly residential, suburb is affected by aircraft noise and under anticipated aircraft activity will continue to be affected. However, it is also of the view that the benefits arising from redevelopment of sites to include residential uses will:

- assist in catering for a growing population, within the heart of Leichhardt, and where everyday services, facilities and transport are readily available.
- provide an opportunity to ensure redevelopment is designed and constructed to reduce those impacts.

In addition, clauses within the current local planning instrument encourage residential uses within certain local centres, including Leichhardt. This Planning Proposal reinforces existing objectives and aims to incentivise redevelopment. In turn, these opportunities can play a role in facilitating enhanced liveability through appropriate measures to reduce impacts.

**Focus Area 6: Housing**

6.1 Residential Zones	Consistent
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The Planning Proposal encourages housing choice through the proposed uplift and resulting increased housing supply. It will broaden the location of housing available for sale and rent (including affordable housing), make more efficient use of existing infrastructure and services, and facilitate new housing that is of good design.

It includes provisions to ensure that prior to any development occurring under the incentive controls, the proposed development seeking additional capacity over existing LEP controls makes appropriate state/ regional infrastructure contributions.

The Planning Proposal also seeks to implement PRCUTS, which is approved by - the Secretary of DPE.

**Focus Area 7: Industry and Employment**

Consistent

The Planning Proposal:

- a) is consistent with all three objectives of this direction
- b) retains the areas and locations of existing business zones
- c) increases the total potential floor space area for employment uses and related public services in the business zones
- d) does not reduce potential industrial floorspace
- e) is in accordance with PRCUTS approved by the Secretary of DPE.

The Planning Proposal aims to deliver the following non-residential floorspace in the Stage-1 Planning Proposal area of Leichhardt and Kings Bay/ Croydon Precincts:

Leichhardt:

- Current – 12,324m<sup>2</sup>
- Proposed – 42,628m<sup>2</sup>
- Uplift of – 30,304m<sup>2</sup>

Kings Bay/ Croydon:

- Current – 63,375m<sup>2</sup>
- Proposed – 77,872m<sup>2</sup>
- Uplift –14,497m<sup>2</sup>

There are no changes proposed to industrial or business zones in the Taverners Hill Precinct. Overall, the Planning Proposal has the capacity to create 1944 new jobs in the Planning Proposal area.

There are two E3 Productivity Support Opportunity Sites in Kings Bay/ Croydon where residential flat buildings are being introduced as an additional permissible use. The sites will continue to provide employment uses on the ground floor. These sites have been strategically selected to provide residential uses to capitalise on the proximity to proposed Five Dock Metro Station to create more liveable, sustainable, and walkable neighbourhoods.

This may reduce the theoretical employment floorspace capacity on these two sites, but overall, there would be a significant increase of potential employment floorspace and job numbers in Kings Bay/ Croydon Precinct. Consequently, this is considered to be a minor inconsistency.

This is also justified by the attached Economic Assessment (*Appendix 4*) which demonstrates that permitting residential uses on these sites will not negatively impact the economic productivity outcomes of the precinct and in turn deliver positive outcomes through the generation of new type of jobs and employment floorspace.

Potential land use conflicts arising from the Kings Bay/ Croydon Opportunity Sites will be managed through the proposed site-specific LEP provision which allows residential flat buildings but only if the proposed development provides commercial/ business uses on the entirety of the ground floor that are compatible with residential uses above. Further the supporting draft DCP amendments include provisions to manage environmental impacts including noise, odour, and air pollution to further minimise any potential land use conflicts.

**Focus Area 8: Resources and Energy – N/A**

**Focus Area 9: Primary Production – N/A**

**Section C – Environmental, social and economic impact**

**Q7. Is there any likelihood that critical habitat or threatened species, populations or ecological communities, or their habitats, will be adversely affected as a result of the proposal?**

The proposed changes are unlikely to result in any adverse effects on critical habitat for threatened species and ecological communities.

Land to the south of Parramatta Road between the Hawthorne Canal and Palace St, Petersham is identified as “Biodiversity” on the Natural Resource—Biodiversity Map under IWLEP 2022 (see Figure 10 below). Clause 6.4 Terrestrial biodiversity of the IWLEP 2022 requires the consent authority to consider the potential impacts to fauna and flora, and their habitats, in the assessment of a development application.

The Planning Proposal will not alter the extent of the biodiversity mapping, nor the requirements to be considered in the assessment of a development application on land identified as Biodiversity. Therefore, the Planning Proposal will not adversely impact threatened species, populations or ecological communities, or their habitats.

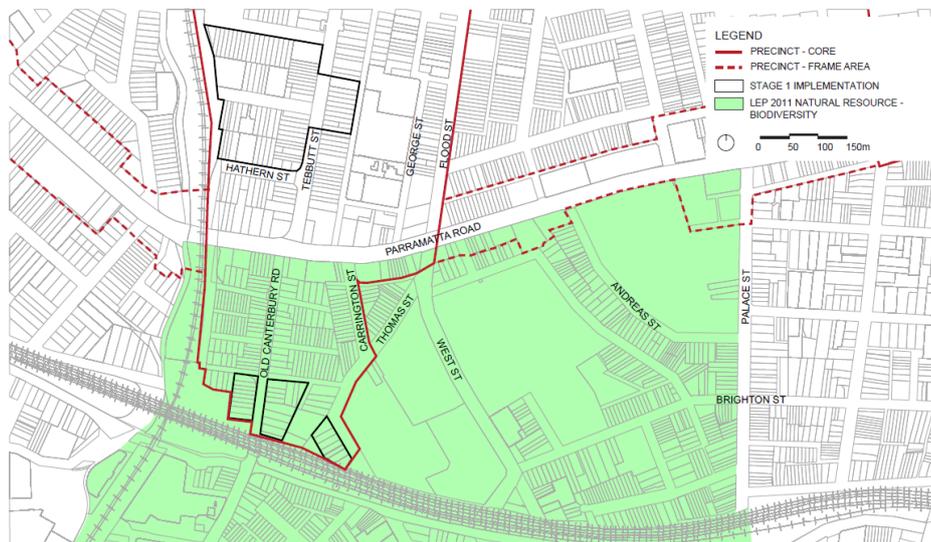


Figure 10 – Extract from IWLEP 2022 indicating the extent of Terrestrial Biodiversity (in green) in the Planning Proposal area (in black boundary).

**Q8. Are there any other likely environmental effects as a result of the planning proposal and how are they proposed to be managed?**

It is unlikely that the proposal will result in any environmental effects that cannot be managed through the proposed LEP provisions in conjunction with the existing LEP provisions.

The Planning Proposal in fact includes provisions which will positively contribute to the environmental considerations in the Parramatta Road Corridor. In particular, the proposed amendments through FSR and HOB incentives mechanism will ensure that urban design, built form, amenity, stormwater management and environmental sustainability are given thorough consideration prior to granting consent to any additional development capacity above the existing LEP controls.

The Planning Proposal will result in beneficial environmental effects as it seeks to implement improved sustainability measures in relation to high performing buildings. It also seeks to include provisions for tree canopy cover, green infrastructure, reduced car parking and end-of-trip facilities – all of which will result in positive environmental effects.

Overall, the proposed regime of FSR and bonus incentives, as well as the general provisions of the assessment process, will ensure that any environmental effects are appropriately managed and mitigated.

**Q9. Has the planning proposal adequately addressed any social and economic effects?**

The Planning Proposal will result in net positive social and economic effects as it seeks to facilitate 1516 new dwellings (including affordable housing) and 1944 new jobs. Notwithstanding these new residents, workers and visitors will generate demand for schools and tertiary education facilities as well as increased usage of sporting and recreation facilities and community infrastructure.

Council is committed to delivering coordinated land-use development and infrastructure outcomes in the LGA. To inform the development of the Inner West Local Infrastructure Contributions Plan 2023, Council undertook a number of studies to ascertain the current and projected needs of the community with regard to recreation, community assets and traffic and transport.

In relation to State infrastructure, the Planning Proposal includes provisions which require that new development makes satisfactory State/ Regional Infrastructure Contributions.

These measures will ensure that any potential social and economic effects of this Planning Proposal from the proposed levels of growth are adequately managed.

**Section D – State and Commonwealth interests**

**Q10. Is there adequate public infrastructure for the planning proposal?**

There is currently insufficient public infrastructure to support the proposed levels of growth. There are specific concerns regarding traffic congestion and amenity along Parramatta Road.

Accordingly, the Planning Proposal includes provisions to support new development (above the existing LEP controls) subject to provision of State Infrastructure Contributions for:

- i. State and regional roads
- ii. bus interchanges and bus lanes
- iii. land required for regional open space
- iv. social infrastructure and facilities (such as schools, hospitals, emergency services and justice purposes)
- v. light rail infrastructure.

Council will also engage with relevant State agencies as required by the Gateway Determination regarding the provision of public infrastructure in this Corridor.

The full range of utility services including electricity, telecommunications, water and sewer are all currently available across Inner West. It is expected that these services will be upgraded where required as individual development occurs.

From 1 October 2023, Housing and Productivity Contributions will be phased in for Greater Sydney and other high growth areas of NSW. These will be payable for all development types that increase dwelling numbers or provide additional commercial, retail or industrial floorspace, with the exception of public housing, seniors housing affordable housing and

secondary dwellings. Contribution rates in Greater Sydney will be \$10,000/dwelling for residential units, \$15/sqm of new industrial floorspace and \$30/sqm of new commercial or retail floorspace.

Contributions to the Housing and Productivity Fund will support delivery of State and regional infrastructure such as schools, hospitals, regional open space and transport. Funds will be spent in line with an Infrastructure Opportunities Plan which will set out 0–20-year growth expectations and infrastructure opportunities. Housing and Productivity Contributions will be paid in addition to local infrastructure contributions.

The abovementioned measures alongside Council's local infrastructure provisions will ensure that adequate public infrastructure is available to support the proposed levels of growth.

**Q11. What are the views of state and Commonwealth public authorities consulted in accordance with the Gateway determination?**

Council received Gateway Determination on 20<sup>th</sup> October 2022, which advised a list of public authorities that are to be consulted with as part of the Planning Proposal process. Public authority consultation will be undertaken concurrently with community consultation.

- Ausgrid;
- Adjoining Councils;
- Greater Cities Commission;
- Commonwealth Department of Infrastructure, Transport, Regional Development and Communications;
- Department of Education;
- Environment and Heritage Group of the Department of Planning and Environment;
- Environment Protection Agency;
- Heritage NSW;
- Jemena;
- NSW Health;
- State Emergency Service;
- Sydney Airport Corporation;
- Sydney Metro;
- Sydney Trains;
- Sydney Water Corporation; and
- Transport for NSW.

**Part 4 – Mapping**

The Planning Proposal seeks to make the following amendments to IWLEP2022 maps:

- Amend existing Land Use Zoning Maps
- Amend existing Heritage Maps
- Amend existing FSR map for 2 Hay Street, Leichhardt
- Create new Floor Space Ratio Incentives Maps
- Create new Height of Buildings Incentives Maps
- Create new Key Sites Maps
- Create new Active Frontages Maps
- Create new Land Use and Transport Integration Maps
- Create new Additional Permitted Uses Maps
- Create new Opportunity Sites Maps

Appendix 1 provides the existing and proposed IWLEP 2022 maps.

### Part 5 – Community Consultation

Extensive consultation was undertaken with the DPE prior to Gateway which informed the amendments sought in the Planning Proposal.

Preliminary engagement was also undertaken with Sydney Airport Corporation Limited and Commonwealth Department of Infrastructure, Regional Infrastructure and Communications. Comments from these agencies is provided in Q11 and under MD5.3.

While the Gateway determination requires that the Planning Proposal be publicly exhibited for a minimum of 28 days, Council has decided to make it available for 42 days.

Consultation will include:

- Inner West Council's Yoursay website
- Letters to owners and occupiers of affected properties and in the surrounding area
- Notification to public authorities
- Social Media and other channels
- Precinct-specific community meetings
- In-person, precinct-specific, drop-in sessions
- Exhibition material available in Council Service Centres and libraries
- NSW Government Planning Portal

### Part 6 – Project timeline

In accordance with the requirements set out in *Local Environmental Plan Making Guideline 2021* (former 'A guide to preparing planning proposals' 2016) the table below outlines the anticipated project timeline of the planning proposal progress through the LEP plan making process.

Table 28 - Anticipated timeline for this Planning Proposal

Anticipated timeframe for planning proposal		
Action	Period	Date
Submit Planning Proposal to DPE for Gateway consideration	30 June 2022	30 June 2022
Anticipated Gateway Determination	July – August 2022	20 October 2022
Complete any changes required by Gateway Determination	October 2022 – September 2023	20 September 2023
Public exhibition & government agency consultation	November 2023 - December (6 weeks)	20 December 2023
Consideration of submissions	December 2023 – February 2024	28 February 2024
Council meeting to consider outcomes of	March 2024	12 March 2024

Planning Proposal: Parramatta Road Corridor Stage 1 – September 2023

exhibition (Date to be confirmed)		
Update LEP in line with Council resolution	March 2024	29 March 2024
Submission of Planning Proposal to the Secretary of DPE to arrange for the drafting of the updated LEP	March 2024	29 March 2024

## 5.0 List of Appendices

Appendix 1 – Proposed LEP Maps

Appendix 2 – Urban Design Studies

Appendix 2a: Leichhardt, Taverners Hill and Kings Bay/ Croydon Urban Design Review (Architectus, 2023)

Appendix 2b: Inner West Council's Draft Structure Plan October 2020

Appendix 2c: Leichhardt Council's Parramatta Road Urban Design Study 2016

Appendix 3 – Heritage Study and Inventory Sheets

Appendix 4 – Economic Assessment

Appendix 5 – Flood Impact Risk Assessment (WMS, 2023)

Appendix 6 – Preliminary Site Investigations

Appendix 7 – Aircraft Noise Assessment

Appendix 8 – Draft Affordable Housing Contribution Scheme

Appendix 9 – Parramatta Road Corridor Stage 1 Sustainability Study (WSP, 2023)

Appendix 10 – Parramatta Road Corridor Precinct-wide Traffic and Transport Study and its Implementation (Camperdown, Leichhardt, Taverners Hill and Kings Bay/ Croydon precincts)

Appendix 11 – Parramatta Road Corridor Implementation Stage 1 - Justification Study

Appendix 12 – Parramatta Road Urban Amenity Improvement Plan (Inner West Council Masterplan)

INNER WEST

**Development Control Plans**

**Parramatta Road Corridor Implementation Stage 1  
September 2023**



Item 14

Attachment 3

INNER WEST

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Part D Section 14 - Kings Bay Precinct.....124



DRAFT FOR EXHIBITION – NOVEMBER 2023

**LEP2A – Parramatta Road Corridor Stage 1 – Draft Development Control Plans–  
to Inner West Development Control Plans**

**Implementing PRCUTS – Stage 1**

Inner West Council is taking a staged approach to implementing the Parramatta Road Corridor Urban Transformation Strategy (PRCUTS). Stage 1 of this implementation is identified in Figure 1 and includes:

- The Inner West section of **Kings Bay Precinct**, being the subject of Part D, Section 13 of Inner West Comprehensive (Ashfield) DCP, 2016
- Specific locations in **Taverners Hill Precinct**, including:
  - Taverners Hill North: Tebbutt and Beeson Streets, being the subject of Part G, Section 13 of Leichhardt DCP 2013
  - Taverners Hill South: Old Canterbury Road and Thomas Street, being the subject of Part 9, Section 49 of Marrickville DCP 2011
- Specific locations in **Leichhardt Precinct**:
  - A focussed area centred on Norton Street, Balmain Road and Parramatta Road, being the subject of Part G, Section 14 of the Leichhardt DPC 2013
  - A focussed area centred on Parramatta Road and Crystal Street, being the subject of Part 9, Section 9.50 of Marrickville DCP 2011.



Figure 1: Parramatta Road Corridor Precincts in the Inner West and Stage 1 Implementation locations

DRAFT FOR EXHIBITION – NOVEMBER 2023

Schedule of Amendments

Inner West Comprehensive (Ashfield) DCP 2016

Section/Chapter	Amendment to	Proposed amendment
Contents	Update existing contents list	Update to include new Section 13 – Parramatta Road Corridor Kings Bay Precinct to Part D – Precinct Guidelines
<b>Section 1: Preliminary, Chapter A: Preliminary</b>		
Order of Priority for Applying Guidelines	Modify existing wording –  General controls have priority over Precinct Specific Controls or Development Category Controls.	Minor change: Update to reinforce “Plain English” content. Proposed to reword as follows: <i>General Controls detailed in Part A – Miscellaneous, Part B – Public Domain, Part C – Sustainability, Part E1 – Heritage Items and Conservation Areas and Part F – Development Category Controls, supplement Part D – Precinct Controls. Where Part D – Precinct Controls are silent, Controls in relevant Parts of the DCP prevail.</i>
<b>Section 2: General Guidelines, Chapter D: Precinct Guidelines</b>		
Part 6 – Enterprise Zone (B6) Parramatta Road	Modify existing wording –  This Guideline applies to the following development categories:  All development along Parramatta Road generally zoned E3 Productivity Support under the Inner West LEP 2022.	Update Application to exclude Area 1 North by rewording as follows:  <i>This Guideline applies to all development along Parramatta Road generally zoned E3 Productivity Support under the Inner West LEP 2022, excluding E3 Productivity zoned land identified as Area 1 North when seeking to rely on Incentive Floor Space Ratio Map, Incentive Height of Buildings Map and Clause XX of the Inner West LEP 2022. In this circumstances Part 13 Parramatta Road Corridor – Kings Bay Precinct of this DCP prevails over this Part.</i>
Part 13 – Parramatta Road Corridor – Kings Bay Precinct	Nil – new content	New Section to include Parramatta Road Corridor – Kings Bay Precinct
<b>Section 2: General Guidelines, Chapter G: Definitions</b>		
Definitions	Update to include new definitions	Include the following new definitions: <i>Through-site link</i> means a 24 hour publicly accessible walking, cycling or other mobility aid link between two streets and is registered on title as an easement.
<b>Section 2: General Guidelines, Chapter H: Amendments</b>		
Amendment No. 8	Update schedule to include proposed new content	Add to Chapter D Precinct Guidelines: <ul style="list-style-type: none"> <li>by inclusion of Part 13 – Parramatta Road Corridor – Kings Bay Precinct, and</li> <li>associated amendments to Part 6 – B6 Enterprise Corridor and Chapter G – Definitions.</li> </ul>

DRAFT FOR EXHIBITION – NOVEMBER 2023

### Leichhardt Development Control Plan 2013

Section/Chapter	Existing content	Proposed amendment
Amendment Schedule	Update schedule to include proposed new content	Amendment to include: Part G – Site specific controls to include: <ul style="list-style-type: none"> <li>Section 13 – Parramatta Road Corridor – Taverners Hill Precinct</li> <li>Section 14 – Parramatta Road Corridor – Leichhardt Precinct</li> </ul>
Contents	Amend existing	Amend to include Part G – Site specific controls for: <ul style="list-style-type: none"> <li>Section 13 – Parramatta Road Corridor – Taverners Hill Precinct</li> <li>Section 14 – Parramatta Road Corridor – Leichhardt Precinct</li> </ul>
<b>Part G: Site Specific Controls</b>		
Contents	Amend existing	Amend to include: <ul style="list-style-type: none"> <li>Section 13 – Parramatta Road Corridor – Taverners Hill Precinct</li> <li>Section 14 – Parramatta Road Corridor – Leichhardt Precinct</li> </ul>
Section 1 – Site Specific Control Overview	Amend existing	Amend Figure G1 to include Area 13 – Parramatta Road Corridor – Taverners Hill Precinct and Area 14 – Leichhardt Precinct
C2.2.3.5 Leichhardt Commercial Distinctive Neighbourhood	Amend existing	Amend C2.2.3.5 by inserting before Figure C82: <i>The Leichhardt Precinct – Parramatta Road Corridor lies within the Leichhardt Commercial Distinctive Neighbourhood. The new desired future character for the Leichhardt Precinct is included in Part G, Section 13 and is compatible with the adjoining neighbourhoods in the Leichhardt Commercial Distinctive Neighbourhood.</i>
Section 13 – Parramatta Road Corridor – Taverners Hill Precinct	Nil – new content	New Section to include Parramatta Road Corridor – Taverners Hill Precinct
Section 14 – Parramatta Road Corridor – Leichhardt Precinct	Nil – new content	New Section to include Parramatta Road Corridor – Leichhardt Precinct
Appendix A – Glossary	Update schedule to include new definitions	<i>Through-site link</i> means a 24 hour publicly accessible walking, cycling or other mobility aid link between two streets and is registered on title as an easement.

### Marrickville Development Control Plan 2011

Section/Chapter	Existing content	Proposed amendment
Amendment Schedule	Update schedule to include proposed new content	Amendment to include: Part 9 – Strategic Context to include: <ul style="list-style-type: none"> <li>9.49 – Parramatta Road Corridor – Taverners Hill Precinct</li> <li>9.50 – Parramatta Road Corridor – Leichhardt Precinct</li> </ul>
Contents	Amend existing	Amend to include Chapter 9 – Strategic Context for: <ul style="list-style-type: none"> <li>9.49 – Parramatta Road Corridor – Taverners Hill Precinct</li> <li>9.50 – Parramatta Road Corridor – Leichhardt Precinct</li> </ul>
<b>Part 9 – Strategic Context</b>		
Contents	Amend existing	Amend to include: <ul style="list-style-type: none"> <li>9.49 – Parramatta Road Corridor – Taverners Hill Precinct</li> <li>9.50 – Parramatta Road Corridor – Leichhardt Precinct</li> </ul>

Draft Development Control Plan – Cover Sheet

3

DRAFT FOR EXHIBITION – NOVEMBER 2023

Section/Chapter	Existing content	Proposed amendment
Part 9 Introduction	Amend existing	<p>Amend to include:</p> <ul style="list-style-type: none"> <li>9.49 – Parramatta Road Corridor – Taverners Hill Precinct</li> <li>9.50 – Parramatta Road Corridor – Leichhardt Precinct</li> </ul> <p>Include an update to number of planning precincts in the preamble.</p> <p>Amend map illustrating location of all precincts to include 9.49 and 9.50 precincts. Map included in version online. <a href="#">Marrickville DCP 2011 - 9.0 Precincts Map.pdf</a> Map to show Precinct 50 as being within the boundary of Precinct 35 (as per Precinct 48)</p>
9.1 – Lewisham North (Precinct 1)	Amend existing	<p>Update Application to exclude Parramatta Road Corridor – Taverners Hill Precinct, Area 2 – Taverners Hill South, Old Canterbury Road and Thomas Street by including:</p> <p>After Map of Precinct: Include the following wording – <i>When seeking to rely on Incentive Floor Space Ratio Map, Incentive Height of Buildings Map and Clause XX of the Inner West LEP 2022 on land identified in blue as Masterplan Sites, Part 9.49 Parramatta Road Corridor – Taverners Hill Precinct prevails of this Section of the DCP.</i></p>
9.35 – Parramatta Road (Commercial Precinct 35)	Amend existing	<p>Update Application to exclude Parramatta Road Corridor – Leichhardt Precinct, Area 2 – Parramatta Road Street by including:</p> <p>After Map of Precinct: Add a sub-heading – Exclusions to Parramatta Road (Commercial Precinct 35) Include a map showing Parramatta Road Corridor, Leichhardt Precinct, Area 2 – Parramatta Road.</p> <p>Include the following wording – <i>When seeking to rely Inner West LEP 2022 Incentive Maps and Clause XX on land identified as Area 1 – Crystal Street and Area 2 – Parramatta Road in 9.50 Parramatta Road Corridor – Leichhardt Precinct prevails over this Section of the DPC where there is any inconsistency.</i></p>
9.49 – Parramatta Road Corridor – Taverners Hill Precinct	Nil – new content	New Section to include Parramatta Road Corridor – Taverners Hill Precinct
9.50 – Parramatta Road Corridor – Leichhardt Precinct	Nil – new content	New Section to include Parramatta Road Corridor – Leichhardt Precinct
<b>Part 10 – Definitions</b>		
	Update schedule to include new definitions	<i>Through site link</i> means a 24 hour publicly accessible walking, cycling or other mobility aid link between two streets and is registered on title as an easement.

Part G – Site Specific Controls

## Parramatta Road Corridor – Leichhardt Precinct

### 14.1. Application

Part G, Site Specific Controls, Section 14, Parramatta Road Corridor: Leichhardt Precinct applies:

- to that part of Leichhardt Precinct shown in **Figure 1: Parramatta Road Corridor: Leichhardt Precinct Land Application Map** as Area 1, Area 3, Area 4 and Area 5, and
- where development seeks to rely on the Incentives Floor Space Ratio Map, Incentives Height of Buildings Map and meets Clause X.X of the Inner West LEP 2022.

Where development does not seek to rely on the Incentives provisions, Part G, Section 14 does not apply. In this circumstance, relevant provisions of this DCP apply.

The Leichhardt Precinct has five Areas that have varying functions and intended outcomes. As detailed above, this Section applies to five of those Areas:

- Area 1 – Parramatta Road
- Area 3 – Norton Street East
- Area 4 – Norton Street Opportunity Site
- Area 5 – Norton Street North
- Area 6 – Balmain Road

Where seeking to rely on incentive provisions, all development will achieve the Desired Future Character, Objectives and Controls detailed in:

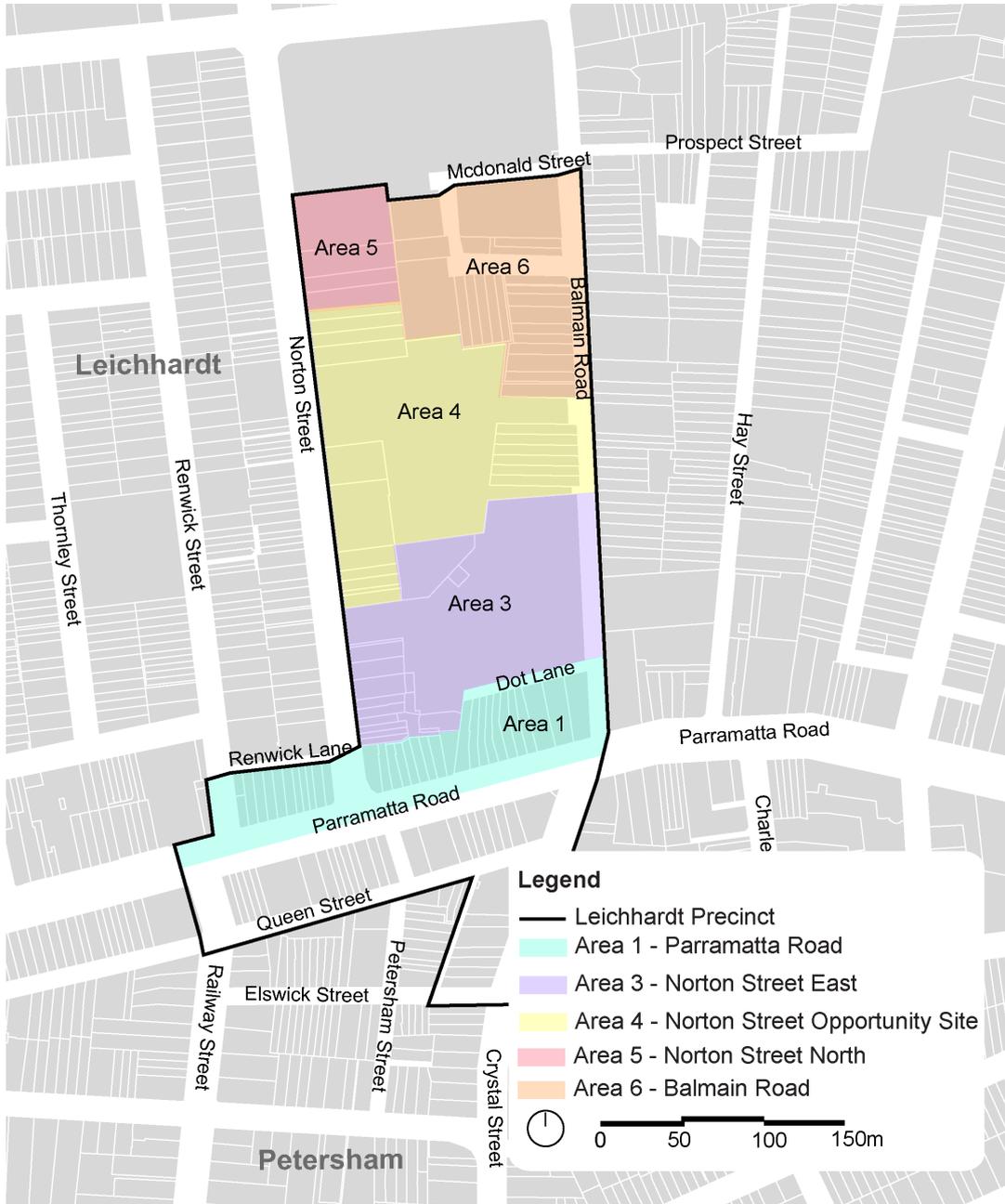
- **Section 14.3** that applies to all Areas in the Leichhardt Precinct, and as applicable
- **Section 14.4** that applies to Area 1 – Parramatta Road, or
- **Section 14.5** that applies to Area 3 – Norton Street East, or
- **Section 14.6** that applies to Area 4 – Norton Street Opportunity Site
- **Section 14.7** that applies to Area 5 – Norton Street North
- **Section 14.8** that applies to Area 6 – Balmain Road.

These sections supplement and should be read in conjunction with relevant provisions of this DCP.

Where Part G Section 14 applies and there is an inconsistency between this Section and the other provisions of this DCP, this Section prevails.

Part G – Site Specific Controls

Figure 1: Parramatta Road Corridor: Leichhardt Precinct Land Application Map



Part G – Site Specific Controls

## 14.2. Context

### Parramatta Road Corridor Urban Transformation Strategy

Parramatta Road Corridor - Leichhardt Precinct is one of eight Precincts of the Parramatta Road Corridor Urban Transformation Strategy (PRUCS). PRUCS is the NSW Government's 30-year plan setting out how the Parramatta Road Corridor will grow and bring new life to local communities living and working along the Corridor.

The vision for Parramatta Road Corridor is:

**A high-quality multi-use corridor with improved transport choices, better amenity and balanced growth of housing and jobs.**

The vision is supported by seven principles:

**1. Housing choice and affordability**

Plan for diversity in housing types to accommodate a wide range of community needs, including affordable, family, student and seniors housing.

**2. Diverse and resilient economy**

Plan for and position the corridor to attract new businesses and support existing business that create a diversity of jobs and promote jobs closer to home.

**3. Accessible and connected**

Reshape and better connect places and movement networks to better serve customers and encourage sustainable travel.

**4. Vibrant community places**

Promote quality places and built form outcomes to transform the corridor over time.

**5. Green spaces and links**

Embellish existing open space and provide for new open spaces that support the recreational needs of the community and encourage active and healthy lifestyles.

**6. Sustainability and resilience**

Create liveable local Precincts along the corridor that are sustainable, resilient and make Sydney a better place.

**7. Delivery**

Deliver, drive, facilitate and monitor action.

### PRUCS and Inner West

Four of the eight PRUCS Precincts are within Inner West Council local government area. These include:

- Part of **Kings Bay/ Croydon Precinct** in Croydon that extends from Lang Street, Croydon in the west to Iron Cove Creek in the east. The remaining areas of Kings Bay/ Croydon Precinct are in Burwood and Canada Bay local government areas.
- **Taverners Hill Precinct** that extends from Petersham in the east to Summer Hill in the west and includes areas in Leichhardt and Lewisham.
- **Leichhardt Precinct** in the suburbs of Leichhardt and Petersham.
- Part of **Camperdown Precinct**. The remaining area of Camperdown Precinct is in City of Sydney local government area.

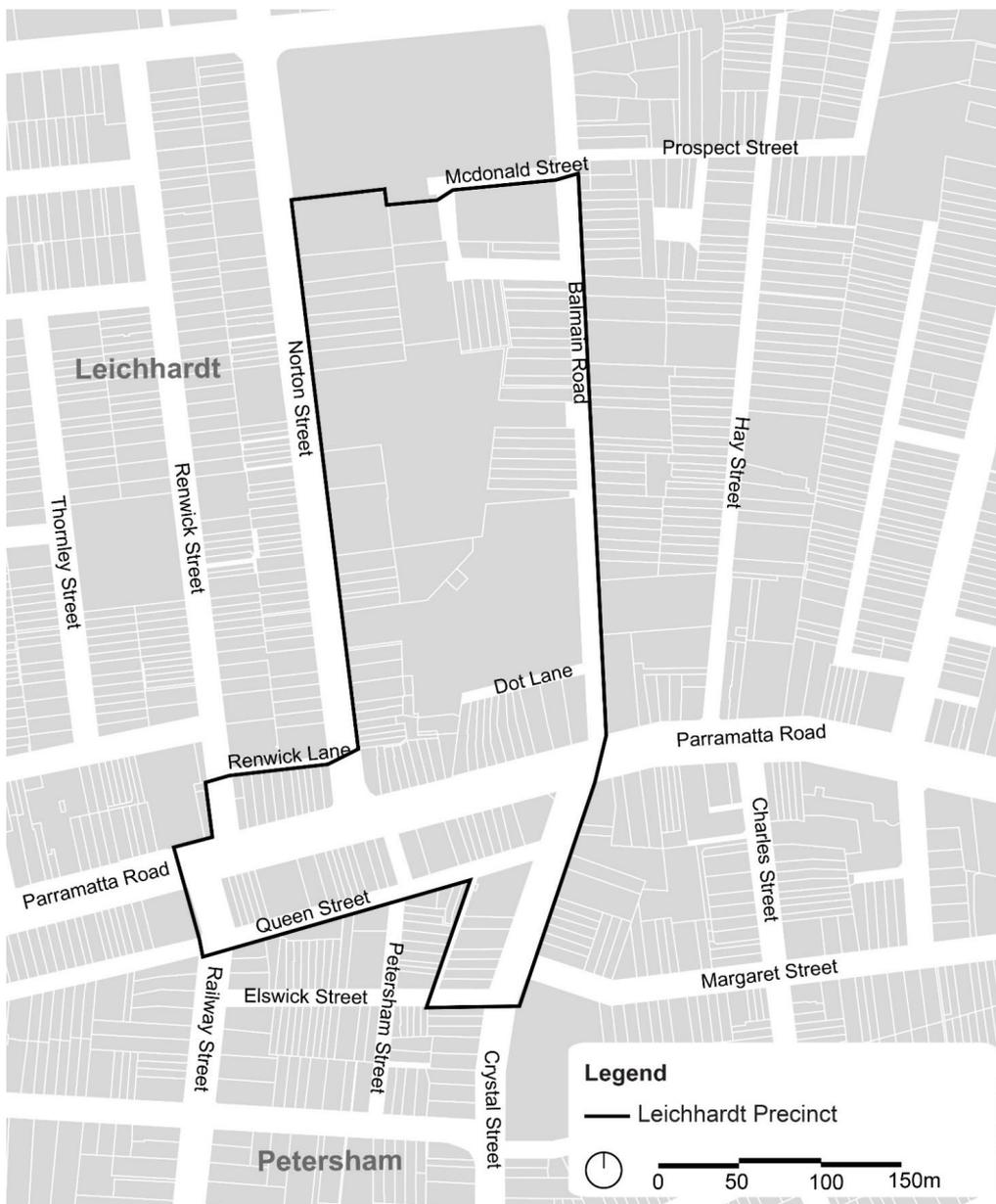
Part G – Site Specific Controls

14.3. Leichhardt Precinct

14.3.1. Application

Section 14.3 applies to the entire Leichhardt Precinct as identified in Figure 2.

Figure 2: Parramatta Road Corridor: Leichhardt Precinct



Draft Section 14 – Leichhardt Development Control Plan

Part G – Site Specific Controls

**14.3.2. Leichhardt Precinct Desired Future Character**

- Norton Street is a strong vibrant and bustling activity strip that creates a sense of community and is supported by increased residential density.
- Parramatta Road provides affordable small-scale retail and employment premises and where new development respects heritage and the fine grain character of the streetscape.
- Housing within the Precinct is well located and diverse, serving the needs of people of all ages, abilities and incomes.
- Busy pedestrian routes provide access to and from key destinations within the Precinct and new east-west pedestrian connections breaking down large blocks on Norton Street.
- People enjoy a public domain that is well-designed, activated and landscaped.
- The iconic views and vistas along Parramatta Road and north-south streets, of historic landmarks at street junctions and glimpses to the city skyline remain.
- Living and work environments are sustainable and comfortable as a result of:
  - buildings having a high standard environmental performance
  - integrated water management
  - building design, landscape and materials reducing urban heat effects
  - building design reducing noise and air quality improvements
  - promoting active and public transport
  - catering for electric charging infrastructure.
- The built form is high quality, suitably scaled, transitions to neighbouring areas and meets the needs of intended uses.
- Taller buildings are concentrated between Norton Street and Balmain Road which protects the lower scale and well recognised streetscape along Norton Street and Parramatta Road.
- Residents benefit from building design that maximises their amenity while protecting the amenity of nearby developments.
- Community facilities and civic services such as library, town hall and schools support the Precinct.
- Pedestrians and bike riders benefit from:
  - enhanced connections across Parramatta Road and along Railway Street to Petersham Station
  - increased east-west permeability
  - safe cycling connections north-south.
- Reliance on private vehicles has reduced to support sustainable living through:
  - reducing on-site car parking provision for origin and destination locations
  - setting maximum car parking rates instead of requiring minimum car parking
  - implementing new models such as unbundled parking and shared car use
  - leveraging proximity to public transport networks, including rapid transport on dedicated lanes on Parramatta Road.

**14.3.3. Connectivity and accessibility**

**Objectives**

- O1. To increase connectivity, permeability and accessibility across the Precinct for pedestrians and bike riders by enhancing links between workplaces and residential areas to key locations.
- O2. To enhance local connectivity between Norton Street and Balmain Road.

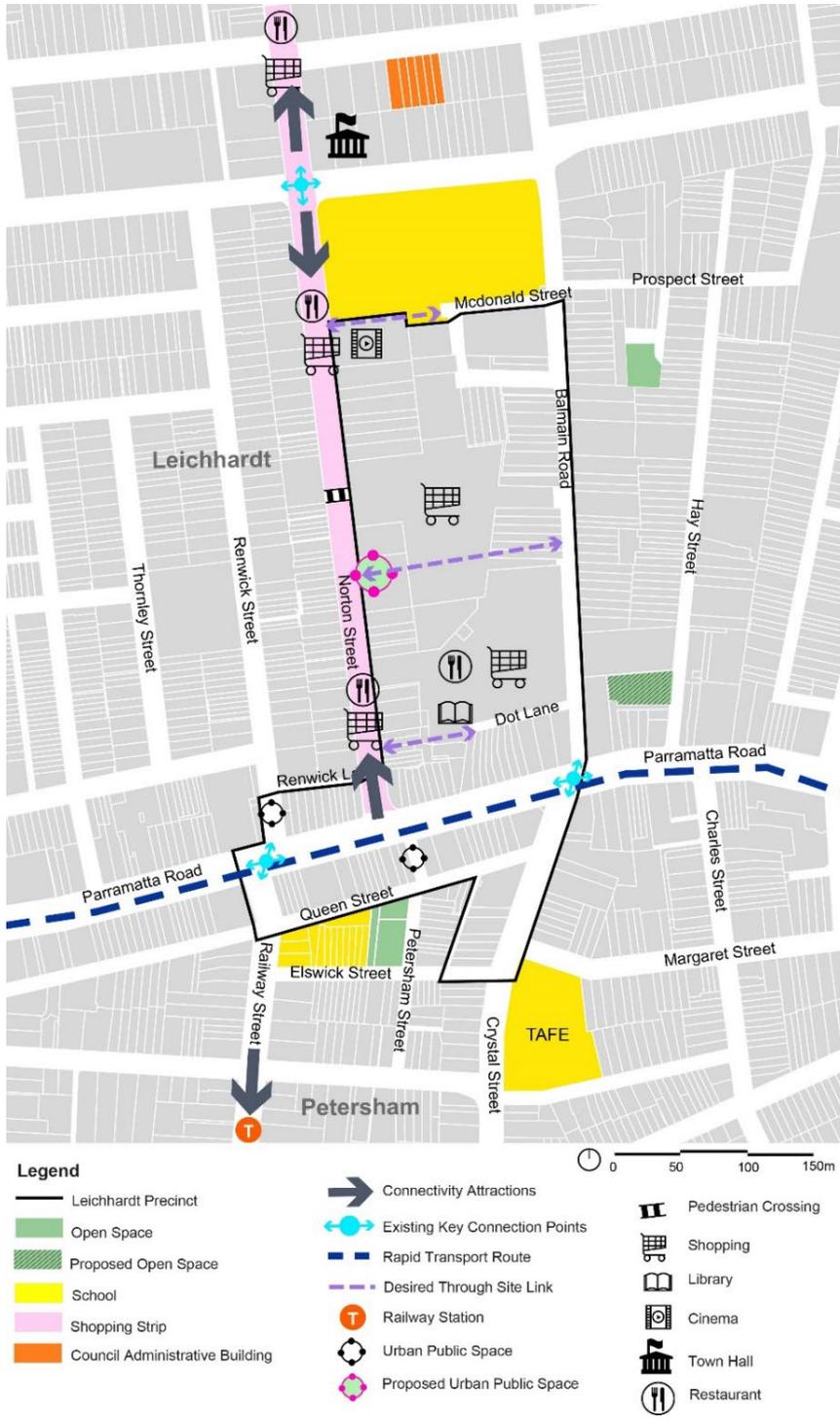
Part G – Site Specific Controls

**Controls**

- C1. Built form and streetscape treatments reinforce pedestrian and cycling connections identified in Figure 3: Leichhardt Precinct connectivity and accessibility map, including:
  - a. north-south connections between the Precinct and Petersham Train Station
  - b. east-west connections to improve permeability between Balmain Road and Norton Street:
    - i. a midblock through-site link through the Opportunity site/ Norton Plaza
    - ii. a through site link connecting Dot Lane to Norton Street
    - iii. a through site link connecting McDonald Street to Norton Street.
- C2. Where a desired through-site link is identified on Figure 3, lot amalgamation and development contribute to mid-block connections to increase connectivity between Norton Street and Balmain Road.

Part G – Site Specific Controls

Figure 3: Leichhardt Precinct connectivity and accessibility map



Draft Section 14 – Leichhardt Development Control Plan

Part G – Site Specific Controls

### 14.3.4. Streetscape and public domain

#### Objectives

- O3. To improve the amenity and safety of the streetscape of the Precinct in a manner that:
  - a. contributes to the street character and intended land uses
  - b. is supported by built form that interfaces well with the streetscape and heritage
  - c. reduces street clutter and improves the visual amenity of the public domain
  - d. reinstates or upgrades the footpath to provide enhanced public domain
  - e. protects existing street trees
  - f. positively contributes to water management and is waterwise
  - g. results in a durable and low maintenance public domain.

#### Controls

- C3. Development contributes towards enhancing the streetscape for the extent of the street frontage by:
  - a. providing required built form setbacks and a built form that interfaces well with the streetscape
  - b. ensures the pedestrian movement area is clear of obstacles
  - c. integrates pedestrian entries into the streetscape design

Notes:

- 8. Refer to *Inner West Public Domain Design Guide (202X)* for details of road types, footpath area functions and finishes.
- 9. Refer to Controls related to built form and landscaping as detailed in Sections 14.4, 14.5, 14.6, 14.7 and 14.8 as relevant to each Area within the Precinct.

### 14.3.5. Development utility infrastructure

#### Objectives

- O4. To reduce street clutter, provide opportunity for viable street trees and enhance the public domain.
- O5. To locate and design mechanical plant and essential services in a way that:
  - a. improves the visual amenity of the public domain
  - b. does not conflict with landscaping or street tree planting
  - c. is located outside the public domain.

#### Controls

- C4. Relocate existing overhead cables underground, and where possible, co-locates with other underground services.
- C5. Mechanical plant and essential services equipment are:
  - a. contained wholly within the property
  - b. located off the primary street frontage, or
  - c. where on the primary street frontage are located behind the building line and screened from view
  - d. integrated with the building and landscape design.

Part G – Site Specific Controls

**14.3.6. Affordable housing**

**Objectives**

- O6. To increase the supply of well-designed affordable housing in the Inner West to meet community needs and in appropriate locations across Leichhardt Precinct.
- O7. To ensure affordable housing is managed and retained in perpetuity.

**Controls**

- C6. Affordable housing units:
  - a. include a range of sizes to cater for different household sizes
  - b. are designed and constructed to the same standard as other residential accommodation in the development
  - c. are distributed throughout the development with a unit mix determined by Council in consideration of affordable housing need and social inclusion.
- C7. Affordable housing units are to be provided and managed in accordance with the relevant Affordable Housing Contributions Scheme and Council’s Affordable Housing Policy.

Notes:

- 10. Affordable housing has a statutory definition under the NSW Environmental Planning and Assessment Act 1979 of “housing for very low-income households, low income households or moderate income households, being such households as are prescribed by the regulation or are as provided for in an environmental planning instrument.”
- 11. Refer to *Inner West Affordable Housing Policy 2022* for details of Affordable housing requirements.

**14.3.7. Lot amalgamation**

**Objectives**

- O8. To promote efficient use of land and orderly redevelopment by:
  - a. avoiding isolating lots and reducing development potential
  - b. providing intended uses and built form outcomes that make a positive contribution to the streetscape.

**Controls**

- C8. Lot amalgamation:
  - a. does not result in isolated lots that are impractical for redevelopment to the scale and intensity desired for the area
  - b. combines narrow lots and lots in fragmented ownership.

**14.3.8. Sustainability and resilience**

**Objectives**

- O9. To achieve a high standard of environmental building performance that:
  - a. reduces greenhouse gas emissions and water use
  - b. results in comfortable living and working environments.

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- O10. To reduce urban heat island effects through incorporating and integrating mechanisms that collectively mitigate the impacts, including:
- a. green infrastructure in the form of landscape and surface treatments that incorporate water storage and treatment while reducing water usage
  - b. trees that offer shade to the built form, hard surfaces and vegetation
  - c. building materials and colours that reduce heat impacts, contribute to energy efficiency and thermal comfort, and minimise nuisance caused by glare or heat radiation.

**Controls**

- C9. Building Environmental Performance Report or BASIX certificate demonstrates that the development:
- a. achieves a reduction of greenhouse gas emissions and water use
  - b. results in comfortable living and working environments
  - c. includes passive design features such as optimal orientation, increased insulation, effective shading, cross ventilation and lower solar absorptance external surface finishes
  - d. optimises rooftop solar photovoltaic systems
  - e. achieves full electrification of utilities including cooking (other than in commercial kitchens), heating and hot water (heat pumps)
  - f. for residential development:
    - i. achieves an average thermal performance of 7-star NatHERS
    - ii. incorporates ceiling fans in bedrooms and living rooms.
- C10. Mitigate urban heat island effects by:
- a. achieving required tree canopy through:
    - i. site layout maximising retention of existing mature trees
    - ii. including advanced containerised trees (greater than 200 litre) of a species that within 10 years will achieve 50% of their potential at maturity
    - iii. incorporating trees and vegetation across various storeys (roof tops, terraces, atriums, and the like), in addition to, or where necessary, as an alternative to ground level planting
    - iv. a combination of the above that collectively achieves, or exceeds, the tree canopy requirements.
  - b. integrating green roofs and walls as a component of the landscape and built form design specifically in northern and western facing locations
  - c. incorporating permeable surfaces, rain gardens, and other water sensitive measures in landscape treatments
  - d. using materials and colours that:
    - i. have a high solar reflectance index (lighter colours) on roofs, facades, glazing or ground surfaces subject to their purpose and aligned to orientation and exposure to sunlight
    - ii. where it may cause nuisance due to glare or reflection do not exceed 20% reflectivity (darker colours).

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**14.3.9. Access and parking**

**Objectives**

- O11. To ensure developments reduce private motor vehicle use, minimise traffic impacts and encourage sustainable transport.
- O12. To enhance the public domain, improve pedestrian experience and safety, and limit the number of vehicle access points throughout the Precinct.
- O13. To reduce private vehicle ownership through unbundled parking, car share schemes and decoupled parking, where on-site car parking is provided.
- O14. To maximise efficient use of non-residential car parking by incorporating shared use of parking spaces subject to peak demand of various building uses.
- O15. To ensure development provides facilities for electric vehicles.
- O16. To future proof infrastructure to support increased take-up of electric vehicles.
- O17. To ensure vehicle parking, servicing and loading areas are designed to:
  - a. reduce their visual impact on the public domain
  - b. support all vehicle types anticipated by development including service vehicles and loading areas
  - c. maximise potential adaptation at a future point in time when less parking is required.
- O18. To ensure delivery areas prioritise servicing outside peak pedestrian activity for key streets.
- O19. To ensure bike riders have sufficient accessible and secure parking.
- O20. To provide on-site workers facilities for employment generating uses that encourage active transport commuting, healthy workplaces and cater for worker needs.

**Controls**

- C11. Travel plans are to include the following:
  - a. baseline travel demand and mode share estimates from established similar developments
  - b. targets for reduced private motor vehicle trips and an increased mode share for sustainable transport
  - c. actions to be implemented to achieve the mode shift targets, with a written commitment from the property owner and/or business operator to implement them
  - d. a process for monitoring and review of actions and targets
  - e. a guide for residents, employees and visitors associated with the development to assist with the mode shift
  - f. public transport subsidies for workers for commuter and for-work trips and parking charges for workers who commute by car and/or payments to employees who don't
  - g. on-site carshare schemes and memberships, and priority parking for multiple occupancy vehicles, e.g. employees who car pool
  - h. subsidised bicycle purchase and quality bicycle parking and associated end-of-trip facilities
  - i. provision of peak period shuttle buses, relocation allowances and flexible working hours.
- C12. Vehicular access is located to:

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- a. use secondary streets or rear accessways and laneways
- b. consolidate vehicle access to reduce the number of crossovers through a maximum of one driveway per site or one-way pair.

Note: Refer to additional Controls relevant to specific Areas within the Precinct in Sections 14.4, 14.5, 14.6, 14.7 and 14.8.

C13. Provision of private vehicle parking:

- a. is listed on a separate title (unbundled) from the development (i.e. separated from dwelling, commercial units and building ownership)
- b. is decoupled from the development, as relevant
- c. includes car share vehicle(s) that:
  - i. are located either on-site or on the street at the discretion of Council
  - ii. do not result in the maximum car parking rates being exceeded
  - iii. are publicly available and readily accessible at all times.

Notes:

1. **Unbundled parking** means parking that is separated from the cost or rent of a dwelling, commercial units and building ownership.
  2. **Car share scheme** means a scheme in which any car share operator provides vehicles for shared use and hires those vehicles exclusively to members of the scheme for occasional use for short periods of time, on demand and on a pay-as-you go basis.
  3. **Decoupled parking** means provision of off-site car parking, usually in the form of consolidated car parking in close proximity to the development to satisfy the parking requirements.
- C14. Where shared use of car parking spaces is included, they are determined on a case-by-case basis dependant on anticipated tenancies/uses.
- C15. Provide electric vehicle (EV) ready to use (including cabling, power outlet or charging head) car parking spaces:
- a. for non-residential development – Level 3, or faster, at a rate of 10% for all spaces – dedicated and visitor
  - b. for residential development – Level 1, or faster, at a rate of:
    - i. 20% for resident spaces
    - ii. 10% for visitor spaces.
- C16. Design electric infrastructure services (distributions boards, conduits and cables) to ensure:
- a. sufficient energy and capacity, preferably from renewable sources
  - b. reticulated fixed charging facilities cater for a minimum of:
    - i. for non-residential development – 50% of all parking spaces
    - ii. for residential development – 100% of all parking spaces.
  - c. any future EV charger does not require a cable of more than 50m from the parking space to the EV-ready connection.
- C17. On-site ground level exposed car parking is not provided, and parking areas:
- a. are concentrated below ground or sleeved by other uses
  - b. are not open structures that are visible from the public domain
  - c. where below ground:
    - i. do not protrude:
      - above ground level at any point along street frontages

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- into setbacks areas that are identified as landscape areas
    - ii. are designed to facilitate break out walls where required
  - d. do not impede the provision of viable vegetation
  - e. are designed to accommodate all vehicle types anticipated by the development
  - f. provide sufficient manoeuvring space to allow vehicles to enter and exit the site in a forward direction
  - g. designed in a manner that encourages opportunities for adaptation for other uses over time.
- C18. Service delivery times are restricted to avoid conflict with peak pedestrian periods on key streets, where practical.
- C19. Bicycle parking:
- a. complies with the minimum requirements detailed in Table 1
  - b. is in accessible and visible locations for residents, workers and visitors
  - c. is secure through provision of bike cages for residents and workers or bike stands for visitors
  - d. is provided with ready-to-use electric charging points at a minimum rate of:
    - i. for non-residential development - 1 per 4 bicycle spaces
    - ii. for residential development - 1 per 2 bicycle spaces
    - iii. where there are multiple parking areas, facilities are distributed equally across all locations.

**Table 1: Minimum bicycle parking**

Land Use	Resident/Worker	Visitor
Residential	1 space per dwelling	1 space per 10 dwellings
Commercial	1 space per 150m <sup>2</sup> GFA	1 space per 400m <sup>2</sup> GFA
Retail	1 space per 250m <sup>2</sup> GFA	2 spaces + 1 per 100m <sup>2</sup> GFA
Industrial	1 per 250m <sup>2</sup> GFA	1 space per 500m <sup>2</sup> GFA

- C20. On-site workers facilities:
- a. comply with the minimum requirements detailed in Table 2
  - b. are in secure locations
  - c. where more than 1 shower/change cubicle is required, separate and equal numbers of male and female facilities are provided.

**Table 2: Minimum worker facilities for all employment generating uses**

Land Use	Resident/Worker	Visitor
Residential	1 space per dwelling	1 space per 10 dwellings
Commercial	1 space per 150m <sup>2</sup> GFA	1 space per 400m <sup>2</sup> GFA
Retail	1 space per 250m <sup>2</sup> GFA	2 spaces + 1 per 100m <sup>2</sup> GFA
Industrial	1 per 250m <sup>2</sup> GFA	1 space per 500m <sup>2</sup> GFA

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Anticipated number of workers	Personal Lockers	Showers and change cubicles
0-49	1 per 2 workers	1 unisex
50 - 99	1 per 3 workers	2
100-199	1 per 4 workers	4
200+	1 per 5 workers	+ 1 per 200 workers

### 14.3.10. Heritage

#### Objectives

- O21. To ensure development:
- a. respects the significance of Heritage Items in the locality
  - b. in the vicinity of Heritage Items is designed and sited to minimise impacts on the significance of the item.

#### Controls

- C21. To ensure development responds to historic built form in the locality by:
- a. for Heritage Items – conserving and enhancing the significance, character, fabric and features of these buildings and conforming with the Burra Charter
  - b. for all other buildings – respects the items by:
    - i. appropriately siting and designing new development
    - ii. ensuring new development does not physically overwhelm or dominate the items
    - iii. using sympathetic materials, colours and finishes that reflect and harmonise with original materials to maintain the character of the items.

### 14.3.11. Active street frontages

#### Objectives

- O22. Active street frontages are provided to reinforce the vitality and liveliness of the public domain.
- O23. Active street frontages are provided:
- a. with ground floor frontages being pedestrian orientated and of a high design quality to add vitality to streets
  - b. by incorporating frequent pedestrian entries that open towards the street.

#### Controls

- C22. Provide active street frontages by including the following uses at street level:
- a. shops, commercial premises and other employment uses
  - b. commercial and residential lobbies and reception areas
  - c. public buildings or community facilities.
- C23. Active street frontages contribute to the liveliness and vitality of streets by:
- a. providing a minimum of 70% of the ground floor frontage as transparent glazing with an unobstructed view from the adjacent footpath to at least a depth of 6m within the building

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- b. maximising entries, display windows, customer service areas and key activities to provide pedestrian interest and interaction
- c. minimising blank walls, fire escapes, service doors, plant and equipment hatches
- d. providing elements of visual interest, such as display cases, or creative use of materials where fire escapes, service doors and equipment hatches cannot be avoided
- e. providing a high standard of finish and appropriate level of architectural detail for building facades
- f. providing passive surveillance to enhance safety and security
- g. providing ground floor pedestrian entry at the same level as the street to maximise accessibility for all users
- h. not including driveways and service entries
- i. if including security measures, using grilles or screens that are fitted internally and are a minimum of 60% perforated/transparent when closed.

**14.3.12. Built form**

**Objectives**

- O24. To provide for a high quality and well-designed built form that:
  - a. strengthens the urban character and identity of the Precinct
  - b. supports intended land uses
  - c. promotes a positive image for businesses
  - d. is of a bulk and scale and has site layout that complements the local context
  - e. minimises adverse amenity impacts
  - f. enhances the public domain for pedestrians
  - g. incorporates lighting that contributes to the quality and safety of the night-time urban environment, is sustainable and easy to maintain
  - h. does not create nuisance or hazard from glare, noise and odour for pedestrians, motorists, or occupants of nearby buildings.
- O25. To ensure development for residential purposes achieves a high-quality living environment and mitigates urban hazards by taking an integrated and innovative approach to:
  - a. address road and aircraft noise, and air quality impacts
  - b. the orientation of development and individual dwellings
  - c. minimise the need for mechanical ventilation and heating or cooling
  - d. protect and enhance the amenity of nearby residential development.
- O26. To provide appropriate employment uses on the ground floor in mixed used development that:
  - a. are compatible with the residential uses above
  - b. are separated from residential uses through subdivision
  - c. safeguard the provision and viability of business uses
  - d. provide large floor plates and high ceilings to ensure functionality and flexibility in accommodating a diverse range of business uses.

**Controls**

- C24. Building design:
  - a. includes architectural features and façade articulation to reduce apparent building bulk
  - b. emphasises building corners at intersections

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- c. does not result in overshadowing or loss of privacy
  - d. locates pedestrian entries:
    - i. on the primary street frontage and is visible from the street
    - ii. at the same level as the street to maximise accessibility for all users
  - e. where incorporating external lighting it:
    - i. is integrated into the building design and highlights distinctive architectural features
    - ii. is energy efficient, high quality, durable and low maintenance
    - iii. does not cause nuisance or hazard to occupants of the building or nearby buildings
    - iv. minimises light spill into the night sky
    - v. supports street lighting to enhance safety and security.
    - vi. negates adverse noise and odour emissions from activities, plant or equipment.
- C25. Residential development results in comfortable and enjoyable internal environments through:
- a. meeting the required standards for residential development near busy roads
  - b. ensuring buildings are designed to achieve internal noise levels as detailed in AS 2021
  - c. using a variety of integrated built form design, construction techniques and acoustic solutions to ameliorate negative impacts including but not limited to:
    - i. materials and glazing choices
    - ii. angled walls and modulated surfaces
    - iii. solid balconies and winter gardens
    - iv. screens, louvres and hopper windows
    - v. locating single aspect dwellings away from the north and west street frontages
    - vi. incorporating light wells, atriums and internal articulation to enhance sun capture and air movement
  - d. retaining privacy and solar access while improving noise impacts for nearby residential development.
- C26. Building design facilitates employment uses on the ground floor:
- a. are compatible with residential uses
  - b. activate the street frontage
  - c. provide suitable floor plates
    - i. limit ground floor use for services, storage and other business needs, and where required locate these to the rear of the building
    - ii. are larger in scale and designed to provide flexibility to adapt to different uses.
  - d. include a stratum subdivision scheme to delineate land use separation, ownership structures and obligations to the overall building regarding requiring owners' corporation consent for the submission of development applications and complying development certificates for employment uses separate from residential uses.

**14.3.13. Building materials and finishes**

**Objectives**

- O27. To provide building materials, fittings and finishes that are high are high-quality, sustainable and complement the locality.
- O28. To reduce building waste by effectively re-using or recycling building materials where demolition or deconstruction of existing development is required to facilitate new development.

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**Controls**

- C27. Building materials, fittings and finishes:
  - a. are durable, of high-quality and textured, to complement materials used in nearby buildings
  - b. on facades have a light reflectivity of 20% or less
  - c. are sustainable with low embodied carbon such as:
    - i. replacement of Portland cement with supplementary cementitious materials (SCMs) in concrete (i.e., 30% SCM across all pre-cast and in-situ cement)
    - ii. high recycled content in steel
    - iii. timber framing instead of steel framing
    - iv. cross laminate timber
  - d. incorporate recycled materials, where possible.
  
- C28. The Deconstruction Plan demonstrates that the majority of demolished building materials, excluding hazardous materials, are integrated into the design and construction of development by re-using on-site or through appropriate recycling.

**14.3.14. Landscaping**

**Objectives**

- O29. To ensure on-site landscaping:
  - a. includes species native to the area
  - b. is suited to the location
  - c. provides habitat to enhance biodiversity
  - d. positively contributes to water management and is waterwise
  - e. contributes to mitigating urban heat
  - f. is durable and low maintenance.

**Controls**

- C29. The Landscaping Strategy demonstrates, landscape:
  - a. is provided in dedicated setbacks
  - b. include:
    - i. water sensitive urban design solutions
    - ii. trees and supporting vegetation
    - iii. greening opportunities including green cover, green roofs, green walls, pergolas with climbers, podiums, planters, lawns and gardens, rain gardens and permeable pavements
    - iv. 50% native species.

**14.3.15. Views**

**Objectives**

- O30. To reinforce view corridors and vistas with buildings, structures, public art or landscape treatments.

**Controls**

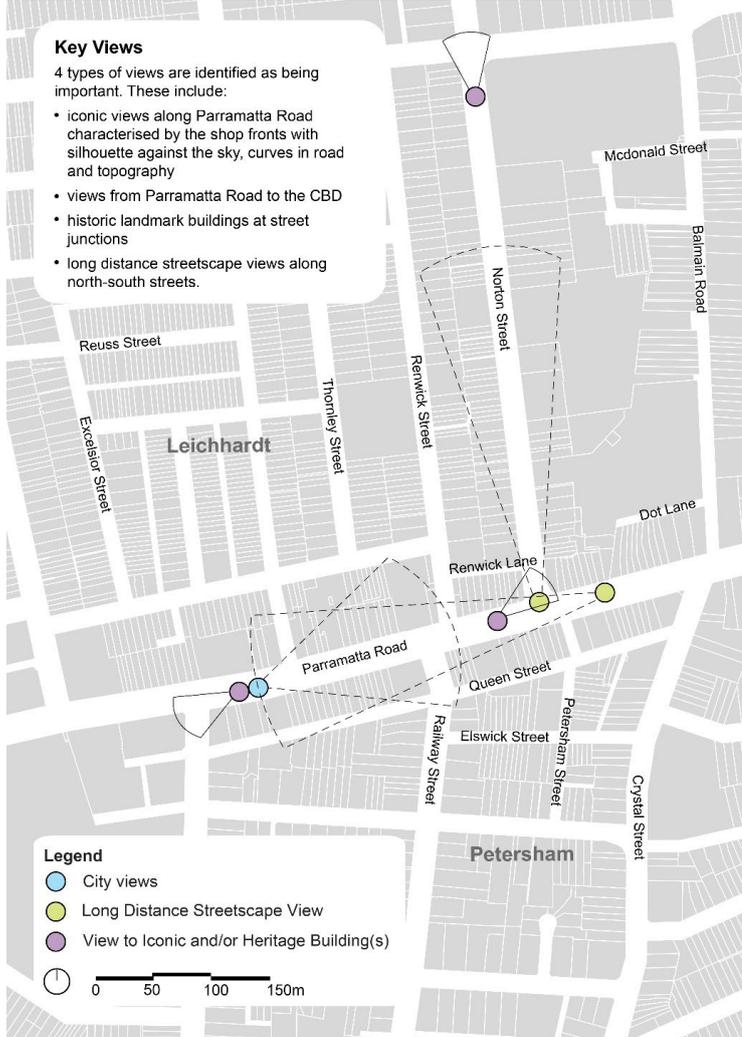
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C30. Development maintains and, where possible, enhances views as identified in Figure 4: Leichhardt

Precinct Key Views Map:

- a. to the City skyline
- b. to landmark buildings
- c. to street vistas identified.

Figure 4: Leichhardt Precinct key views



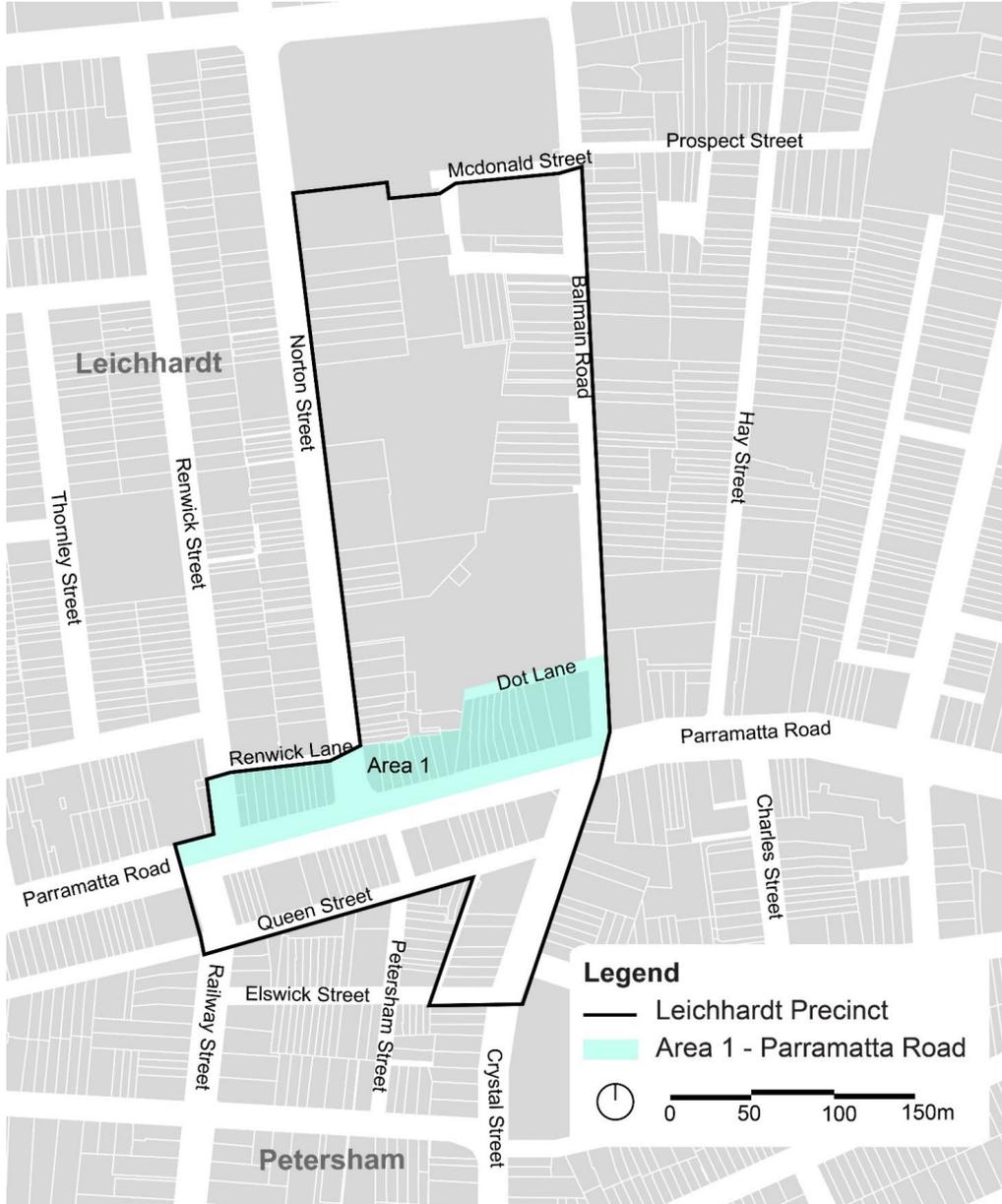
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14.4. Area 1 – Leichhardt: Parramatta Road

14.4.1. Application

Section 14.4 applies to Area 1 – Leichhardt: Parramatta Road as shown in Figure 5.

Figure 5: Area 1 – Leichhardt: Parramatta Road



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**14.4.2. Area 1 – Leichhardt: Parramatta Road Desired Future Character**

The Desired Future Character for Area 1 supplements and should be read in conjunction with the Desired Future Character detailed in Section 14.3 for the Leichhardt Precinct.

Leichhardt: Area 1 – Parramatta Road:

- Continues to provide its role as a productive economic corridor that attracts investment and new employment opportunities.
- Is reinvigorated as a retail business high street in its appearance and function.
- Its heritage significance is protected and revitalised with new development that respected the original built form.
- Pedestrians and bike riders benefit from the new urban shared space, Renwick Street, which enhances connectivity between Renwick Street and Railway Street.
- New built form:
  - is high quality
  - responds to and retains the heritage fabric and fine grain appearance of the area
  - is cohesive and presents a consistent street wall to Parramatta Road
  - positively interacts with the street
  - protects solar access, privacy and amenity of surrounding residential uses.

**14.4.3. Heritage**

**Objectives**

- O31. To ensure development responds to the historic built form of the location by:
- a. for Heritage Items – conserving and enhancing the significance, character, fabric and features of these buildings
  - b. for Contributory Buildings – restoring or reconstructing, altered or missing fabric of buildings
  - c. for all other buildings – be sympathetic to key architectural or streetscape features found in the Heritage Conservation Area (HCA)
  - d. not negatively impact on Heritage Items outside Area 1 – Parramatta Road of the Leichhardt Precinct.

**Controls**

- C31. Development responds sensitively to heritage and proactively retains, restores and enhances the heritage features of the location by:
- a. demonstrating that achievement of the floor space ratio and height of buildings incentive provisions does not have an adverse impact on the Heritage Items or the HCA, including Contributory Buildings in the HCA
  - b. for Heritage Items: alterations to the existing fabric are limited to restoration
  - c. for Heritage Items and Contributory Buildings as identified in Figure 6: Parramatta Road Heritage Features:
  - d. new built form:
    - i. is sympathetic and clearly distinguishable from the existing architecture
    - ii. complements the scale, form and materials of the streetscape and its desired future character including wall heights and roof forms

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- iii. pays particular attention to the transition from old to new and respects the existing façade and parapet in a way that ensures its architectural design remains a predominant feature
- iv. retains existing openings, and no new openings are introduced into the façade, including the parapet
- v. retains existing floor to floor heights and where new floor levels are introduced, these do not intersect with existing openings
- vi. evidences the original narrow fine grain width of shop fronts
- e. for all development:
  - i. retain the prominence of Heritage Items and landmark buildings in the immediate streetscape and surrounding area
  - ii. use sympathetic materials, colours and finishes to harmonise with the character of the HCA
  - iii. retain, or where required, replace suspended awnings to ensure consistency with adjoining and original fabric.

**14.4.4. Lot Amalgamation**

**Objectives**

- O32. To ensure development that relies on lot amalgamation results:
- a. in a built form character that retains the existing fine-grain appearance of Parramatta Road
  - b. in orderly and efficient land use.

**Controls**

- C32. Development that relies on amalgamation:
- a. evidences the original subdivision pattern in the resulting built form and shop front pattern
  - b. where basement levels are proposed, the resulting lot is a minimum of 17m wide and retains a fine-grain built form appearance to Parramatta Road
  - c. does not isolate or prevent surrounding lots from redeveloping.

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Figure 6: Leichhardt: Parramatta Road Heritage Items and Contributory Buildings



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**14.4.5. Built form**

**Objectives**

- O33. To ensure building height:
  - a. facilitates economic growth and new housing
  - b. responds appropriately to the heritage character of Parramatta Road
  - c. protects the amenity of surrounding land uses
  - d. provides a consistent street wall to Parramatta Road that is suited to the street proportions and defines the street edge.
- O34. To ensure storey height:
  - a. at the ground and first storey, allows for a variety of uses and provides flexibility to cater for change over time
  - b. above ground, is suited to intended land uses
  - c. retains existing floor to floor heights for Heritage Items and Contributory Buildings.
- O35. To maintain a consistent setback to Parramatta Road and ensure new built form responds appropriately to the existing and desired future streetscape.
- O36. To provide setbacks to rear streets that:
  - a. support access for a range of vehicles expected by the development
  - b. increase in depth aligned to building height to provide a built form transition, and amenity and privacy of surrounding properties
  - c. define the street edge
  - d. provide passive surveillance.

**Controls**

- C33. Building height:
  - a. does not exceed 6 storeys
  - b. has a street wall of:
    - i. 2 storeys to Parramatta Road
    - ii. 1 storey to Renwick Lane
    - iii. 4 storeys to Dot Lane
  - c. responds appropriately to Heritage Items through reduced height or transitioning heights to match the item.
- C34. Floor to floor height:
  - a. for Heritage Items or Contributory Buildings – ground floor and 2nd storey retain the existing floor to floor height
  - b. for other – aligns with adjoining Heritage Item or Contributory Building
  - c. for 3rd floor and above is 3.2m

Note: Ground level floor to floor height includes a slope/topography allowance.
- C35. Parramatta Road setback:
  - a. zero
  - b. 3m from 3rd storey
  - c. an additional 6m for 6th storey
- C36. Setback to rear lanes:

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- a. Renwick Lane setback:
  - i. for first storey at ground floor – zero
  - ii. for second storey and above determined on a site-by-site basis by demonstrating that the development:
    - can achieve appropriate solar access and visual privacy
    - will not impact the amenity including solar access and visual privacy of existing or future residential properties
    - will enhance the casual surveillance of Renwick Street.
- b. Dot Lane setback: zero

Notes:

1. Figure 7 depicts indicative built form bulk, scale and site layout on shallower lots (33m) being the typical size between Parramatta Road and Renwick Lane.
2. Figure 8 depicts indicative built form bulk, scale and site layout on deeper lots (46.5m) being the typical size between Parramatta Road and Dot Lane.

#### 14.4.6. Vehicle and service access locations

##### Objective

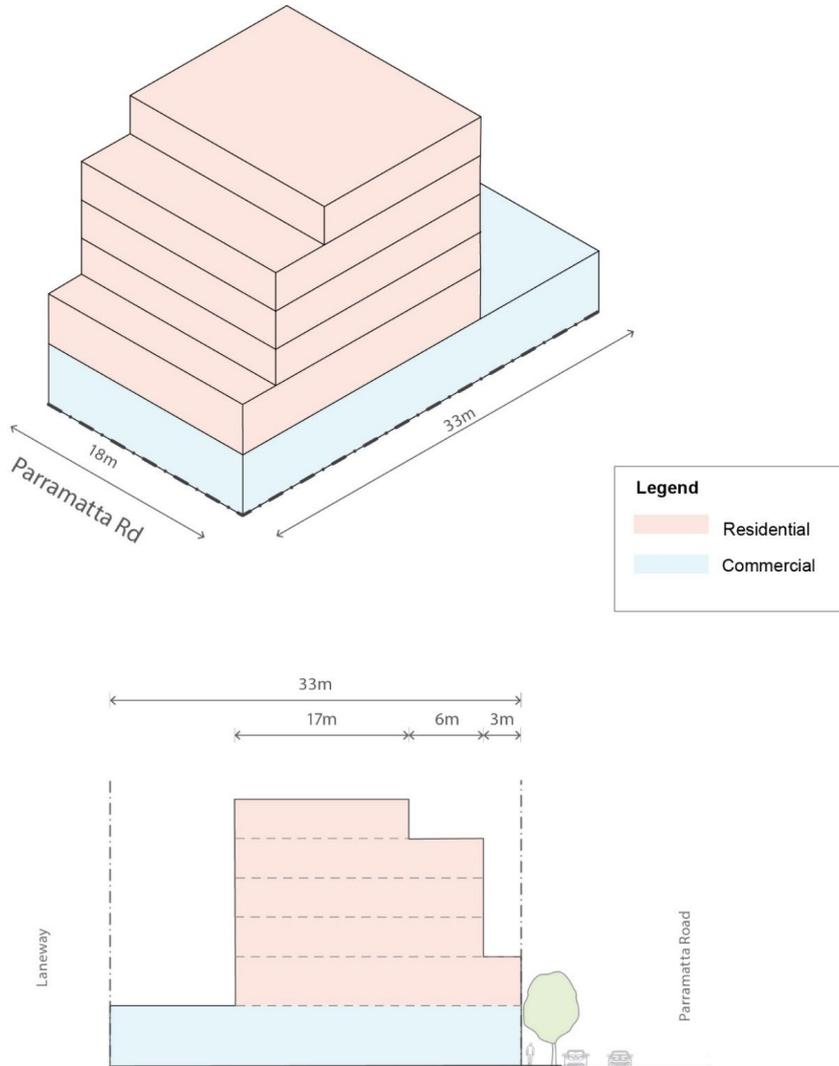
O37. To ensure vehicle and service access is via secondary streets maintaining the primary function of Parramatta Road.

##### Control

C37. Vehicle and service access is from Renwick Lane or Dot Lane.

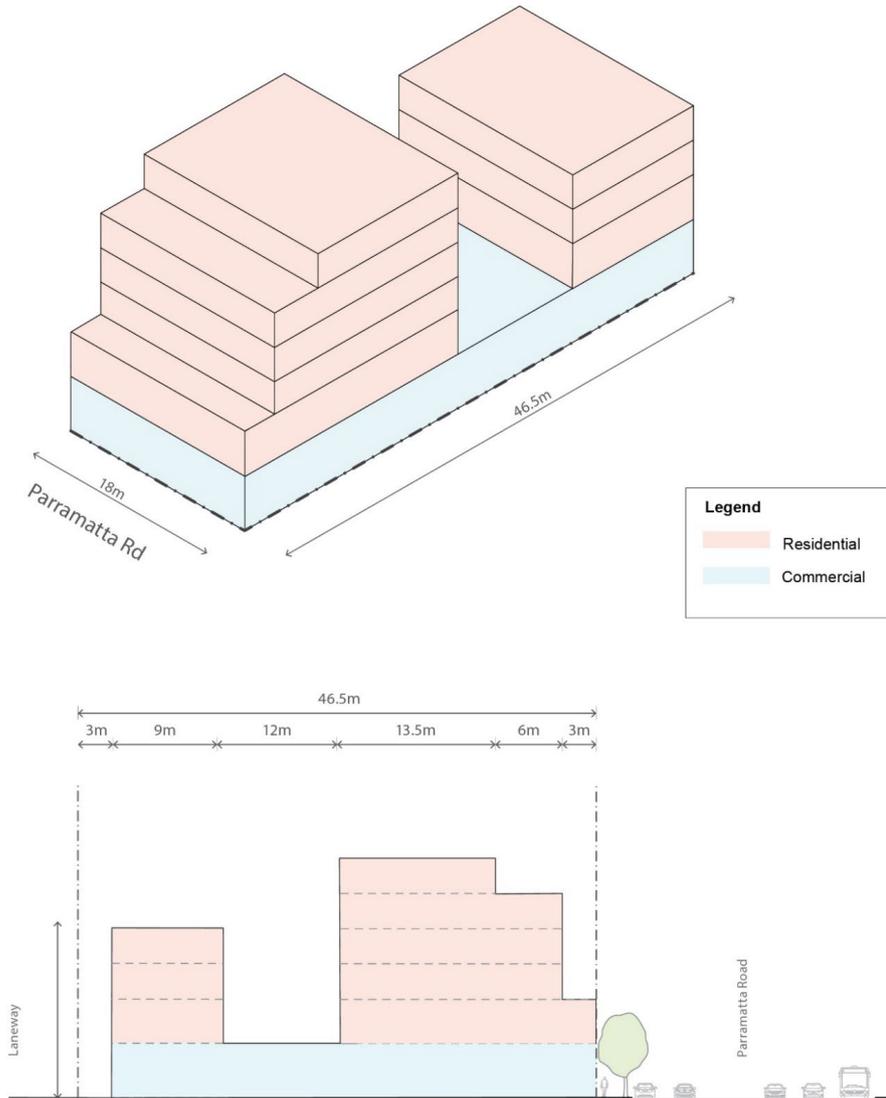
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Figure 7: Indicative Area 1- Parramatta Road shallow lot (33m) - built form bulk, scale and site layout – axonometric and section



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Figure 8: Indicative Area 1- Parramatta Road deeper lot (46.5m) - built form bulk, scale and site layout – axonometric and section



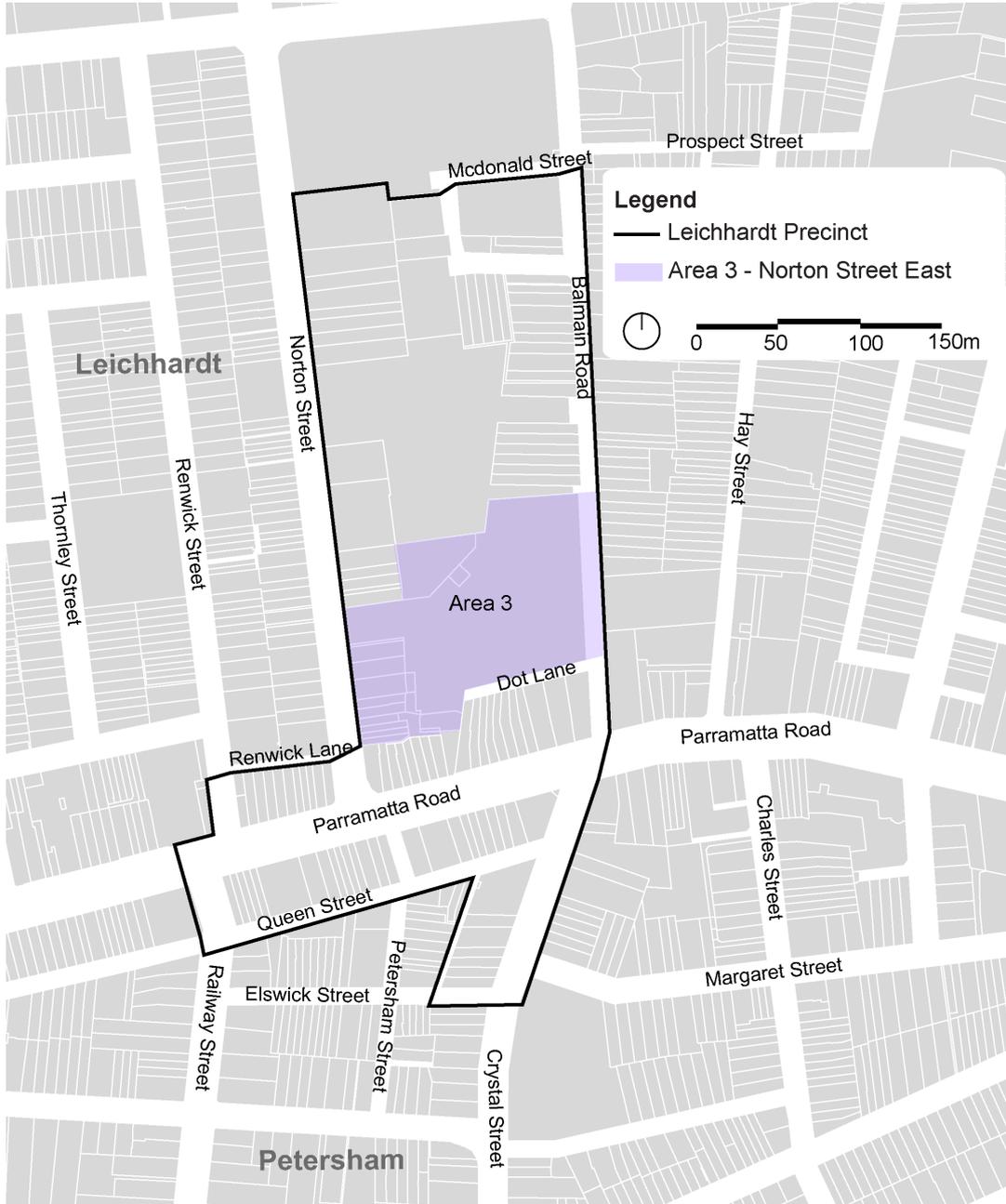
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14.5. Area 3 – Leichhardt: Norton Street East

14.5.1. Application

Section 14.5 applies to Area 3 – Leichhardt: Norton Street East as shown in Figure 9.

Figure 9: Area 3 – Leichhardt: Norton Street East



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**14.5.2. Desired Future Character**

The Desired Future Character for Area 3 supplements and should be read in conjunction with the Desired Future Character for Leichhardt Precinct detailed in Section 14.3.

Leichhardt: Area 3 Norton Street East:

- Norton Street is revitalised through intensification of residential and commercial uses.
- It maintains its high-street character through active street uses and enhanced public domain.
- The local heritage of Norton Street is reflected in the retention of contributory building facades, fine-grain built form, with a two storey street wall and active ground floors interacting with the street.
- East-west link between Balmain Road and Norton Street via Dot Lane provides permeability for pedestrians and cyclists.

**14.5.3. Heritage**

**Objectives**

- O38. To ensure development responds to the historic built form of the HCA by:
- a. for Contributory Buildings - restoring or reconstructing, altered or missing fabric of buildings
  - b. development responds sensitively to heritage items and the HCA in the immediate vicinity.

**Controls**

- C38. Development responds sensitively to heritage and proactively retains, restores and enhances the heritage features of the location by:
- a. demonstrating that achievement of the floor space ratio and height of buildings incentive provisions do not have an adverse impact on Heritage Items or the HCA, including Contributory Buildings that support the HCA
  - b. for Contributory Buildings as identified in Figure 6: Parramatta Road Heritage items and Contributory Buildings, new built form:
    - i. is sympathetic and clearly distinguishable from the existing architecture
    - ii. complements the scale, form and materials of the streetscape and its desired future character including wall heights and roof forms
    - iii. pays particular attention to the transition from old to new and respects the existing façade and parapet in a way that ensures its architectural design remains a predominant feature
    - iv. retains existing openings, and no new openings are introduced into the façade, including the parapet
    - v. retains existing floor to floor heights and where new floor levels are introduced, these do not intersect with existing openings
    - vi. evidences the original narrow fine grain width of shop fronts.

**14.5.4. Lot amalgamation**

**Objectives**

- O39. To ensure development that relies on lot amalgamation results:

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- a. for Contributory buildings - in built a form character that retains the existing fine-grain appearance
- b. in orderly and efficient land use.

**Controls**

C39. Development that relies on lot amalgamation:

- a. for Contributory Buildings – evidences the original subdivision pattern in the resulting built form and shop front pattern
- b. where basement levels are provided, the resulting lot is a minimum of 17m wide
- c. where there is no rear lane access, the resulting lot is a minimum of 30m frontage.

**14.5.5. Built form**

**Objectives**

O40. To ensure building height:

- a. facilitates economic growth and new housing
- b. responds appropriately to surrounding heritage items and heritage conservation areas
- c. protects the amenity of surrounding land uses.

O41. To ensure storey height:

- a. at the ground and 1st storey, allows for a variety of uses and provides flexibility to cater for change over time
- b. above ground, is suited to intended land uses
- c. retains existing floor to floor heights for Contributory Buildings.

O42. To maintain a consistent setback to Norton Street and ensures new built form responds appropriately to the desired future streetscape.

**Controls**

C40. Building height:

- a. does not exceed 6 storeys
- b. has a street wall to Norton Street of:
  - i. 4 storeys, or
  - ii. 2 storeys - if in the Heritage Conservation Area.

C41. Floor to floor height:

- a. for 1st storey at ground level is 5m, or
- b. for Contributory Buildings - retain the existing
- c. for 2nd storey is 4m, or
- d. for Contributory Buildings - retains the existing
- e. for 3rd storey and above is 3.2m.

Note: Ground level floor to floor height includes a slope/topography allowance.

C42. Norton Street setback:

- a. for Contributory Buildings:
  - i. retains existing setback at ground level
  - ii. for 3<sup>rd</sup> storey and above is 3m
  - iii. for 6<sup>th</sup> storey – an additional 6m

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- b. for other buildings:
  - i. is zero
  - ii. for 5<sup>th</sup> and 6<sup>th</sup> storey - 3m.

**14.5.6. Vehicle and service access locations**

**Objective**

O43. To minimise vehicle and pedestrian conflict along Norton Street.

**Controls**

- C43. Vehicle and service access to Norton Street is minimised wherever possible through:
- a. provision of minimal car parking, and increased use of public and sustainable transport modes
  - b. providing access via secondary streets or easements where possible
  - c. maintaining or, where possible, reducing existing number of vehicle cross-over locations
  - d. combining driveway access wherever possible.
  - e. no new driveway access locations
  - f. amalgamation of lots to provide consolidated access for vehicular cross-overs
  - g. breakout walls to allow for shared use of basements.

**14.5.7. Through site link between Balmain Road and Norton Street**

**Objective**

O44. To provide a g through-site link between Balmain Road and Norton Street through extension of Dot Lane.

**Controls**

C44. Redevelopment to provide a future cycle and pedestrian connection between Balmain Road and Norton Street through extension of Dot Lane.

Part G – Site Specific Controls

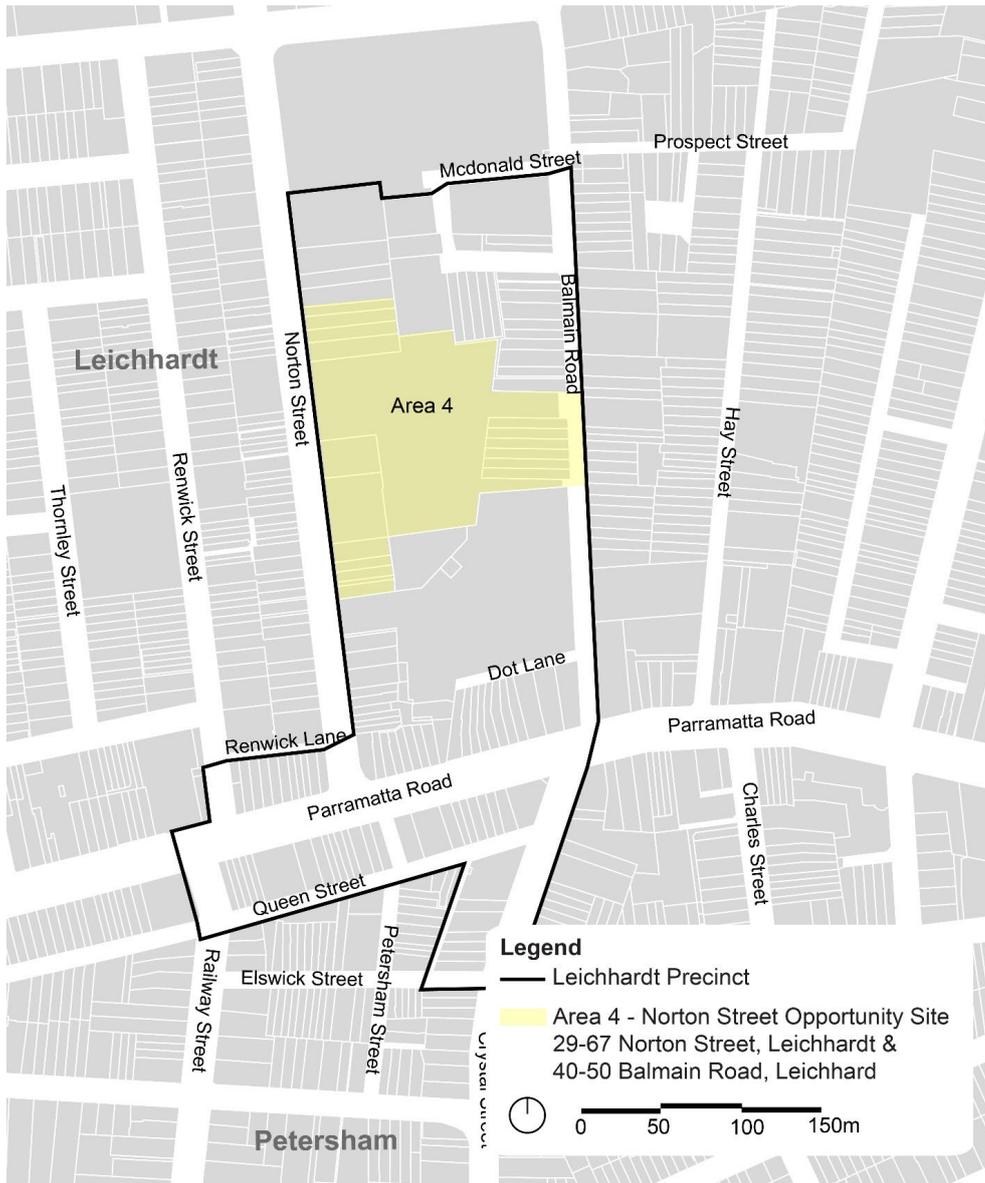
14.6. Area 4 – Leichhardt: Norton Street Opportunity Site

14.6.1. Application

Section 14.6 applies to Area 4 – Leichhardt: Norton Street Opportunity Site as shown in Figure 10 and comprises:

- 29-67 Norton Street Leichhardt
- 40-50 Balmain Road, Leichhardt.

Figure 10: Area 4 – Leichhardt: Norton Street Opportunity Site



Part G – Site Specific Controls

**14.6.2. Desired Future Character**

The Desired Future Character for Area 4 supplements and should be read in conjunction with the Desired Future Character detailed in Section 14.3 for Leichhardt Precinct.

Leichhardt: Area 4 – Norton Street Opportunity Site:

- Is the core of commercial activity and businesses on Norton Street.
- Support Norton Street’s historic role as a high street and its growing community.
- Enhanced public domain and improved permeability through new east-west connections between Norton Street and Balmain Road.
- Landscaped urban plaza provides a vibrant meeting place and activity hub on Norton Street.
- Built form is high quality architecture and maximises amenity for new and surrounding residents.

**14.6.3. Lot amalgamation**

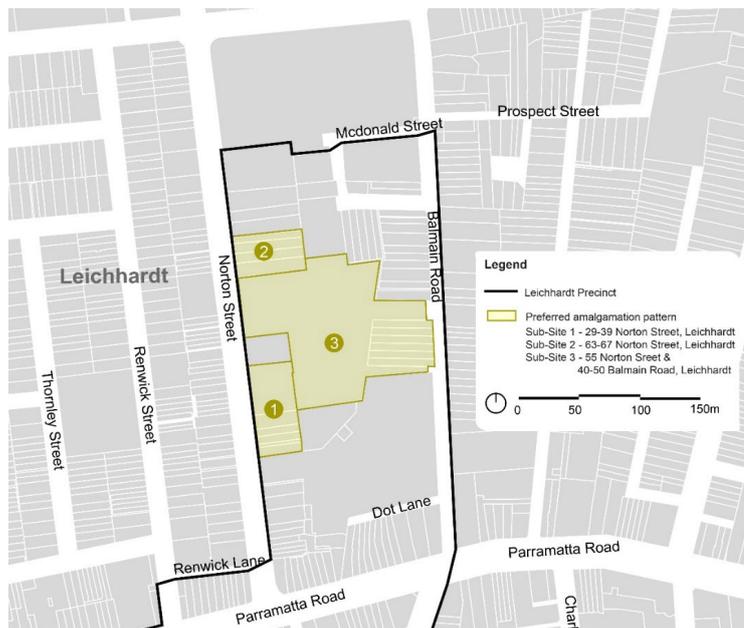
**Objectives**

O45. To ensure lot amalgamation facilitates redevelopment for commercial, business and residential uses, including delivering an urban plaza and through-site link as public spaces.

**Controls**

C45. Lot amalgamation aligns with Figure 11: Preferred lot amalgamation pattern – Norton Street Opportunity Site.

Figure 11: Preferred lot amalgamation pattern – Norton Street Opportunity Site



Part G – Site Specific Controls

**14.6.4. Built form**

**Objectives**

- O46. To ensure building height:
  - a. is suited to intended uses
  - b. has a bulk and scale which reflects the desired future character
  - c. retains solar access and privacy for residential dwellings.
- O47. To provide setbacks that:
  - a. create a consistent street wall to Norton Street
  - b. to provide a setback to Balmain Road that facilitates a landscaped setback that extends across all storeys including the basement
  - c. reduce the apparent bulk and scale of buildings
  - d. facilitate a new connection between Balmain Road and Norton Street
  - e. create a landscaped public open space/plaza fronting Norton Street.
- O48. To ensure storey height:
  - a. at the ground and 1<sup>st</sup> storeys, allows for a variety of uses and provides flexibility to cater for change over time
  - b. above ground, is suited to intended land uses.

**Controls**

- C46. Building height does not exceed 9 storeys and varies within the sub-sites. The building height should be generally consistent in form and scale as follows:
  - Sub-Site 1
    - a. provide a consistent street wall to Norton Street of four storeys and an overall height of 6 storeys.
  - Sub-Site 2
    - a. provide a consistent street wall to Norton Street of 3 storeys and an overall height of 7 storeys
    - b. locate taller building elements to the rear of the site.
  - Sub-Site 3
    - a. provide a consistent street wall to Norton Street of 3 storeys and overall height of 4 storeys
    - b. to Balmain Road is 5 storeys and an overall height of 8 storeys
    - c. within the site – ranges between 3 storeys and 9 storeys
    - d. the 9 storey component is located central to the site to minimise overshadowing and visual bulk and scale to Norton Street and Balmain Road.
- C47. Development is to comply with the following setbacks:
  - Sub-site 1
    - a. Setback to Norton Street:
      - i. for 1 to 4 storeys is zero
      - ii. for 5<sup>th</sup> storey and above is 3m
    - b. Setback to the north and existing buildings to create proposed urban plaza:
      - i. for 1 to 4 storeys is 25m
      - ii. for 5<sup>th</sup> storey and above is 28m

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- c. Setback to the rear is variable, being 0-14m demonstrating that the development meets its objectives.

Sub-site 2

- d. setback to Norton Street:
  - i. for 1 to 3 storeys is zero
  - ii. for 4<sup>th</sup> storey is 1.5m
  - iii. for 5<sup>th</sup> storey and above is 17m
- e. setback to the rear is variable, being 9-12m demonstrating that the development meets its objectives

Sub-site 3

- f. setback to Balmain Road:
  - i. for one to five storeys is 5m, including the basement
  - ii. for sixth storey and above is 8m
- g. buildings within the site setback to create an 18m wide through-site link from Balmain Road to Norton Street.

Note: Refer to Section 14.6.6 for Controls for the through-site link.

C48. Floor to floor height:

- a. for ground level is 5m
- b. for second storey is 4m
- c. for residential storeys above is 3.2m

Notes:

- 3. Refer to Figures 12 to 16 for indicative site layout, built form, bulk and scale, including storeys, setbacks, through site link and public space/urban plaza.
- 4. Note: Variation to built form controls may be considered on site-specific basis where the proposed development demonstrates achievement of the Precinct wide (14.3.2) and Area 4 (14.6.2) Desired Future Character and Precinct wide (14.3.12) and Area 4 (14.6.4) Built form Objectives and delivers better outcomes.

**14.6.5. Landscaping**

**Objectives**

O49. To ensure landscaping is provided:

- a. in the setback along Balmain Road that:
  - i. enhances pedestrian amenity
  - ii. contributes to defining the intersection with the through-site link
- b. to the through-site link and urban plaza that incorporates landscaping that provides shade and ground cover suited to the environment and purpose
- c. achieves 40% tree canopy cover across the site.

**Controls**

C49. The Landscaping Strategy demonstrates, landscaping:

- a. in the setback along Balmain Road:
  - i. includes deep soil planting and mature tree planting with appropriate setbacks to the basement

Part G – Site Specific Controls

- ii. is designed to enhance amenity and prioritise pedestrian movement along Balmain Road
- b. to the through-site link and urban plaza:
  - i. is designed to include tree canopy and greening for shade and improved thermal comfort
  - ii. incorporates drought tolerant species
- c. complies with the required tree canopy of 40% across the site.

Note: Landscaping requirements should be read in conjunction with Sections 14.3.4, 14.3.8 and 14.3.13. Where landscaping targets cannot be achieved, it must be demonstrated that landscaping has incorporated alternate greening measures including green cover, green roofs, green walls, pergolas with climbers, podiums, planters, lawns and gardens, rain gardens and permeable pavement.

**14.6.6. Through-site link and urban plaza on Norton Street**

**Objectives**

- O50. To provide a new publicly accessible through-site link from Balmain Road to connect with a new urban plaza on Norton Street that:
  - a. increases east-west block permeability
  - b. provides a safe space for walking and offers universal access
  - c. is visually and physically integrated with the surroundings
  - d. incorporates landscape treatments, street furniture, public art and materials that are high quality, fit for purpose, durable and sustainable.
- O51. To create an urban plaza on Norton Street which connects with the proposed through-site link to Balmain Road that:
  - a. provides a vibrant community hub and meeting place that can be used for events and gatherings
  - b. ensures all landscape treatments, street furniture, public art and materials are high quality, fit for purpose, durable and sustainable.

**Controls**

- C50. Development provides a through-site link that:
  - a. delivers a minimum 18m wide through-site link open to the sky that will connect to a proposed urban public space at Norton Street
  - b. provides unrestricted access 24 hours a day seven days a week
  - c. is registered as an easement on title
  - d. is designed so that the level change between Norton Street and Balmain Road does not restrict access to all users
  - e. is well designed to provide:
    - i. outdoor seating and lighting, appropriate for afterhours use
    - ii. a continuous path for all users including people with prams, wheelchair users, children/adults on bikes and scooters, whilst discouraging commuter cycling
    - iii. a clear line of sight between each end of the through-site link is designed to be stepped and landscaped to appropriately transition between the Balmain Road and Norton Street levels

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- iv. seating for a variety of uses and users
- f. integrates with Balmain Road streetscape and reinforces the intersection
- g. is designed so that adjoining developments minimise overshadowing and provide passive surveillance
- h. incorporates active ground floor frontages for its full extent
- i. incorporates awnings for the full extent of the through-site link that are:
  - i. translucent to provide wind and weather protection
  - ii. are a minimum of 5m above the public space
  - iii. do not impact on landscaping or the provision of trees
- j. provides way finding signage.

C51. Development:

- a. delivers an urban public space with a minimum width of 25m on the Norton Street frontage
- b. provides unrestricted public access 24 hours a day, seven days a week
- c. is registered as an easement on title
- d. is well designed and landscaped to provide:
  - i. outdoor seating and lighting
  - ii. a flexible space that is capable of being used as a place for events and gatherings
  - iii. seating for a variety of uses and users
  - iv. infrastructure, such as three phase power, waste facilities and drinking water to support a range of activities
  - v. a continuous path for all users including people with prams and wheelchair users
- e. incorporates awnings on all frontages that:
  - i. provide wind and weather protection
  - ii. are a minimum of 5m above the public space
  - iii. do not impact on landscaping or the provision of trees
- f. incorporates active frontages.

**14.6.7. Vehicle and service access locations**

**Objectives**

O52. To minimise pedestrian conflict by minimising the number of vehicle driveways along Norton Street.

**Controls**

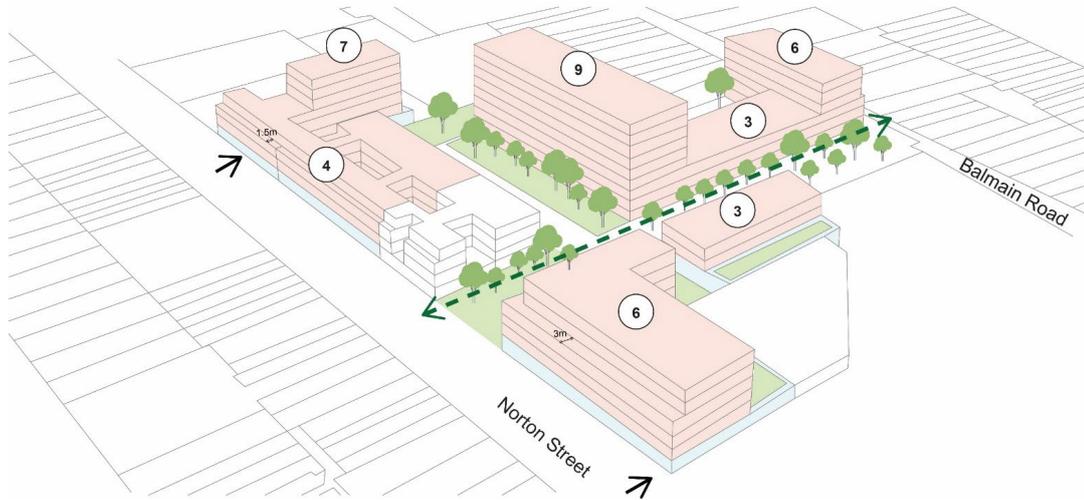
C52. Balmain Road is prioritised for vehicle and service access.

Part G – Site Specific Controls

Figure 12: Norton Street Opportunity Site – plan view



Figure 13: Norton Street Opportunity Site – axonometric view from Norton Street



Part G – Site Specific Controls

Figure 14: Norton Street Opportunity Site – axonometric view from Balmain Road

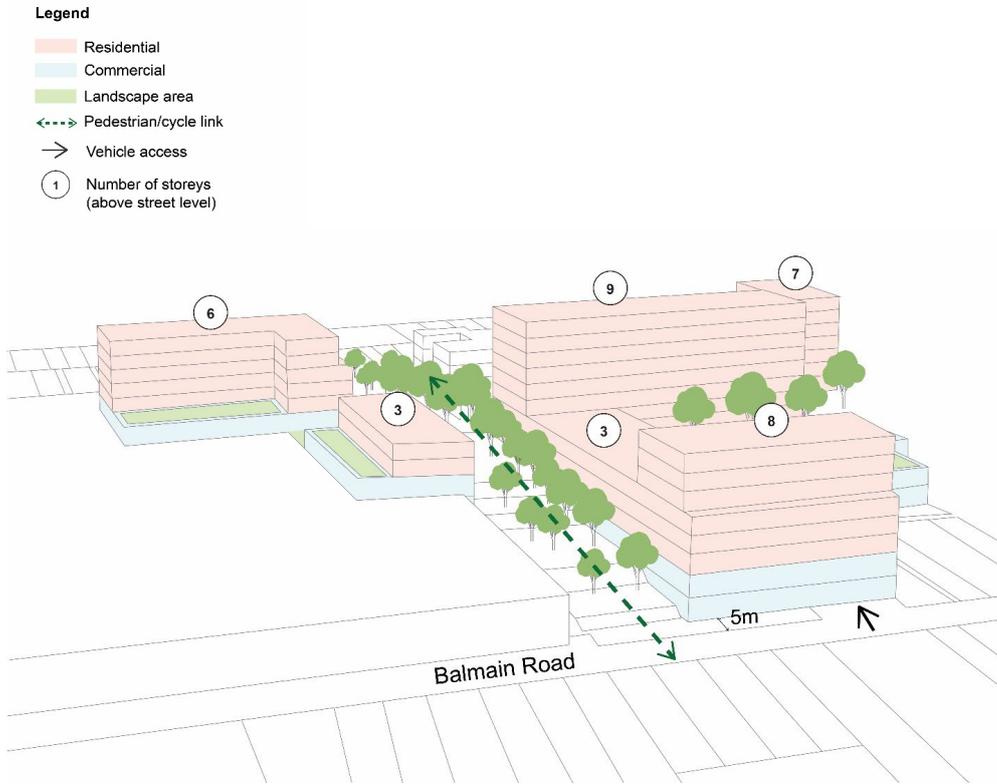
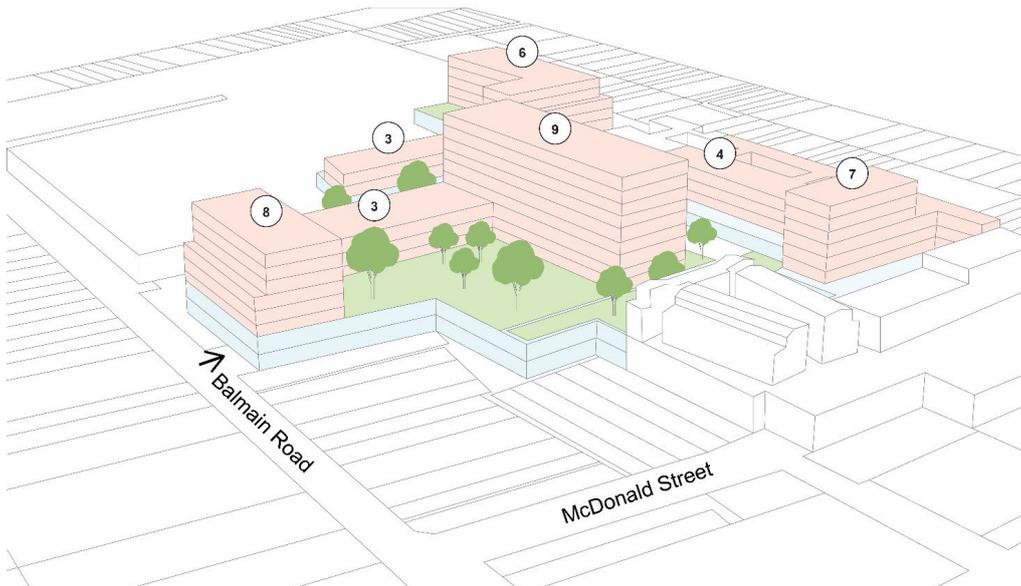
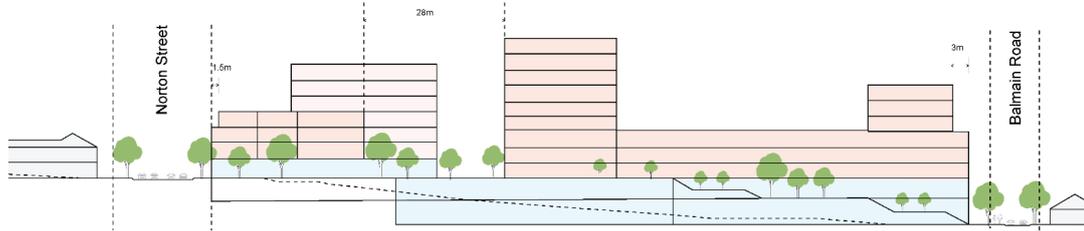


Figure 15: Norton Street Opportunity Site – axonometric view from Balmain Road and McDonald Street



Part G – Site Specific Controls

Figure 16: Norton Street Opportunity Site – section view



**Note:** Variation to built form controls may be considered on site specific basis where the proposed development demonstrates achievement of the Precinct wide (14.3.2) and Area 4 (14.6.2) Desired Future Character and Precinct wide (14.3.12) and Area 4 (14.6.4) Built form Objectives and delivers better outcomes.

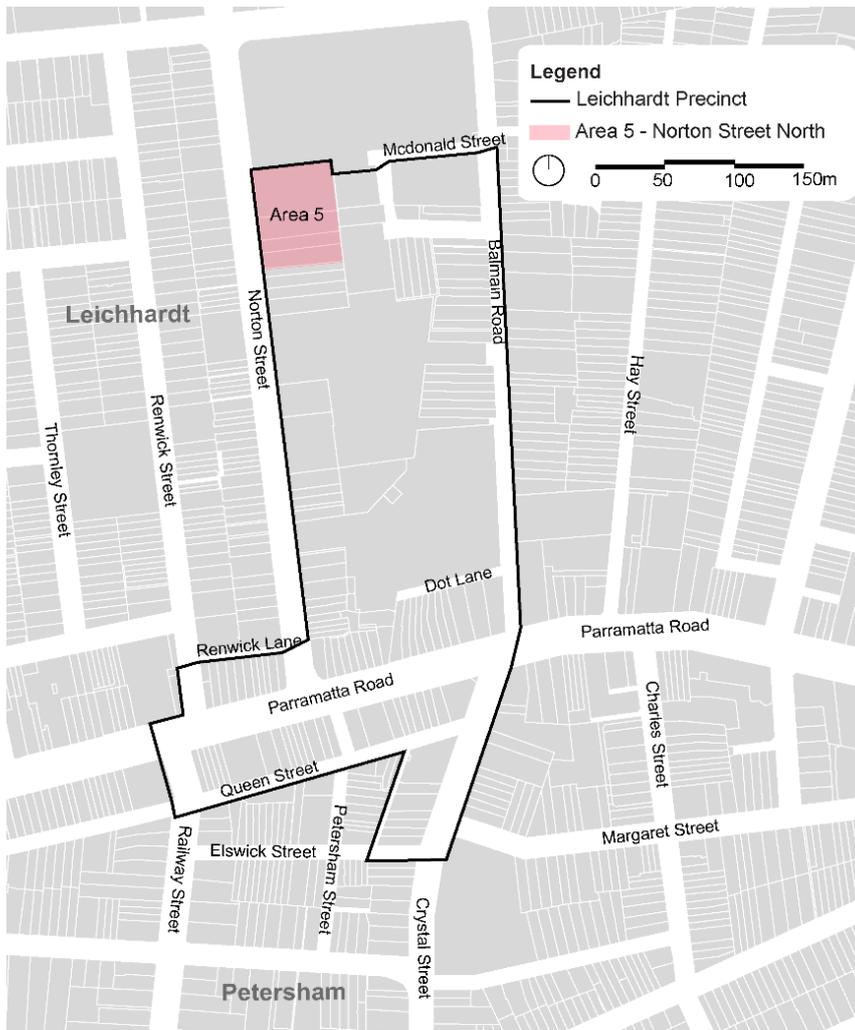
Part G – Site Specific Controls

14.7. Area 5 – Leichhardt: Norton Street North

14.7.1. Application

Section 14.7 applies to Area 5 – Leichhardt: Norton Street North as shown in Figure 17.

Figure 17: Area 5 – Leichhardt: Norton Street North



14.7.2. Desired Future Character

The Desired Future Character for Area 5 supplements and should be read in conjunction with the Desired Future Character for Leichhardt Precinct detailed in Section 14.3.

Area 5 Leichhardt: Norton Street North

- Norton Street is revitalised through intensification of residential and commercial uses.
- It maintains its high-street character through active street uses and enhanced public domain

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- Pedestrian and bike riders benefit from the new east-west through-site link to McDonald Street that provides a broader east-west connection to Prospect Street and Catherine Street
- New built form positively interacts with the Leichhardt Public School.

14.7.3. Heritage

Objectives

O53. To ensure development has no negative impact on the Leichhardt Public School Heritage item

Controls

C53. Development responds sensitively to the Heritage Items and landmark buildings in the immediate streetscape and surrounding area.

14.7.4. Lot amalgamation

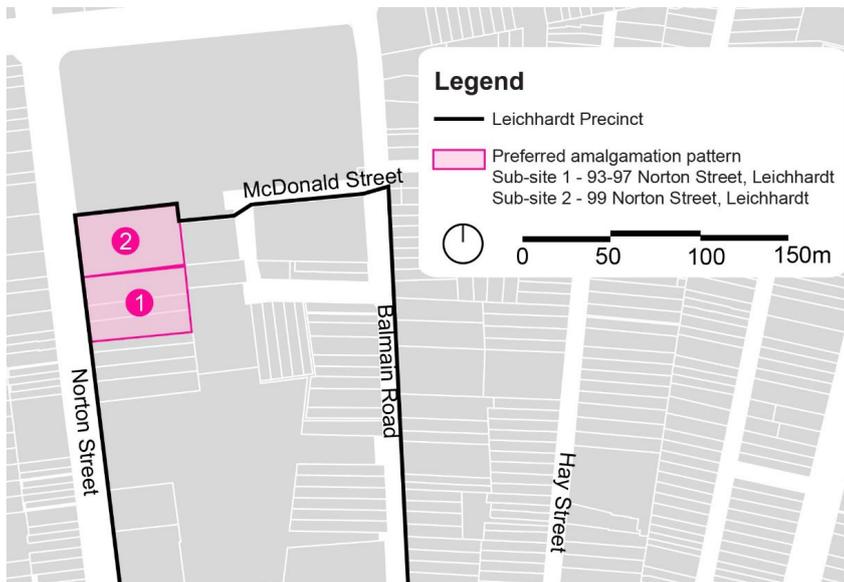
Objectives

O54. To ensure lot amalgamation promotes the orderly redevelopment of land for intended residential uses and identified built form.

Controls

C54. Lot amalgamation aligns with Area 5 – Leichhardt: Norton Street North preferred lot amalgamation pattern.

Figure 18: Preferred lot amalgamation pattern – Norton Street North



14.7.5. Built form

Objectives

- O55. To ensure building height:
- a. facilitates economic growth and new housing

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- b. responds appropriately to heritage items
- c. protects the amenity of surrounding land uses.

O56. To ensure storey height:

- a. at the ground and first storey, allows for a variety of uses and provides flexibility to cater for change over time
- b. above ground, is suited to intended land uses

O57. To maintain a consistent setback to Norton Street and ensures new built form responds appropriately to the desired future streetscape.

**Controls**

C55. Building height:

Sub-site 1

- a. does not exceed 7 storeys
- b. has a street wall to Norton Street of 3 storeys

Sub-site 2

- c. does not exceed 5 storeys
- d. has a street wall to Norton Street of 3 storeys

C56. Floor to floor height:

- a. for first storey at ground level is 5m
- b. for second storey is 4m
- c. for residential floors is 3.2m

Note: Ground level floor to floor height includes a slope/topography allowance.

C57. Building setbacks to:

Norton Street:

- a. is zero
- b. for fourth storey is 3m
- c. for fifth storey is 17m

Leichhardt Public School:

- d. is 6m
- e. is 4m from building line above 3 storeys

**14.7.6. Vehicle and service access locations**

**Objective**

O58. To minimise vehicle and pedestrian conflict along Norton Street.

Notes:

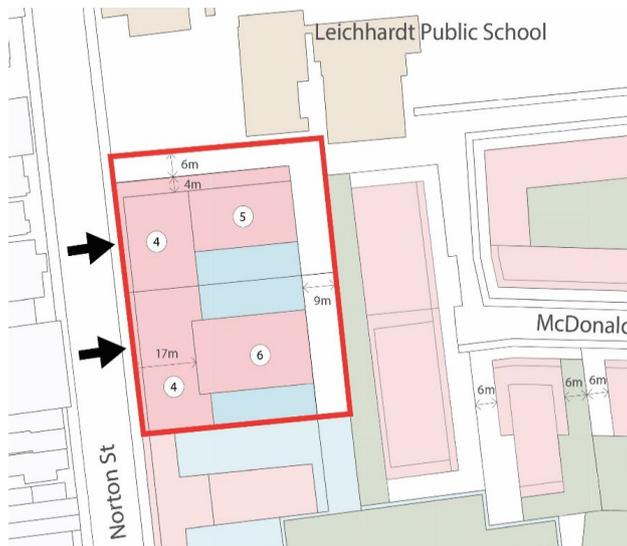
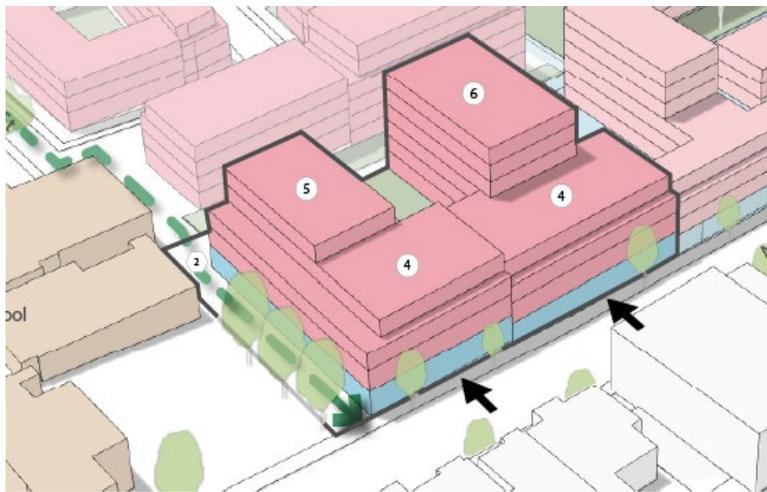
1. Figure 19 provides an indicative built form
2. Variation to built form controls may be considered on site specific basis where the proposed development demonstrates achievement of the Precinct wide (14.3.2) and Area 5 (14.7.2) Desired Future Character and Precinct wide (14.3.12) and Area 5 (14.7.5) Built form Objectives and delivers better outcomes.

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Controls

- C58. Vehicle and service access to Norton Street is minimised wherever possible through:
- a. provision of minimal car parking, and increased use of public and sustainable transport modes
  - b. maintaining or, where possible, reducing existing number of vehicle cross-over locations
  - c. combining driveway access wherever possible.
  - d. no new driveway access locations
  - e. amalgamation of lots to provide consolidated access for vehicular cross-overs
  - f. breakout walls to allow for shared use of basements.

Figure 19: Indicative Area 5: Norton Street North built form bulk, scale and site layout – axonometric and plan view



- 1 East west link linking Leichhardt Public School through Prospect St, St Fiacre's Catholic Primary School to Catherine St
- 2 Covered through site link to Norton St.

Legend

- Site Investigation Area
- Non-residential
- Residential
- Heritage Item
- Communal open space
- Vehicle Access

Part G – Site Specific Controls

**14.7.7. Through-site link between Norton Street and Balmain Road – McDonald Street**

**Objective**

O59. To safeguard a future cycle and pedestrian connection to provide a through site link between Norton Street to McDonald Street and beyond to Prospect Street and Catherine Street.

**Controls**

C59. Redevelopment of Sub-site 1 to provide a 6m setback to the northern boundary.

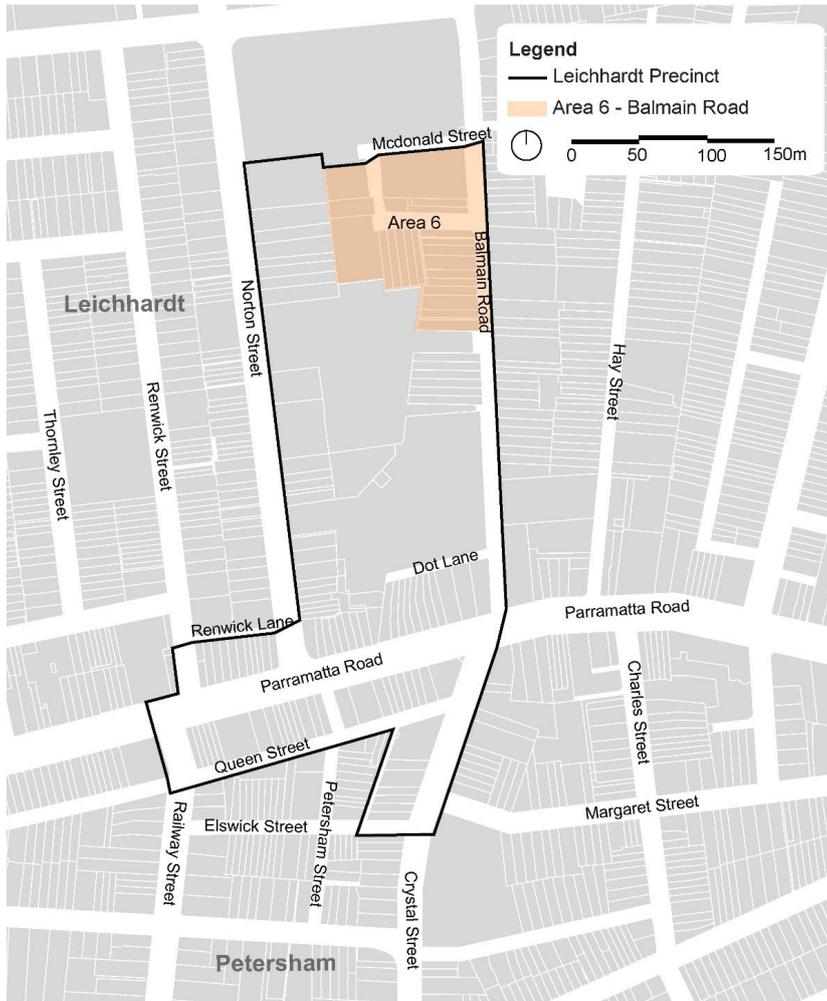
Part G – Site Specific Controls

## 14.8. Balmain Road

### 14.8.1. Application

Section 14.8 applies to Area 6 – Leichhardt: Balmain Road as shown in Figure 20.

Figure 20: Area 6– Leichhardt: Balmain Road



### 14.8.2. Desired Future Character

The Desired Future Character of this site supplements and should be read in conjunction with the Desired Future Character Statement detailed in Section 14.3.

Leichhardt: Area 6 –Balmain Road:

- Has provided a diverse range of residential dwellings in an area close to Leichhardt town centre, public transport, schools and community and civic services.
- Its built form:

Part G – Site Specific Controls

- Is high quality architecture
- has responded sensitively to scale of surrounding buildings and heritage items
- protects solar access, privacy and amenity of surrounding residential uses
- public domain has been enhanced through provision of landscaping and new street trees.

**14.8.3. Heritage**

**Objectives**

O60. To ensure development has no negative impact on the Leichhardt Public School Heritage item.

**Controls**

C60. Development responds sensitively to the Leichhardt Public School Heritage Item and landmark buildings in the immediate streetscape and surrounding area.

**14.8.4. Lot amalgamation**

**Objectives**

O61. To ensure lot amalgamation promotes the orderly redevelopment of land for intended residential uses and identified built form.

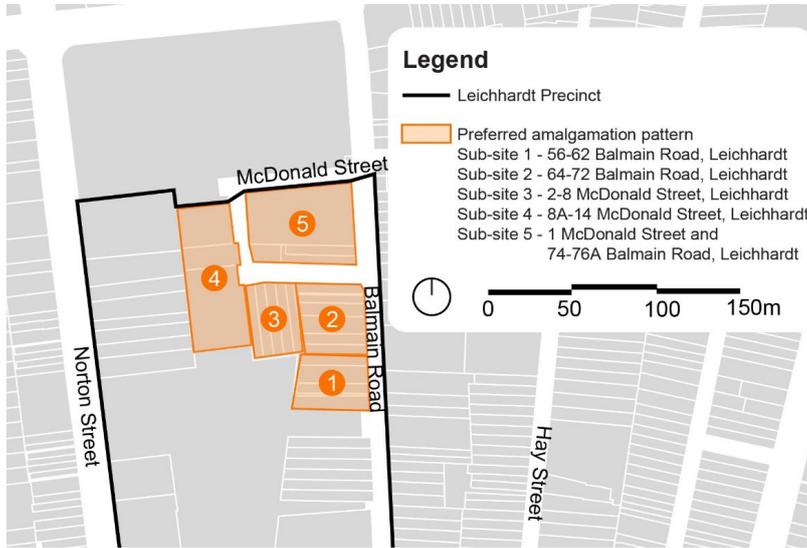
**Controls**

C61. Lot amalgamation:

- a. aligns with Area 6 Leichhardt: Balmain Road preferred lot amalgamation pattern, or
- b. achieves the following criteria:
  - i. does not isolate lots from redevelopment
  - ii. achieves required landscape area and communal open space
  - iii. provides required setbacks
  - iv. consolidates vehicle access and reduces their impact on pedestrian movement paths
  - v. provides appropriate access for servicing and waste management
  - vi. facilitates basement parking where on-site parking is provided.

Part G – Site Specific Controls

Figure 21: Preferred lot amalgamation pattern – Balmain Road



14.8.5. Built form

Objectives

O62. To ensure building height:

- a. facilitates economic growth and new housing
- b. responds appropriately to heritage items
- c. protects the amenity of surrounding land uses.

O63. To ensure storey height is suited to support residential uses.

O64. To maintain a consistent setback to Balmain Road and ensures new built form responds appropriately to the desired future streetscape.

Controls

C62. Building height does not exceed 7 storeys and varies within the sub-sites

Sub-Site 1

- a. provide a consistent street wall of 5 storeys and an overall height of 7 storeys

Sub-site 2

- b. provide a consistent street wall of 4 storeys to Balmain Road and an overall height of 6 storeys to Balmain Road
- c. provide a consistent street wall of 3 storeys to McDonald Street and an overall height of 4 storeys

Sub-site 3

- d. provide a consistent street wall of 3 storeys to McDonald Street and an overall height of 6 storeys

Sub-site 4

- e. provide a consistent street wall of 3 storeys to McDonald Street and an overall height of 6 storeys and 4 storeys adjacent to the Leichhardt Public School

Part G – Site Specific Controls

Sub-site 5

- f. provide a consistent street wall of 3 storeys to Balmain Road and McDonald Street and an overall height of 4 storeys and 3 storeys adjacent to the Leichhardt Public School.

C63. Floor to floor height:

- a. for first storey at ground level is 4m
- b. for floors above is 3.2m

Note: Ground level floor to floor height includes a slope/topography allowance.

C64. Building setbacks to:

- a. Balmain Road:
  - i. Sub-sites 1 and 2
    - 5m
    - for 5th floor and above – 3m
  - ii. Sub-site 5
    - 3m
    - for 4th storey – 3m
- b. McDonald Street setback:
  - i. Sub-sites 2, 3 and 5
    - 3m
    - for 4th floor and above – 3m
  - ii. Sub-site 4
    - 4m
    - For 4th storey and above – 3m

Notes:

1. Figure 22 provides an indicative built form
2. Variation to built form controls may be considered on site specific basis where the proposed development demonstrates achievement of the Precinct wide (14.3.2) and Area 6 (14.8.2) Desired Future Character and Precinct wide (14.3.12) and Area 6 (14.8.5) Built form Objectives and delivers better outcomes.

**14.8.6. Vehicle and service access locations**

**Objective**

O65. To minimise vehicle and pedestrian conflict along Norton Street.

**Controls**

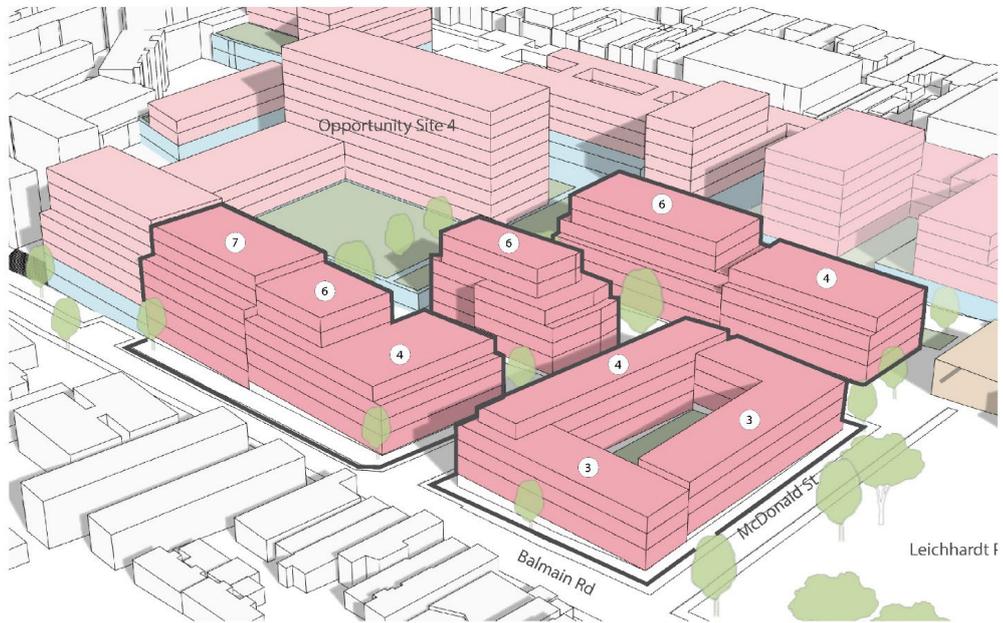
- C65. Vehicle and service access to Balmain Road and McDonald Street is minimised wherever possible through:
- a. provision of minimal car parking, and increased use of public and sustainable transport modes

Part G – Site Specific Controls

- b. maintaining or, where possible, reducing existing number of vehicle cross-over locations
- c. combining driveway access wherever possible.
- d. no new driveway access locations
- e. amalgamation of lots to provide consolidated access for vehicular cross-overs.

Figure 22: Area 6 Leichhardt Balmain Area – axonometric and plan view

Part G – Site Specific Controls



Part 9 – Strategic Context

## 9.50 Parramatta Road Corridor – Leichhardt Precinct

### 9.50.1 Application

Part 9, Strategic Context, Section 9.50, Parramatta Road Corridor – Leichhardt Precinct applies:

- to that part of Leichhardt Precinct shown in Figure 1: Parramatta Road Corridor: Leichhardt Precinct Land Application Map as Area 1 and Area 2, and
- where development seeks to rely on the Incentives Floor Space Ratio Map, Incentives Height of Buildings Map and meets Clause X.X of the Inner West LEP 2022.

Where development does not seek to rely on the incentives provisions Part 9, Section 50 does not apply. In this circumstance, relevant provisions of this DCP apply.

Leichhardt Precinct has five Areas that have varying functions and intended outcomes. As detailed above, this Section applies to two of those Areas:

- Area 1 – Leichhardt: Parramatta Road,
- Area 2 – Leichhardt: Crystal Street.

Where seeking to rely on incentive provisions, all development will achieve the Desired Future Character, Objectives and Controls detailed in:

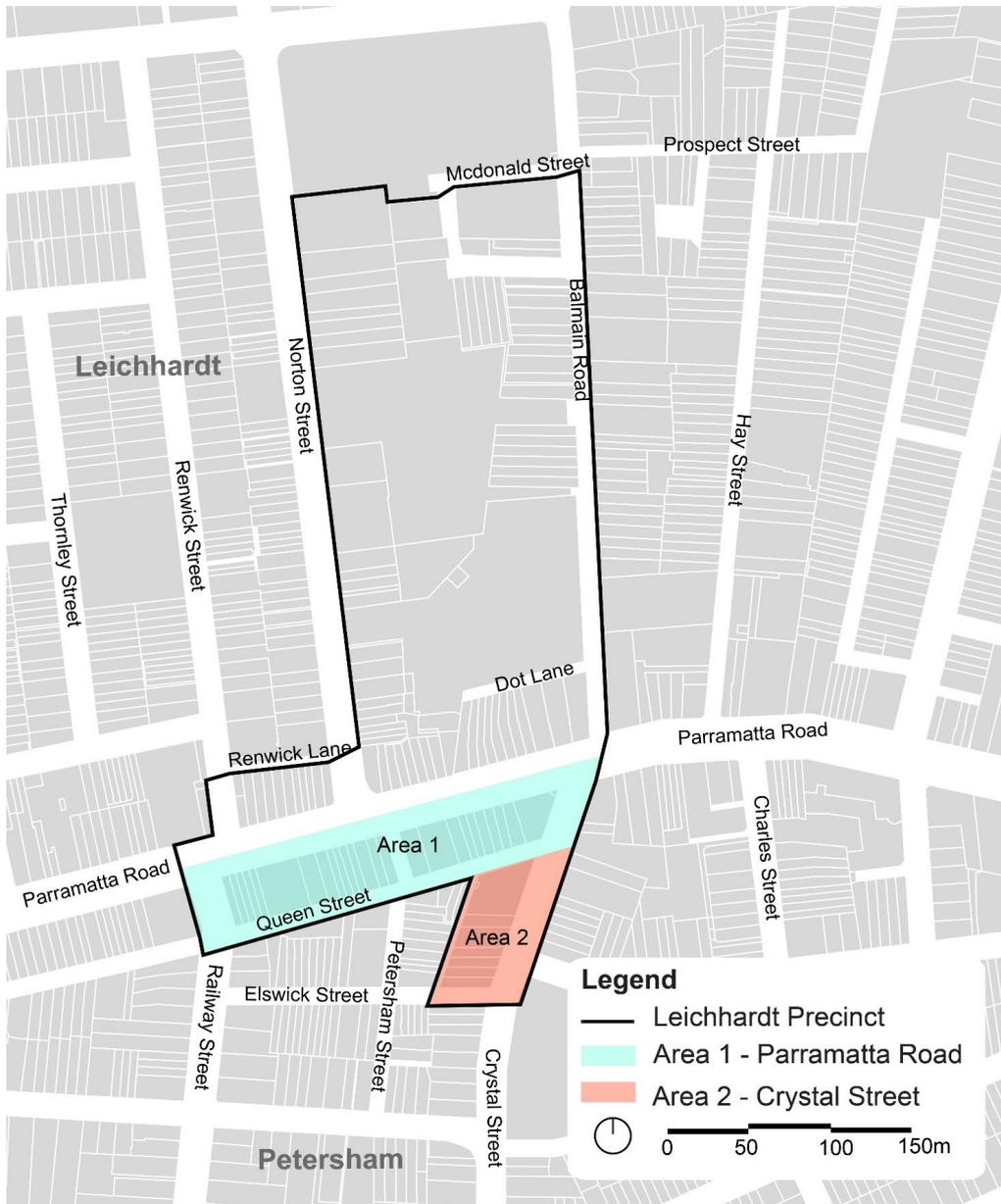
- Section 9.50.3 that applies to all Areas in the Leichhardt Precinct, and as applicable
- Section 9.50.4 that applies to Area 1 – Leichhardt: Parramatta Road, or
- Section 9.50.5 that applies to Area 2 – Leichhardt: Crystal Street.

These Sections supplement and should be read in conjunction with relevant provisions of this DCP.

Where Part 9, Section 9.50 applies and there is an inconsistency between this Section and other provisions of this DCP, this Section prevails.

Part 9 – Strategic Context

Figure 1: Parramatta Road Corridor: Leichhardt Precinct Land Application Map



Part 9 – Strategic Context

9.50.2 Context

**Parramatta Road Corridor Urban Transformation Strategy**

Parramatta Road Corridor – Leichhardt Precinct is one of eight Precincts of the Parramatta Road Corridor Urban Transformation Strategy (PRUCS). PRUCS is the NSW Government’s 30-year plan setting out how the Parramatta Road Corridor will grow and bring new life to local communities living and working along the Corridor.

The vision for Parramatta Road Corridor is:

**A high-quality multi-use corridor with improved transport choices, better amenity and balanced growth of housing and jobs.**

The vision is supported by seven principles:

1. **Housing choice and affordability**  
Plan for diversity in housing types to accommodate a wide range of community needs, including affordable, family, student and seniors housing.
2. **Diverse and resilient economy**  
Plan for and position the corridor to attract new businesses and support existing business that create a diversity of jobs and promote jobs closer to home.
3. **Accessible and connected**  
Reshape and better connect places and movement networks to better serve customers and encourage sustainable travel.
4. **Vibrant community places**  
Promote quality places and built form outcomes to transform the corridor over time.
5. **Green spaces and links**  
Embellish existing open space and provide for new open spaces that support the recreational needs of the community and encourage active and healthy lifestyles.
6. **Sustainability and resilience**  
Create liveable local Precincts along the corridor that are sustainable, resilient and make Sydney a better place.
7. **Delivery**  
Deliver, drive, facilitate and monitor action.

**PRUCS and Inner West**

Four of the eight PRUCS Precincts are within Inner West Council local government area. These include:

- Part of Kings Bay/ Croydon Precinct in Croydon that extends from Lang Street, Croydon in the west to Iron Cove Creek in the east. The remaining areas of Kings Bay Precinct are in Burwood and Canada Bay local government areas.
- Taverners Hill Precinct that extends from Petersham in the east to Summer Hill in the west and includes areas in Leichhardt and Lewisham.
- Leichhardt Precinct in the suburbs of Leichhardt and Petersham.
- Part of Camperdown Precinct. The remaining area of Camperdown Precinct is in City of Sydney local government area.

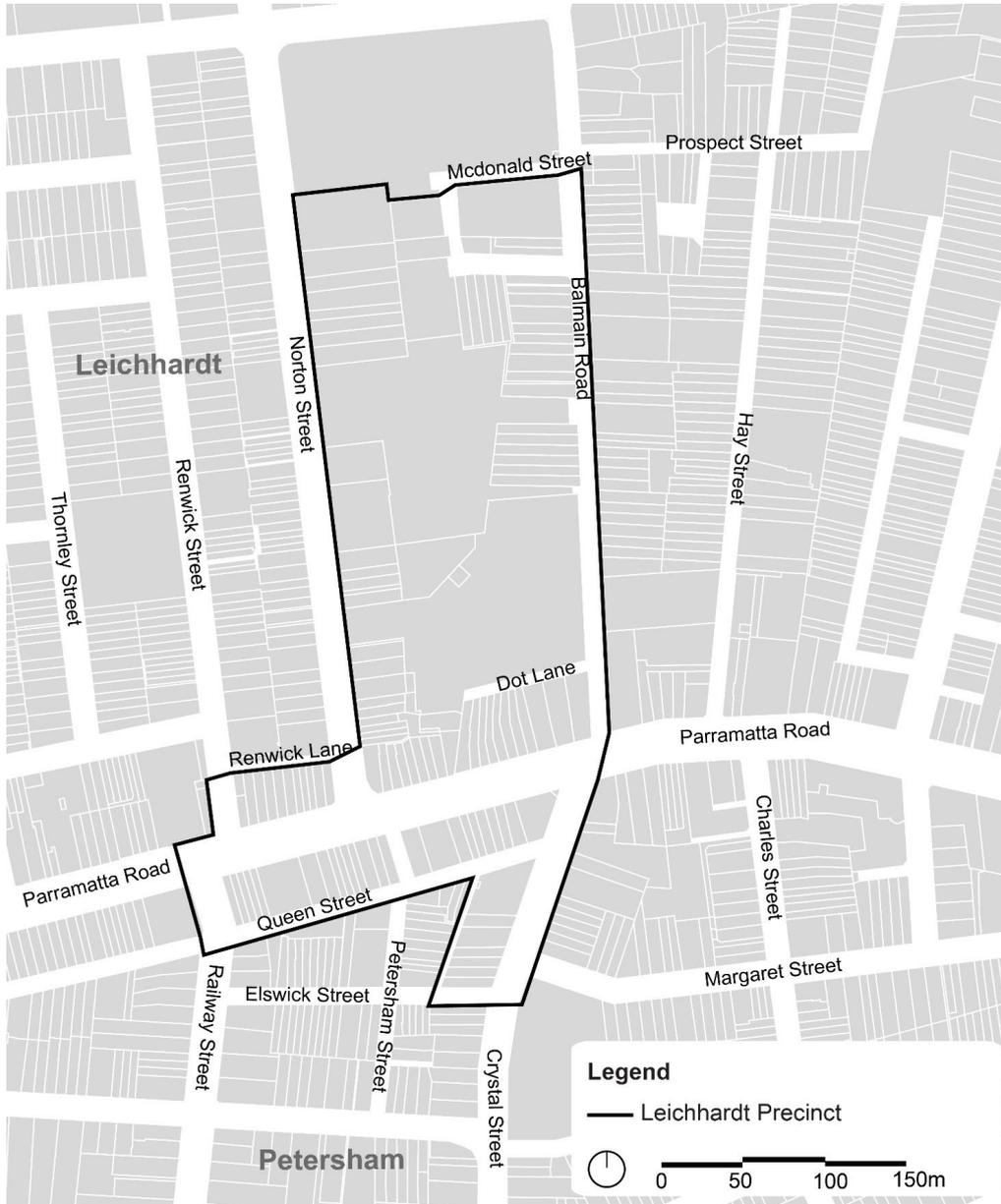
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### 9.50.3 Leichhardt Precinct

#### 9.50.3.1. Application

Section 9.50.3 applies to the entire Leichhardt Precinct as identified in Figure 2:

Figure 2: Parramatta Road Corridor: Leichhardt Precinct



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9.50.3.2. Leichhardt Precinct Desired Future Character

- Norton Street is a strong vibrant and bustling activity strip that creates a sense of community and is supported by increased residential density.
- Parramatta Road provides affordable small-scale retail and employment premises and where new development respects heritage and the fine grain character of the streetscape.
- Housing within the Precinct is well located and diverse, serving the needs of people of all ages, abilities and incomes.
- Busy pedestrian routes provide access to and from key destinations within the Precinct and new east-west pedestrian connections break down large blocks on Norton Street.
- People enjoy a public domain that is well-designed, activated and landscaped.
- The iconic views and vistas along Parramatta Road and north-south streets, of historic landmarks at street junctions and glimpses to the city skyline remain.
- Living and work environments are sustainable and comfortable as a result of:
  - buildings having a high standard environmental performance
  - integrated water management
  - building design, landscape and materials reducing urban heat effects
  - building design reducing noise and air quality improvements
  - promoting active and public transport
  - catering for electric charging infrastructure.
- The built form is high quality, suitably scaled, transitions to neighbouring areas and meets the needs of intended uses.
- Taller buildings are concentrated between Norton Street and Balmain Road which protects the lower scale and well recognised streetscape along Norton Street and Parramatta Road.
- Residents benefit from building design that maximises their amenity while protecting the amenity of nearby developments.
- Community facilities and civic services such as the library, town hall and schools support the Precinct.
- Pedestrians and bike riders benefit from:
  - enhanced connections across Parramatta Road and along Railway Street to Petersham Station
  - increased east-west permeability
  - safe cycling connections north-south.
- Reliance on private vehicles has reduced to support sustainable living through:
  - reducing on-site car parking provision for origin and destination locations
  - setting maximum car parking rates instead of requiring minimum car parking
  - implementing new models such as unbundled parking and shared car use
  - leveraging proximity to public transport networks, including rapid transport on dedicated lanes on Parramatta Road

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**9.50.3.3. Connectivity and accessibility****Objectives**

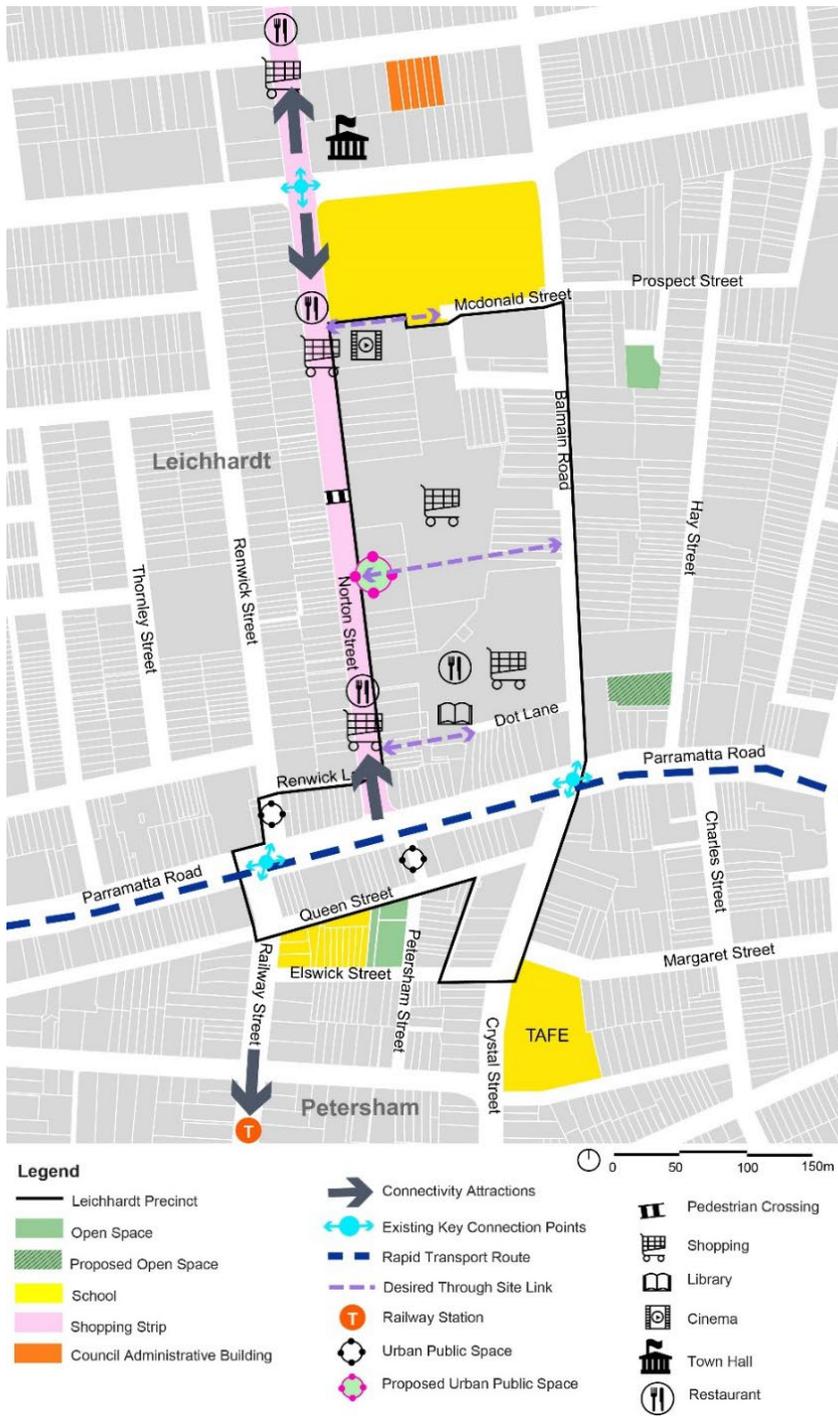
- O1. To increase connectivity, permeability and accessibility across the Precinct for pedestrians and bike riders by enhancing links between workplaces and residential areas to key locations.
- O2. To enhance local connectivity between Norton Street and Balmain Road.

**Controls**

- C1. Built form and streetscape treatments reinforce pedestrian and cycling connections identified in Figure 3: Leichhardt Precinct connectivity and accessibility map, including:
  - a. north-south connections within the Precinct and to Petersham Train Station
  - b. east-west connections between Balmain Road and Norton Street.
- C2. Where a desired through-site link is identified on Figure 3, lot amalgamation and development contribute to mid-block connections to increase connectivity between Norton Street and Balmain Road.

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Figure 3: Leichhardt Precinct connectivity and accessibility map



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**9.50.3.4. Streetscape and public domain**

**Objectives**

- O3. To improve the amenity and safety of the streetscape of the Precinct in a manner that:
  - a. contributes to the street character and intended land uses
  - b. is supported by built form that interfaces well with the streetscape and heritage
  - c. reduces street clutter and improves the visual amenity of the public domain
  - d. reinstates or upgrades the footpath to provide enhanced public domain
  - e. protects existing street trees
  - f. positively contributes to water management and is waterwise
  - g. results in a durable and low maintenance public domain.

**Controls**

- C3. Development contributes towards enhancing the streetscape for the extent of the street frontage by:
  - a. providing required built form setbacks and a built form that interfaces well with the streetscape
  - b. ensuring that pedestrian movement area is clear of obstacles
  - c. integrating pedestrian and vehicular entries into the streetscape design.

Notes:

- 1. Refer to Inner West Public Domain Design Guide (202X) for details of road types, footpath area functions and finishes.
- 2. Refer to Controls related to built form and landscaping as detailed in Sections 9.50.4 and 9.50.5 as relevant to each Area within the Precinct.

**9.50.3.5. Development utility infrastructure**

**Objectives**

- O4. To reduce street clutter, provide opportunity for viable street trees and enhance the public domain.
- O5. To locate and design mechanical plant and essential services in a way that:
  - a. improves the visual amenity of the public domain
  - b. does not conflict with landscaping or street tree planting
  - c. is located outside the public domain.

**Controls**

- C4. Relocate existing overhead cables underground, and where possible, co-locate with other underground services.
- C5. Mechanical plant and essential services equipment are:
  - a. contained within the property
  - b. located off the primary street frontage, or
  - c. where on the primary street frontage are located behind the building line and screened from view
  - d. integrated with the building and landscape design.

**9.50.3.6. Affordable housing**

**Objectives**

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- O6. To increase the supply of well-designed affordable housing in the Inner West to meet community needs and in appropriate locations across Leichhardt Precinct.
- O7. To ensure affordable housing is managed and retained in perpetuity.

**Controls**

- C6. Affordable housing units:
  - a. include a range of sizes to cater for different household sizes
  - b. are designed and constructed to the same standard as other residential accommodation in the development.
  - c. are distributed throughout the development with a unit mix determined by Council in consideration of affordable housing need and social inclusion
- C7. Affordable housing units are to be provided and managed in accordance with the relevant Affordable Housing Contributions Scheme and Council’s Affordable Housing Policy.

Note: Affordable housing has a statutory definition under the NSW Environmental Planning and Assessment Act 1979 of “housing for very low income households, low income households or moderate income households, being such households as are prescribed by the regulation or are as provided for in an environmental planning instrument.”

Refer to Inner West Affordable Housing Policy 2022 for details of Affordable housing requirements.

**9.50.3.7. Lot amalgamation**

**Objectives**

- O8. To promote efficient use of land and orderly redevelopment by:
  - a. avoiding isolating lots and reducing development potential
  - b. providing intended uses and built form outcomes that make a positive contribution to the streetscape.

**Controls**

- C8. Lot amalgamation:
  - a. does not result in isolated lots that are impractical for redevelopment to the scale and intensity desired for the area
  - b. combines narrow lots and lots in fragmented ownership.

**9.50.3.8. Sustainability and resilience**

**Objectives**

- O9. To achieve a high standard of environmental building performance that:
  - a. reduces greenhouse gas emissions and water use
  - b. results in comfortable living and working environments.
- O10. To reduce urban heat island effects through incorporating and integrating mechanisms that collectively mitigate the impacts, including:
  - a. green infrastructure in the form of landscape and surface treatments that incorporate water storage and treatment while reducing water usage
  - b. trees that offer shade to the built form, hard surfaces and vegetation
  - c. building materials and colours that reduce heat impacts, contribute to energy efficiency and thermal comfort, and minimise nuisance caused by glare or heat radiation.

**Controls**

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- C9. Building Environmental Performance Report or BASIX certificate demonstrates that the development:
  - a. achieves a reduction in greenhouse gas emissions and water use
  - b. results in comfortable living and working environments
  - c. includes passive design features such as optimal orientation, increased insulation, effective shading, cross ventilation and lower solar absorptance external surface finishes
  - d. optimises rooftop solar photovoltaic systems
  - e. achieves full electrification of utilities including cooking (other than in commercial kitchens), heating and hot water (heat pumps)
  - f. for residential development:
    - i. achieves an average thermal performance of 7-star NatHERS
    - ii. incorporates ceiling fans in bedrooms and living rooms.
  
- C10. Mitigate urban heat island effects by:
  - a. achieving required tree canopy through:
    - i. site layout maximising retention of existing mature trees
    - ii. including advanced containerised trees (greater than 200 litre) of a species that within 10 years will achieve 50% of their potential at maturity
    - iii. incorporating trees and vegetation across various storeys (roof tops, terraces, atriums, and the like), in addition to, or where necessary, as an alternative to ground level planting
    - iv. a combination of the above that collectively achieve, or exceed, the tree canopy requirements.
  - b. integrating green roofs and walls as a component of the landscape and built form design specifically in northern and western facing locations
  - c. incorporating permeable surfaces, rain gardens, and other water sensitive measures in landscape treatments
  - d. using materials and colours that:
    - i. have a high solar reflectance index (lighter colours) on roofs, facades, glazing or ground surfaces subject to their purpose and aligned to orientation and exposure to sunlight
    - ii. where it may cause nuisance due to glare or reflection do not exceed 20% reflectivity (darker colours).

**9.50.3.9. Access and parking**

**Objectives**

- O11. To ensure developments reduce private motor vehicle use, minimise traffic impacts and encourage sustainable transport.
- O12. To enhance the public domain, improve pedestrian experience and safety, and limit the number of vehicle access points throughout the Precinct.
- O13. To reduce private vehicle ownership through unbundled parking, car share schemes and decoupled parking, where on-site car parking is provided.
- O14. To maximise efficient use of non-residential car parking by incorporating shared use of parking spaces subject to peak demand of various building uses.
- O15. To ensure development provides facilities for electric vehicles.
- O16. To future proof infrastructure to support increased take-up of electric vehicles.

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- O17. To ensure vehicle parking, servicing and loading areas are designed to:
  - a. reduce their visual impact on the public domain
  - b. support all vehicle types anticipated by development including service vehicles and loading areas
  - c. maximise potential adaptation at a future point in time when less parking is required.
- O18. To ensure delivery areas prioritise servicing outside peak pedestrian activity for key streets.
- O19. To ensure bike riders have sufficient accessible and secure parking.
- O20. To provide on-site workers facilities for employment generating uses that encourage active transport commuting, healthy workplaces and cater for worker needs.

**Controls**

- C11. Travel plans are to include the following:
  - a. baseline travel demand and mode share estimates from established similar developments
  - b. targets for reduced private motor vehicle trips and an increased mode share for sustainable transport
  - c. actions to be implemented to achieve the mode shift targets, with a written commitment from the property owner and/or business operator to implement them
  - d. a process for monitoring and review of actions and targets
  - e. a guide for residents, employees and visitors associated with the development to assist with the mode shift
  - f. public transport subsidies for workers for commuter and for-work trips and parking charges for workers who commute by car and/or payments to employees who don't
  - g. on-site carshare schemes and memberships, and priority parking for multiple occupancy vehicles, e.g. employees who car pool
  - h. subsidised bicycle purchase and quality bicycle parking and associated end-of-trip facilities
  - i. provision of peak period shuttle buses, relocation allowances and flexible working hours.
- C12. Vehicular access is located to:
  - a. use secondary streets or rear accessways and laneways
  - b. consolidate vehicle access to reduce the number of crossovers through a maximum of one driveway per site or one-way pair.

Note: Refer to additional Controls relevant to specific Areas within the Precinct in Sections 9.50.4 and 9.50.5.

- C13. Provision of private vehicle parking:
  - a. is listed on a separate title (unbundled) from the development (i.e. separated from dwelling, commercial units and building ownership)
  - b. is decoupled from the development, as relevant
  - c. includes car share vehicle(s) that:
    - i. are located either on-site or on the street at the discretion of Council
    - ii. do not result in the maximum car parking rates being exceeded
    - iii. are publicly available and readily accessible at all times.

Notes:

- 1. **Unbundled parking** means parking that is separated from the cost or rent of a dwelling, commercial units and building ownership.

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2. **Car share scheme** means a scheme in which any car share operator provides vehicles for shared use and hires those vehicles exclusively to members of the scheme for occasional use for short periods of time, on demand and on a pay-as-you go basis.
  3. **Decoupled parking** means provision of off-site car parking, usually in the form of consolidated car parking in close proximity to the development to satisfy the parking requirements.
- C14. Where shared use of car parking spaces is included, they are determined on a case-by-case basis dependant on anticipated tenancies/uses.
- C15. Provide electric vehicle (EV) ready to use (including cabling, power outlet or charging head) car parking spaces:
- a. for non-residential development – Level 3, or faster, at a rate of 10% for all spaces – dedicated and visitor
  - b. for residential development – Level 1, or faster, at a rate of:
    - i. 20% for resident spaces
    - ii. 10% for visitor spaces.
- C16. Design electric infrastructure services (distributions boards, conduits and cables) to ensure:
- a. sufficient energy and capacity, preferably from renewable sources
  - b. reticulated fixed charging facilities cater for a minimum of:
    - i. for non-residential development – 50% of all parking spaces
    - ii. for residential development – 100% of all parking spaces.
  - c. any future EV charger does not require a cable of more than 50m from the parking space to the EV-ready connection.
- C17. On-site ground level exposed car parking is not provided, and parking areas:
- a. are concentrated below ground or sleeved by other uses
  - b. are not open structures that are visible from the public domain
  - c. where below ground:
    - i. do not protrude:
      - above ground level at any point along street frontages
      - into setbacks areas that are identified as landscape areas.
    - ii. are designed to facilitate break out walls where required
  - d. do not impede the provision of viable vegetation
  - e. are designed to accommodate all vehicle types anticipated by the development
  - f. provide sufficient manoeuvring space to allow vehicles to enter and exit the site in a forward direction
  - g. are designed in a manner that encourage opportunities for adaptation to other uses over time.
- C18. Service delivery times are restricted to avoid conflict with peak pedestrian periods on key streets, where practical.
- C19. Bicycle parking:
- a. complies with the minimum requirements detailed in Table 1
  - b. is in accessible and visible locations for residents, workers and visitors
  - c. is secure through provision of bike cages for residents and workers or bike stands for visitors
  - d. is provided with ready-to-use electric charging points at a minimum rate of:
    - i. for non-residential development – 1 per 4 bicycle spaces

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- ii. for residential development – 1 per 2 bicycle spaces
- iii. where there are multiple parking areas, facilities are distributed equally across all locations.

**Table 1: Minimum bicycle parking**

Land Use	Resident/Worker	Visitor
Residential	1 space per dwelling	1 space per 10 dwellings
Commercial	1 space per 150m <sup>2</sup> GFA	1 space per 400m <sup>2</sup> GFA
Retail	1 space per 250m <sup>2</sup> GFA	2 spaces + 1 per 100m <sup>2</sup> GFA
Industrial	1 per 250m <sup>2</sup> GFA	1 space per 500m <sup>2</sup> GFA

C20. On-site workers facilities:

- a. comply with the minimum requirements detailed in Table 2
- b. are in secure locations
- c. where more than 1 shower/change cubicle is required, separate and equal numbers of male and female facilities are provided

**Table 2: Minimum worker facilities for all employment generating uses**

Anticipated number of workers	Personal Lockers	Showers and change cubicles
0-49	1 per 2 workers	1 unisex
50 - 99	1 per 3 workers	2
100-199	1 per 4 workers	4
200+	1 per 5 workers	+ 1 per 200 workers

**9.50.3.10. Heritage**

**Objectives**

O21. To ensure development:

- a. respects the significance of Heritage Items in the locality
- b. in the vicinity of Heritage Items is designed and sited to minimise impacts on the significance of the item.

**Controls**

C21. To ensure development responds to historic built form in the locality by:

- a. for Heritage Items – conserving and enhancing the significance, character, fabric and features of these buildings and conforming with the Burra Charter
- b. for all other buildings – respects the items by:
  - i. appropriately siting and designing new development
  - ii. ensuring new development does not physically overwhelm or dominate the items

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- iii. using sympathetic materials, colours and finishes that reflect and harmonise with original materials to maintain the character of the items.

**9.50.3.11. Active street frontages**

**Objectives**

- O22. Active street frontages are provided to reinforce the vitality and liveliness of the public domain.
- O23. Active street frontages are provided:
  - a. with ground floor frontages being pedestrian orientated and of a high design quality to add vitality to streets
  - b. by incorporating frequent pedestrian entries that open towards the street.

**Controls**

- C22. Provide active street frontages by including the following uses at street level:
  - a. shops, commercial premises and other employment uses
  - b. commercial and residential lobbies and reception areas
  - c. public buildings or community facilities.
- C23. Active street frontages contribute to the liveliness and vitality of streets by:
  - a. providing a minimum of 70% of the ground floor frontage as transparent glazing with an unobstructed view from the adjacent footpath to at least a depth of 6m within the building
  - b. maximising entries, display windows, customer service areas and key activities to provide pedestrian interest and interaction
  - c. minimising blank walls, fire escapes, service doors, plant and equipment hatches
  - d. providing elements of visual interest, such as display cases, or creative use of materials where fire escapes, service doors and equipment hatches cannot be avoided
  - e. providing a high standard of finish and appropriate level of architectural detail for building facades
  - f. providing passive surveillance to enhance safety and security
  - g. providing ground floor pedestrian entry at the same level as the street to maximise accessibility for all users
  - h. not including driveways and service entries
  - i. if including security measures, using grilles or screens that are fitted internally and are a minimum of 60% perforated/transparent when closed.

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9.50.3.12. Built form

Objectives

- O24. To provide for a high-quality and well-designed built form that:
  - a. strengthens the urban character and identity of the Precinct
  - b. supports intended land uses
  - c. promotes a positive image for businesses
  - d. is of a bulk and scale and has a site layout that complements the local context
  - e. minimises adverse amenity impacts
  - f. enhances the public domain for pedestrians
  - g. incorporates lighting that contributes to the quality and safety of the night-time urban environment, is sustainable and easy to maintain
  - h. does not create nuisance or hazard from glare, noise and odour for pedestrians, motorists, or occupants of nearby buildings.
- O25. To ensure development for residential purposes achieves a high-quality living environment and mitigates urban hazards by taking an integrated and innovative approach to:
  - a. address road and aircraft noise, and air quality impacts
  - b. the orientation of development and individual dwellings
  - c. minimise the need for mechanical ventilation and heating or cooling
  - d. protect the amenity of nearby residential developments.
- O26. To provide appropriate employment uses on the ground floor in mixed-use developments that:
  - a. are compatible with the residential uses above
  - b. are separated from residential uses through subdivision
  - c. safeguard the provision and viability of business uses
  - d. provide large floor plates and high ceilings to ensure functionality and flexibility in accommodating a diverse range of business uses.

Controls

- C24. Building design:
  - a. includes architectural features and façade articulation to reduce apparent building bulk
  - b. emphasises building corners at intersections
  - c. does not result in overshadowing or loss of privacy
  - d. locates pedestrian entries:
    - i. on the primary street frontage and visible from the street
    - ii. at the same level as the street to maximise accessibility for all users
  - e. where incorporating external lighting it:
    - i. is integrated into the building design and highlights distinctive architectural features
    - ii. is energy efficient, high quality, durable and low maintenance
    - iii. does not cause nuisance or hazard to occupants of the building or nearby buildings
    - iv. minimises light spill into the night sky
    - v. supports street lighting to enhance safety and security
  - f. negates adverse noise and odour emissions from activities, plant or equipment.
- C25. Residential development results in comfortable and enjoyable internal environments through:
  - a. meeting the required standards for residential development near busy roads
  - b. ensuring buildings are designed to achieve internal noise levels as detailed in AS 2021
  - c. using a variety of integrated built form design, construction techniques and acoustic solutions to ameliorate negative impacts including but not limited to:

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- i. materials and glazing choices
  - ii. angled walls and modulated surfaces
  - iii. solid balconies and winter gardens
  - iv. screens, louvres and hopper windows
  - v. locating single aspect dwellings away from the north and west street frontages
  - vi. incorporating light wells, atriums and internal articulation to enhance sun capture and air movement.
- d. retaining privacy and solar access while improving noise impacts for nearby residential developments.
- C26. Building design facilitates employment uses on the ground floor which:
- a. activate street frontage
  - b. provide suitable floor plates
    - i. limit ground floor use for services, storage and other business needs, and where required locate these to the rear of the building
    - ii. are larger in scale and designed to provide flexibility to adapt to different uses.
  - c. include a stratum subdivision scheme to delineate land use separation, ownership structures and obligations to the overall building regarding requiring owners' corporation consent for the submission of development applications and complying development certificates for employment uses separate from residential uses.

**9.50.3.13. Building materials and finishes**

**Objectives**

- O27. To provide building materials, fittings and finishes that are high-quality, sustainable and complement the locality.
- O28. To reduce building waste by effectively re-using or recycling building materials where demolition or deconstruction of existing development is required to facilitate new development.

**Controls**

- C27. Building materials, fittings and finishes:
- a. are durable, of high-quality and textured, to complement materials used in nearby buildings
  - b. on facades have a light reflectivity of 20% or less
  - c. are sustainable with low embodied carbon such as:
    - i. replacement of Portland cement with supplementary cementitious materials (SCMs) in concrete (i.e., 30% SCM across all pre-cast and in-situ cement)
    - ii. high recycled content in steel
    - iii. timber framing instead of steel framing
    - iv. cross laminate timber.
  - d. incorporate recycled materials, where possible.
- C28. The Deconstruction Plan demonstrates that the majority of demolished building materials, excluding hazardous materials, are integrated into the design and construction of development by re-using on-site or through appropriate recycling.

**9.50.3.14. Landscaping**

**Objectives**

- O29. To ensure on-site landscaping:
- a. includes species native to the area

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- b. is suited to the location
- c. provides habitat to enhance biodiversity
- d. positively contributes to water management and is waterwise
- e. contributes to mitigating urban heat
- f. is durable and low maintenance.

**Controls**

C29. Landscaping Strategy demonstrates that landscape:

- a. is provided in dedicated setbacks
- b. include:
  - i. water sensitive urban design solutions
  - ii. trees and supporting vegetation
  - iii. greening opportunities including green cover, green roofs, green walls, pergolas with climbers, podiums, planters, lawns and gardens, rain gardens and permeable pavements
  - iv. 50% native species.

**9.50.3.15. Views**

**Objectives**

O30. To reinforce view corridors and vistas with buildings, structures, public art or landscape treatments.

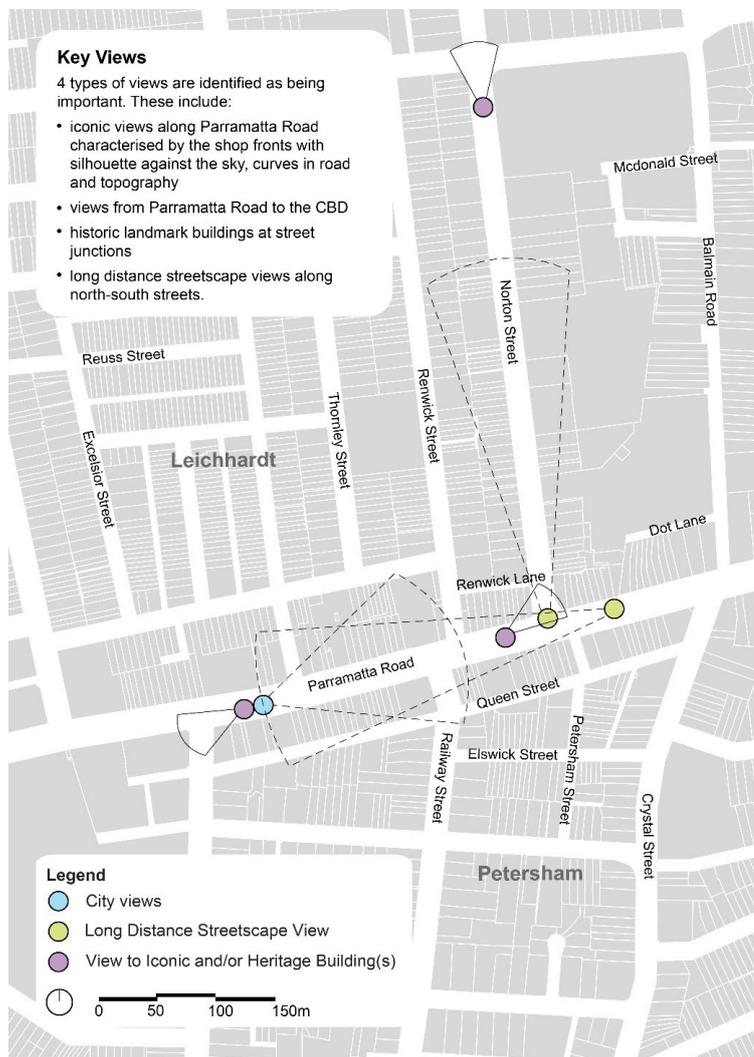
**Controls**

C30. Development maintains and, where possible, enhances views as identified in Figure 4: Leichhardt Precinct key views map:

- a. to the City skyline
- b. to landmark buildings
- c. to street vistas identified.

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Figure 4: Leichhardt Precinct key views



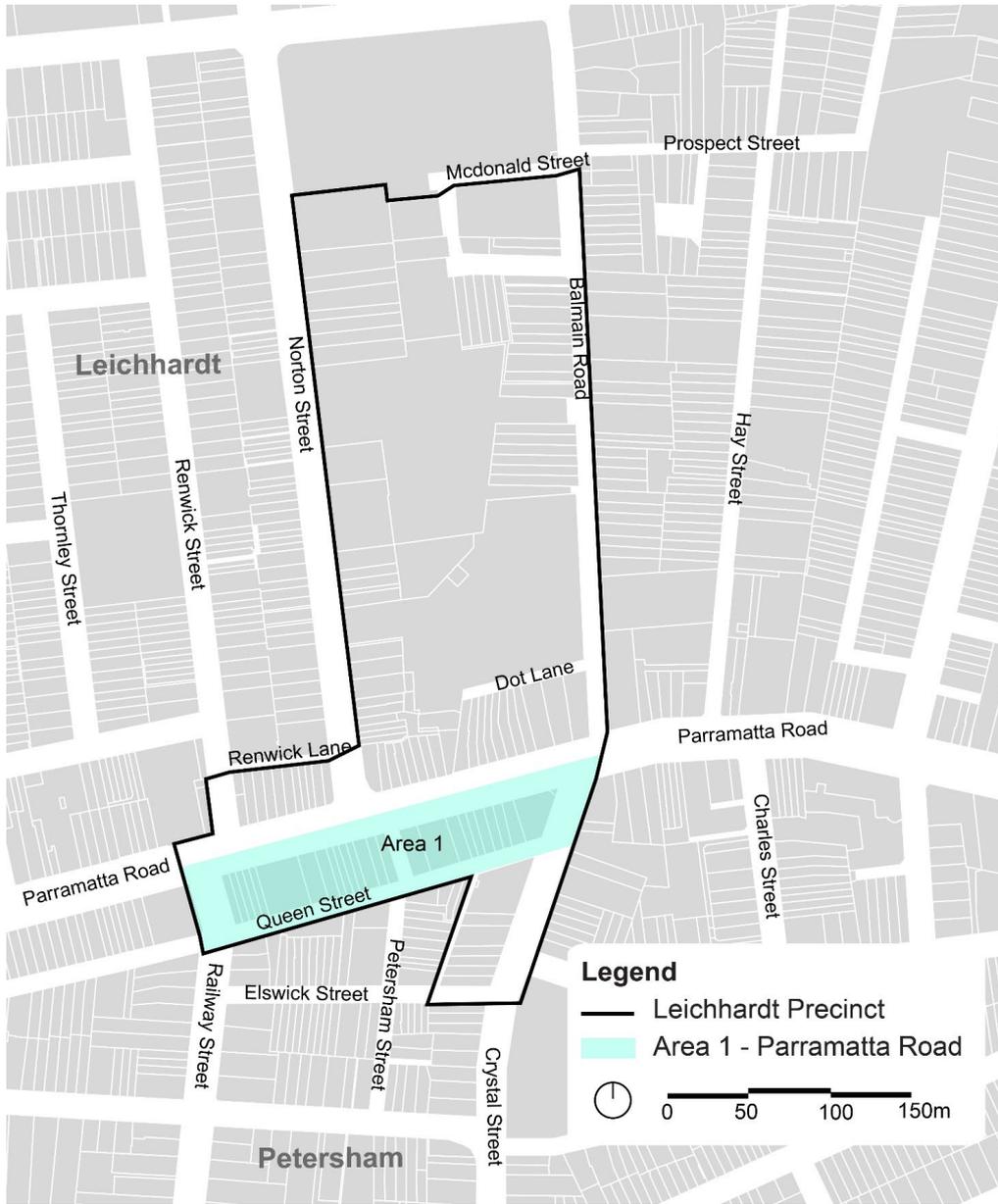
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9.50.4 Area 1 – Leichhardt: Parramatta Road

9.50.4.1. Application

Section 9.50.4 applies to Area 1 – Leichhardt: Parramatta Road as shown in Figure 5:

Figure 5: Area 1 – Leichhardt: Parramatta Road



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**9.50.4.2. Desired Future Character**

The Desired Future Character for Area 1 supplements and should be read in conjunction with the Desired Future Character Statement detailed in Section 9.50.3.2 for the Leichhardt Precinct.

Area 1 – Leichhardt: Parramatta Road:

- Continues as a productive economic corridor that attracts investment and new employment opportunities.
- Is reinvigorated as a retail/business high street in its appearance and function.
- Its heritage significance is protected and revitalised with new development that respected the original built form.
- Pedestrians and bike riders benefit from the new urban space, Petersham Street Park, that has enhanced connectivity between Parramatta Road and Queen Street.
- New built form:
  - is high quality
  - responds to and retains the heritage fabric and fine grain appearance of the area
  - is cohesive and presents a consistent street wall to Parramatta Road
  - positively interacts with the street
  - protects solar access, privacy and amenity of surrounding residential uses.

**9.50.4.3. Heritage**

**Objectives**

- O31. To ensure development responds to the historic built form of the location by:
- a. for Heritage Items – conserving and enhancing the significance, character, fabric and features of these buildings and conforming with the Burra Charter
  - b. for Contributory Buildings – restoring or reconstructing, altered or missing fabric of buildings
  - c. for all other buildings – being sympathetic to key architectural or streetscape features found in the Heritage Conservation Area (HCA)
  - d. not negatively impacting on Heritage Items outside Area 1 – Parramatta Road of the Leichhardt Precinct.

**Controls**

- C31. Development responds sensitively to heritage and proactively retains, restores and enhances the heritage features of the location by:
- a. demonstrating that achievement of the floor space ratio and height of buildings incentive provisions does not have an adverse impact on the Heritage Items or the HCA, including Contributory Buildings in the HCA
  - b. for Heritage Items – alterations to the existing fabric are limited to restoration
  - c. for Heritage Items and Contributory Buildings new built form:
    - i. is sympathetic and clearly distinguishable from the existing architecture
    - ii. complements the scale, form and materials of the streetscape and its desired future character including wall heights and roof forms
    - iii. pays particular attention to the transition from old to new and respects the existing façade and parapet in a way that ensures its architectural design remains a predominant feature
    - iv. retains existing openings, and no new openings are introduced into the façade, including the parapet

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- v. retains existing floor to floor heights and where new floor levels are introduced, these do not intersect with existing openings
- vi. evidences the original narrow fine grain width of shop fronts
- d. for all development:
  - i. retain the prominence of Heritage Items and landmark buildings in the immediate streetscape and surrounding area
  - ii. use sympathetic materials, colours and finishes to harmonise with the character of the HCA
  - iii. retain, or where required replace, suspended awnings to ensure consistency with adjoining and original fabric.

Notes:

1. Refer to Part 8 Heritage of this DCP for detailed controls and guidelines.
2. Relevant Architectural Style Sheets for Parramatta Road Commercial Precinct Heritage Conservation Area include:
  - a. Victorian Italianate/Victorian Filigree. Refer to Section 8.5.1.
  - b. Federation styles. Refer to Section 8.5.2.

**9.50.4.4. Lot amalgamation**

**Objectives**

- O32. To ensure development that relies on lot amalgamation results:
- a. in a built form character that retains the existing fine-grain appearance on Parramatta Road
  - b. in orderly and efficient land use.

**Controls**

- C32. Development that relies on lot amalgamation:
- a. evidences the original subdivision pattern in the resulting built form and shop front pattern
  - b. where basement levels are provided, the resulting lot is a minimum of 17m wide and retains a fine-grain built form appearance to Parramatta Road
  - c. does not isolate or prevent surrounding lots from redeveloping.

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Figure 6: Area 1 – Leichhardt: Parramatta Road Heritage Items and Contributory Buildings



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9.50.4.5. Built form

Objectives

- O33. To ensure building height:
  - a. facilitates economic growth and new housing
  - b. responds appropriately to the heritage character of Parramatta Road
  - c. protects the amenity of surrounding land uses
  - d. provides a consistent street wall to Parramatta Road that is suited to the street proportions and defines the street edge.
- O34. To ensure storey height:
  - a. at the ground and first storey, allows for a variety of uses and provides flexibility to cater for change over time
  - b. above ground, is suited to intended land uses
  - c. retains existing floor to floor heights for Heritage Items and Contributory Buildings.
- O35. To maintain a consistent setback to Parramatta Road and ensure new built form responds appropriately to the desired future streetscape.
- O36. To provide setbacks to Queen Street that:
  - a. support access for a range of vehicle types expected by the development
  - b. increase in depth aligned to building height to provide a built form transition, and ensure solar access, amenity and privacy to the surrounding residential properties.
- O37. To provide clearly defined and accessible business and residential lobbies and entries.

Controls

- C33. Building height:
  - a. does not exceed 6 storeys
  - b. has a street wall of 2 storeys to Parramatta Road
  - c. responds appropriately to Heritage Items through reduced height or transitioning heights to match the adjacent item.
- C34. Floor to floor height:
  - a. for Heritage Items or Contributory Buildings – ground floor and 2nd storey retain the existing floor to floor height
  - b. for other – aligns with adjoining Heritage Item or Contributory Building
  - c. for 3rd floor and above is 3.2m

Note: Ground level floor to floor height include a slope/topography allowance.
- C35. Parramatta Road setback:
  - a. zero
  - b. 3m from 3rd storey
  - c. an additional 6m for 6th storey
- C36. Queen Street setback:
  - a. for ground floor – zero
  - b. above ground floor – determined on a site-by-site basis by demonstrating that the development:
    - i. can achieve appropriate solar access and visual privacy
    - ii. will not impact the amenity, including solar access and visual privacy, of existing or future residential properties

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- iii. will enhance the casual surveillance and safety of Queen Street.

C37. Locate:

- a. the primary pedestrian access for ground floor employment uses on Parramatta Road
- b. residential pedestrian access on Queen Street or Parramatta Road.

Notes:

- 1. Figure 7 provides an indicative built form bulk, scale and site layout including storeys, street wall and setbacks.
- 2. Floor to floor heights include a slope/topography allowance. Variation to built form controls may be considered on site specific basis where the proposed development demonstrates achievement of the Precinct wide (9.50.3.2) and Area 1 (9.50.4.2) and Desired Future Character and Precinct wide (9.50.3.1) and Area 1 (9.50.4.5) Built form Objectives and delivers better outcomes.

**9.50.4.6. Vehicle and service access locations**

**Objective**

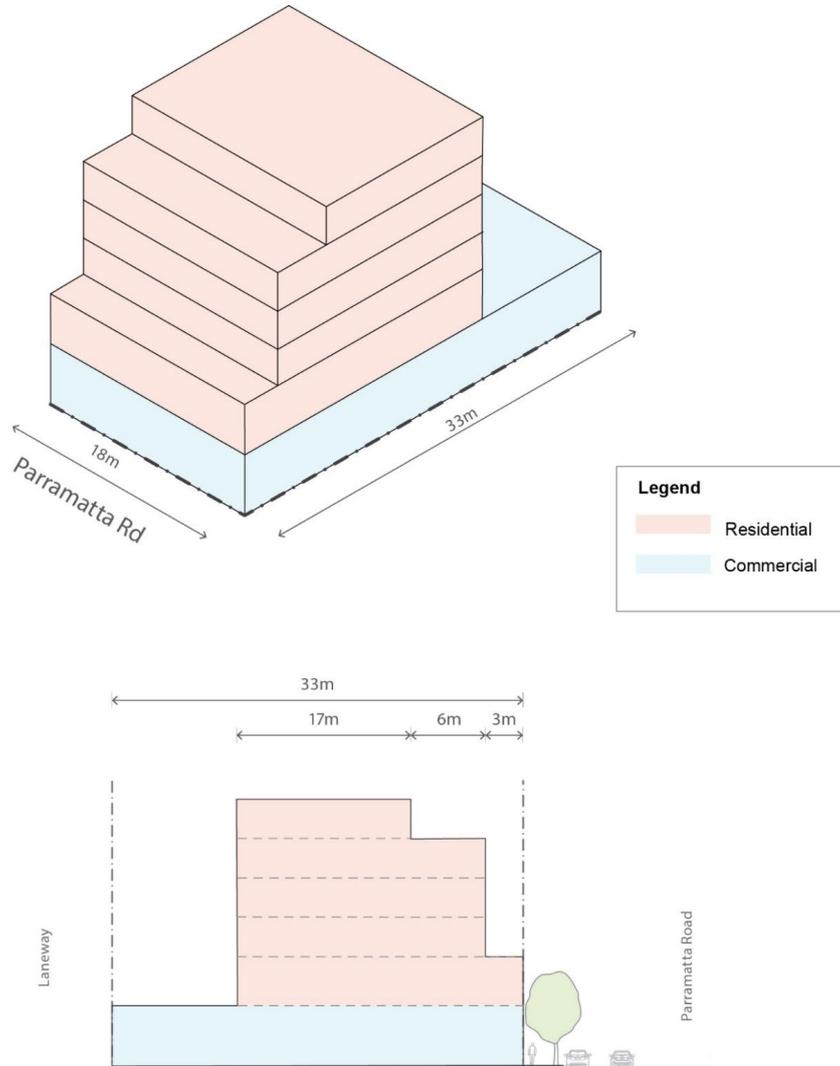
- O38. To ensure vehicle and service access is via secondary street maintaining the primary function of Parramatta Road.

**Control**

- C38. Vehicle and service access is provided from Queen Street.

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Figure 7: Indicative built form bulk and scale including storeys, floor heights, setbacks landscape areas and access – section, plan and axonometric views



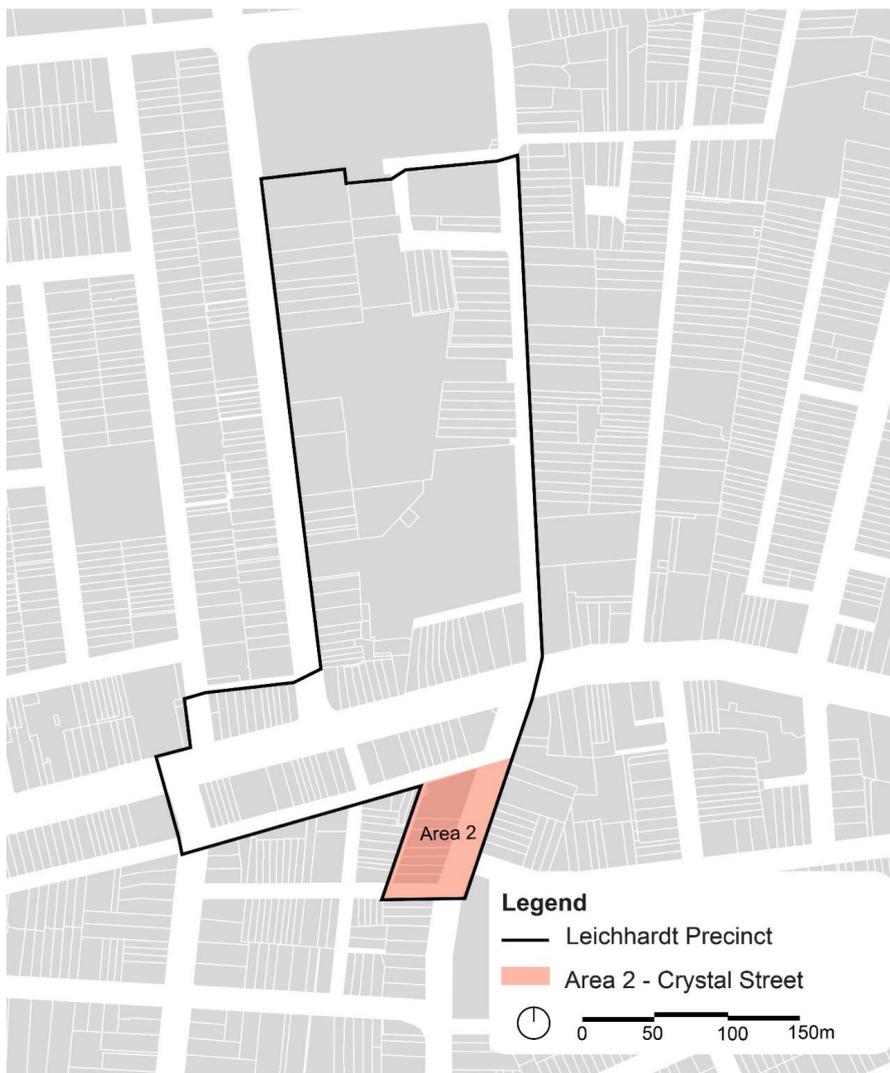
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**9.50.5 Area 2 – Leichhardt: Crystal Street**

**9.50.5.1. Application**

Section 9.50.5 applies to Area 2 – Leichhardt: Crystal Street as shown in Figure 8:

Figure 8: Area 2 – Leichhardt: Crystal Street



**9.50.5.2. Desired Future Character**

The Desired Future Character for Area 2 supplements and should be read in conjunction with the Desired Future Character Statement detailed in Section 50.9.3 for the Leichhardt Precinct.

Area 2 – Leichhardt: Crystal Street:

- Has delivered a suitable scaled transition from Parramatta Road that responds to the local context and complements the adjacent heritage Item.

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- Is supported by lot amalgamation that uses land efficiently, is suited to intended uses and has avoided lots being isolated from future redevelopment.
- Has increased residential densities and housing diversity in the location.
- Its built form:
  - is high quality architecture
  - is cohesive and presents a consistent street wall that defines Crystal Street
  - protects solar access, privacy and amenity of surrounding residential uses.
- Has provided consolidated vehicle access locations which create a safe walking and cycling environment.
- Enhanced public domain and streetscape through provision of landscaping in the front setbacks and new street trees increasing amenity and safety for all users.

**9.50.5.3. Lot amalgamation**

**Objectives**

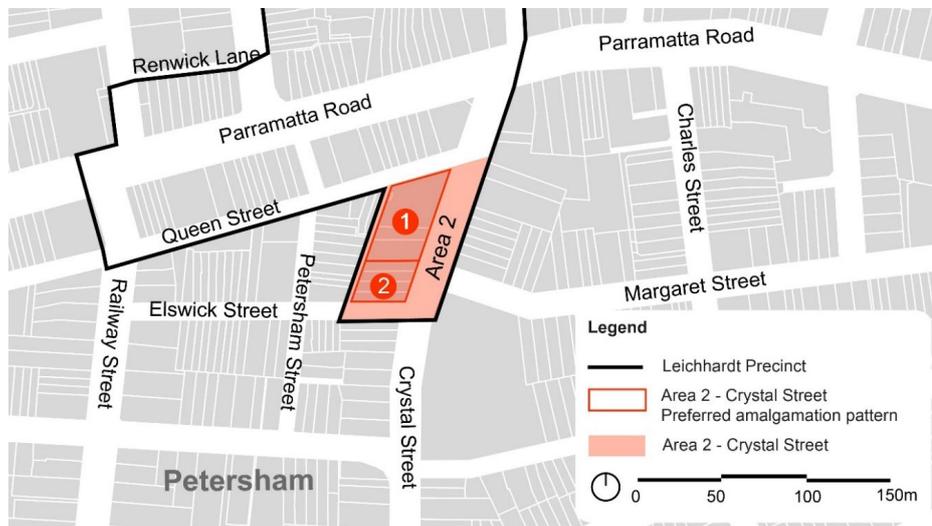
O39. To ensure lot amalgamation promotes the orderly redevelopment of land for intended residential uses and identified built form.

**Controls**

- C39. Lot amalgamation:
- a. aligns with Area 2 – Leichhardt: Crystal Street preferred lot amalgamation pattern, or
  - b. achieves the following criteria:
    - i. does not isolate lots from redevelopment
    - ii. achieves required landscape area and communal open space
    - iii. provides required setbacks
    - iv. consolidates vehicle access and reduces their impact on pedestrian movement paths
    - v. provides appropriate access for servicing and waste management
    - vi. facilitates basement parking where on-site parking is provided.

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Figure 9: Area 2 – Leichhardt: Crystal Street preferred lot amalgamation pattern



9.50.5.4. Built form

Objectives

- O40. To ensure building height:
  - a. is suited to intended uses, being residential flat buildings
  - b. provides an appropriate transition to development to the south and west
  - c. is sympathetic to the surrounding scale of low-density dwellings to the west
  - d. ensures solar access and amenity is maintained to surrounding residential developments.
- O41. To ensure storey height is suited to support residential uses.
- O42. To require setbacks that:
  - a. define the street edge
  - b. reduce the apparent bulk and scale of buildings
  - c. provide landscaped area within the front setback
  - d. facilitate the widening of Petersham Lane for public domain improvements, landscaping and passive surveillance.

Controls

- C40. Building height:
  - a. does not exceed the maximum building height and is equivalent to 5 storeys
  - b. has a street wall to Crystal Street, Queen Street and Petersham Lane of 4 storeys and an overall height of 5 storeys
  - c. has a street wall to Elswick Street of 5 storeys.
- C41. Ground level floor to floor height is 4m.  
Note: Ground level floor to floor height includes a slope/topography allowance.
- C42. First storey and above is 3.2m.
- C43. Setbacks:
  - a. to Crystal Street is 3m, with an additional 5m setback for the 5th storey

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- b. to Queen Street is 2m with an additional 5m setback to the 5th storey
- c. to Elswick Street is zero across all storeys
- d. to Petersham Lane is 5m, of which 3m is for widening of Petersham Lane, and an additional 5m setback to the fifth floor.

Notes:

- 1. Figure 10: provides the indicative–built form bulk, scale and site layout.
- 2. Variation to built form controls may be considered on site specific basis where the proposed development demonstrates achievement of the Precinct wide (9.50.3.2) and Area 2 (9.50.5.2) Desired Future Character and Precinct wide (9.50.3.11) and Area 2 (9.50.5.4) Built form Objectives and delivers better outcomes.

**9.50.5.5. Vehicle, service and pedestrian access locations**

**Objectives**

- O43. To minimise pedestrian/vehicle conflict along Crystal Street.
- O44. To provide clearly defined residential entries that are visible from the primary street frontage.

**Controls**

- C44. Vehicle and service access is from Queen Street or Elswick Street.
- C45. The primary pedestrian access is from Crystal Street.

**9.50.5.6. Landscaping**

**Objectives**

- O45. To mitigate heat island impacts and soften the hardscape of building elements.

**Controls**

- C46. Provide landscaping and greening opportunities through:
  - a. 3m landscaped setback along Crystal Street, including basement
  - b. new street trees along Crystal Street
  - c. new street trees and vegetation along Petersham Lane
  - d. ensuring that location of basement does not preclude opportunities for viable tree planting and greening.

**9.50.5.7. Petersham Lane public domain improvements**

**Objectives**

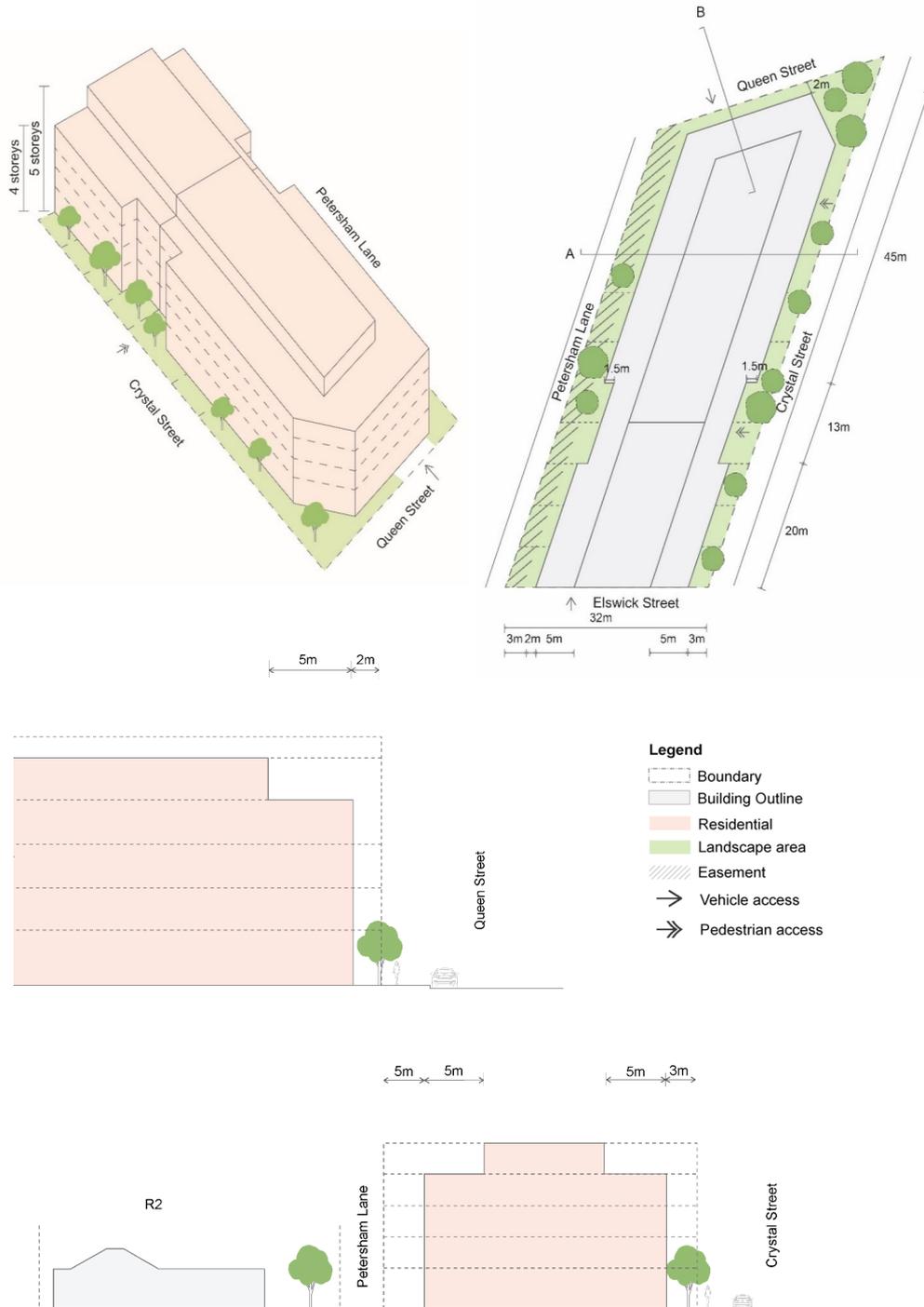
- O46. To improve the existing conditions of the laneway and provide enhanced public domain opportunities for landscaping and passive surveillance.

**Controls**

- C47. Provide an enhanced public domain through 5m setback to the built form including:
  - a. providing public access 24 hours a day, seven days a week by virtue of 3m easement to Council
  - b. construction of new footpath and associated landscaping for pedestrians
  - c. new landscaping and greening opportunities on the kerbside and within the site.

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Figure 10: Area 2 – Leichhardt: Crystal Street – indicative solution that achieves storeys, building envelopment, scale and site layout requirements – plan, sections and axonometric



## 13. Parramatta Road Corridor – Taverners Hill Precinct

### 13.1. Application

Chapter G, Section 13 Parramatta Road Corridor – Taverners Hill Precinct applies:

- to that part of Taverners Hill Precinct shown as Area 1 – Taverners Hill North: Tebbutt and Beeson Streets on **Figure 1: Parramatta Road Corridor – Taverners Hill Precinct Land Application Map**, and
- where development seeks to rely on the Incentives Floor Space Ratio Map, Incentives Height of Buildings Map and Clause XX of the Inner West LEP 2022.

Where development does not seek to rely on the Incentives provisions, Part G, Section 13 does not apply. In this circumstance, relevant provisions of this DCP apply.

Taverners Hill Precinct comprises of two Areas. As detailed above, this Section applies to Area 1 – Taverners Hill North: Tebbutt and Beeson Streets.

Where seeking to rely on Incentives provisions, all development will achieve the Desired Future Character, Objectives and Controls detailed in:

- **Section 13.3** that applies to all Areas in the Taverners Hill Precinct, and
- **Section 13.4** that applies to Area 1 – Taverners Hill North: Tebbutt and Beeson Streets.

These Sections supplement and should be read in conjunction with relevant provisions of this DCP.

Where Part D, Section 13 applies and there is an inconsistency between this Section and the other provisions of this DCP, this Section prevails.

**Figure 1: Parramatta Road Corridor – Taverners Hill Precinct Land Application Map**



## 13.2. Context

### Parramatta Road Corridor Urban Transformation Strategy

Parramatta Road Corridor – Taverners Hill Precinct is one of eight Precincts of the Parramatta Road Corridor Urban Transformation Strategy (PRUCS). PRUCS is the NSW Government’s 30-year plan setting out how the Parramatta Road Corridor will grow and bring new life to local communities living and working along the Corridor.

The vision for Parramatta Road Corridor is:

**A high-quality multi-use corridor with improved transport choices, better amenity and balanced growth of housing and jobs.**

The vision is supported by seven principles:

1. **Housing choice and affordability**  
Plan for diversity in housing types to accommodate a wide range of community needs, including affordable, family, student and seniors housing.
2. **Diverse and resilient economy**  
Plan for and position the corridor to attract new businesses and support existing business that create a diversity of jobs and promote jobs closer to home.
3. **Accessible and connected**  
Reshape and better connect places and movement networks to better serve customers and encourage sustainable travel.
4. **Vibrant community places**  
Promote quality places and built form outcomes to transform the corridor over time.
5. **Green spaces and links**  
Embellish existing open space and provide for new open spaces that support the recreational needs of the community and encourage active and healthy lifestyles.
6. **Sustainability and resilience**  
Create liveable local Precincts along the corridor that are sustainable, resilient and make Sydney a better place.
7. **Delivery**  
Deliver, drive, facilitate and monitor action.

### PRUCS and Inner West

Four of the eight PRUCS Precincts are within Inner West Council local government area. These include:

- Part of **Kings/ Croydon Bay Precinct** in Croydon that extends from Lang Street, Croydon in the west to Iron Cove Creek in the east. The remaining areas of Kings Bay Precinct are in Burwood and Canada Bay local government areas.
- **Taverners Hill Precinct** that extends from Petersham in the east to Summer Hill in the west and includes areas in Leichhardt and Lewisham. **Area 1 – Taverners Hill North: Tebbutt and Beeson Streets**, being the subject of this Section.
- **Leichhardt Precinct** in the suburbs of Leichhardt and Petersham.

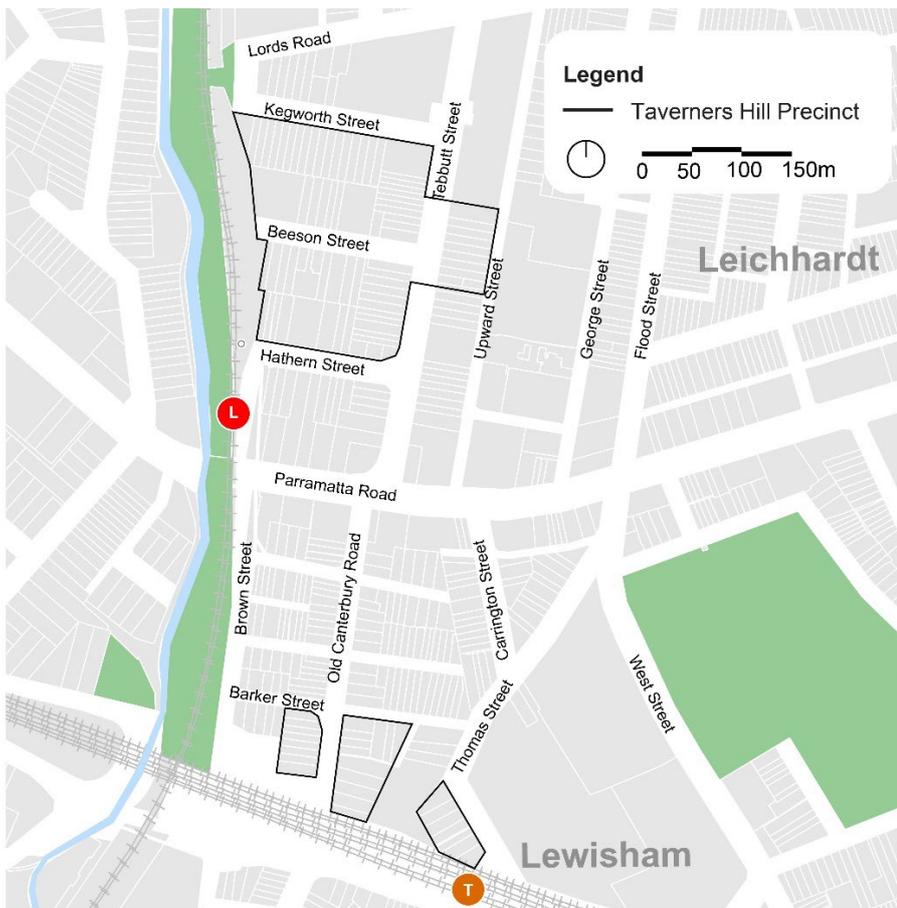
- Part of **Camperdown Precinct**. The remaining area of Camperdown Precinct is in City of Sydney local government area.

### 13.3. Taverners Hill Precinct

#### 13.3.1. Application

Section 13.3 applies to the entire Taverners Hill Precinct as identified in Figure 2.

**Figure 2: Taverners Hill Precinct**



**13.3.2. Desired future character**

Taverners Hill Precinct:

- Is strengthened by strategically located new housing that:
  - serves the needs of people of all ages, abilities and incomes
  - is well located to public transport and open space.
- People enjoy a public domain that:
  - is safe, well designed and landscaped
  - has increased access to nearby public open space and sports facilities, and the GreenWay linking the Bay Run in the north and Cooks River in the south.
- Lot amalgamation has optimised redevelopment opportunities and made efficient use of land.
- Living environments are sustainable and comfortable as a result of:
  - buildings having a high standard environmental performance
  - integrated water management
  - building design, landscape and materials reducing urban heat effects
  - good facilities for active transport and access to public transport
  - catering for electric charging infrastructure.
- Residents benefit from building design that maximises their amenity while protecting the amenity of nearby residential development.
- Development has respected the historic fabric and character of the area.
- Old Canterbury Road and Tebbutt Street form the primary north-south movement link between Market Place on Marion Street and Lewisham Station at Thomas Street.
- Reliance on private vehicles has reduced due to:
  - reducing on-site car parking provision
  - setting maximum car parking rates instead of requiring minimum car parking
  - implementing new models such as unbundled parking and shared car use
  - leveraging proximity to public transport networks, including Light Rail at Taverners Hill and Marion, proximity to Lewisham Station, multiple bus routes and rapid transport on dedicated lanes on Parramatta Road.

**13.3.3. Connectivity and accessibility**

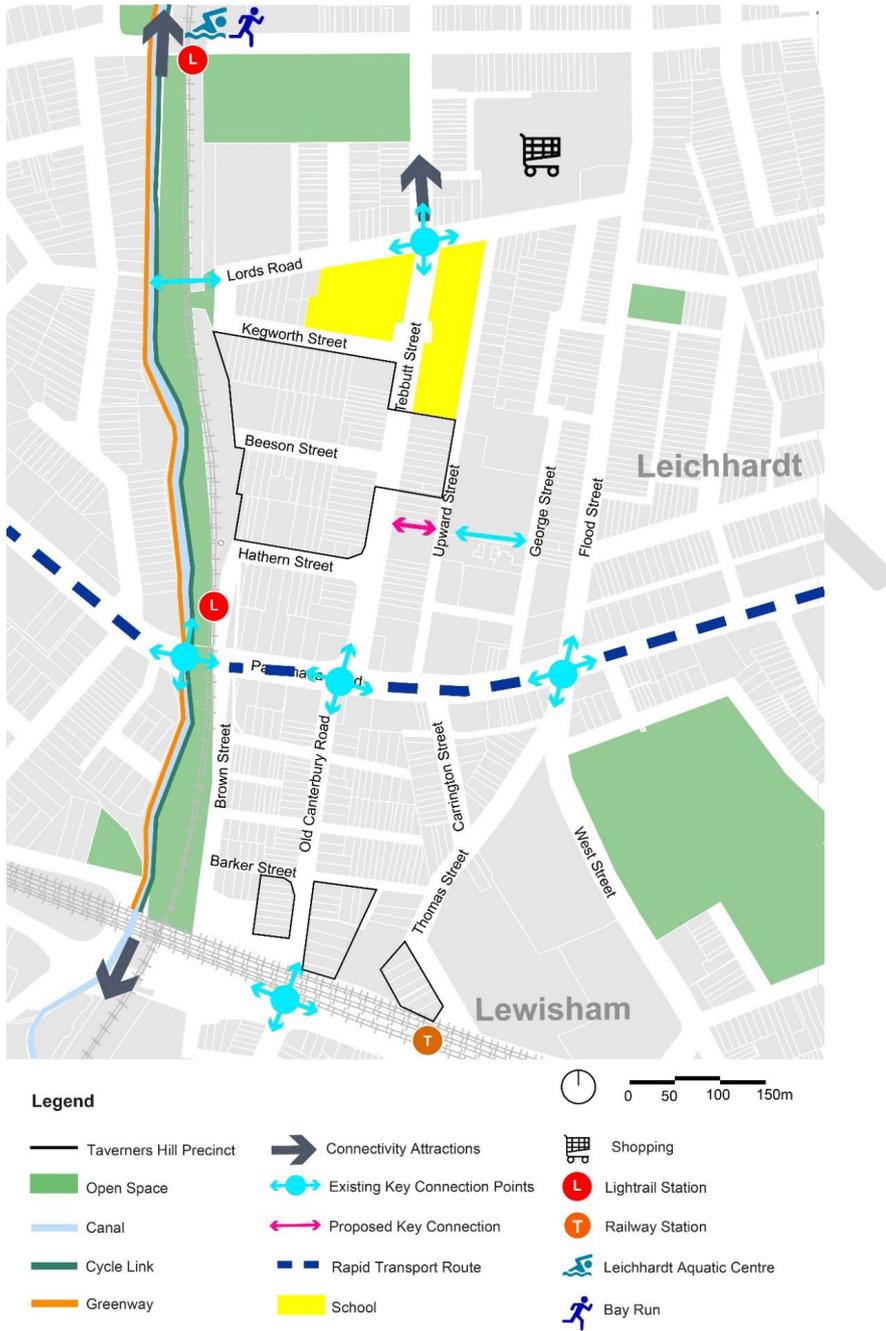
**Objectives**

- O1. To increase connectivity and accessibility across the Precinct for pedestrians and bike riders by enhancing links to key locations.

**Controls**

- C1. Built form and streetscape treatments reinforce the connections identified in Figure 3: Taverners Hill Precinct connectivity and accessibility map, including:
  - a. Parramatta Road and Marion Street
  - b. Taverners Hill and Marion Light Rail Stations
  - c. Parramatta River, the Bay Run and Leichhardt Aquatic Centre
  - d. Market Place and other local shopping areas
  - e. Hawthorne Canal GreenWay
  - f. Lewisham Station.

Figure 3: Taverners Hill Precinct Connectivity and Accessibility Map



**13.3.4. Streetscape and public domain**

**Objectives**

- O2. To improve the amenity and safety of the streetscape in a manner that:
  - a. contributes to the street character and intended land uses
  - b. ensures built form interfaces well with the streetscape
  - c. reduces street clutter and improves the visual amenity of the public domain
  - d. upgrades verges and pedestrian movement paths
  - e. contributes to mitigating urban heat through kerb-side landscaping
  - f. protects existing street trees, in the verge or roadway
  - g. positively contributes to water management and is waterwise
  - h. results in a durable and low maintenance public domain.

**Controls**

- C2. Development contributes towards enhancing the streetscape for the extent of the street frontage by:
  - a. providing a pedestrian movement path that is:
    - i. minimum 1.5m wide
    - ii. clear of obstacles
  - b. integrating pedestrian and vehicular entries into the streetscape design
  - c. providing a kerb-side permeable landscaped area for the remaining width of the verge that:
    - i. acts as a soft barrier between pedestrians and traffic lanes
    - ii. retains and protects existing street trees, in the verge or roadway, and includes new trees where appropriate
    - iii. includes low-level ground cover species that are hardy and suited to the location, can average 0.9m in height (excludes ground level mown grass) and are a minimum of 50% native
    - iv. includes water sensitive urban design solutions
    - v. does not impede walkers, bike riders and vehicles in traffic lanes or designated on-street parking and does not interfere with sight lines
    - vi. is designed to include, as required, public transport infrastructure, street signage and lighting
    - vii. upgrades the kerb to the required standard for the location, or
  - d. where the verge width does not allow for the required movement path and a separate landscaped area - design the movement path to integrate landscape treatments and/or street tree planting or provide landscape buildouts extending into the road reserve.

Note: Refer to *Inner West Public Domain Design Guide (202X)* for details of road types, footpath area functions and finishes.

### 13.3.5. Development utility infrastructure

#### Objectives

- O3. To reduce the street clutter, provide opportunity for viable street trees and enhance the public domain.
- O4. To locate and design mechanical plant and essential services in a way that:
  - a. improves the visual amenity of the public domain
  - b. does not conflict with landscaping or street tree planting
  - c. is located outside the public domain.

#### Controls

- C3. Relocate existing overhead cables underground, and where possible, co-locate with other underground services.
- C4. Mechanical plant and essential services equipment are:
  - a. contained within the property
  - b. located off the primary street frontage, or
  - c. where on the primary street frontage are located behind the building line and screened from view
  - d. integrated with the building and landscape design.

### 13.3.6. Lot amalgamation

#### Objectives

- O5. To promote efficient use of land and orderly redevelopment by:
  - a. avoiding isolating lots and reducing development potential
  - b. providing for intended uses and built form outcomes that make a positive contribution to the streetscape.

#### Controls

- C5. Lot amalgamation does not result in isolated lots that are impractical for redevelopment.

Note: Refer to additional controls in Section 13.4.3.

### 13.3.7. Sustainability and resilience

#### Objectives

- O6. To achieve a high standard of environmental building performance that:
  - a. reduces greenhouse gas emissions and water use
  - b. results in comfortable living environments.
- O7. To reduce urban heat island effect through incorporating and integrating a range of mechanisms that collectively mitigate impacts, including:
  - a. green infrastructure in the form of landscape and surface treatments that incorporate water storage and treatment while reducing water usage
  - b. trees that offer shade to built form, hard surfaces and vegetation

- c. building materials and colours that contribute to energy efficiency and thermal comfort, and minimise nuisance caused by glare or heat radiation.

**Controls**

- C6. Building Environmental Performance Report or BASIX certificate demonstrates that the development:
  - a. achieves a reduction in greenhouse gas emissions and water use
  - b. will result in a comfortable living environment
  - c. includes passive design features such as optimal orientation, increased insulation, effective shading, cross ventilation and lower solar absorptance on external surface finishes
  - d. optimises rooftop solar photovoltaic systems
  - e. achieves full electrification of utilities including cooking, heating and hot water (heat pumps)
  - f. achieves an average thermal performance of 7-star NatHERS
  - g. incorporates ceiling fans in bedrooms and living rooms.
- C7. Mitigate urban heat island effect by:
  - a. achieving required tree canopy through:
    - i. site layout maximising retention of existing mature trees
    - ii. including advanced containerised trees (greater than 200 litre) of a species that within 10 years will achieve 50% of their potential at maturity
    - iii. incorporating trees and vegetation across various storeys (roof tops, terraces, atriums, and the like), in addition to or where necessary, as an alternative to ground level planting
    - iv. a combination of the above that collectively achieve, or exceed, tree canopy requirements
  - b. integrating green roofs and walls as a component of the landscape and built form design specifically in northern and western facing locations
  - c. incorporating permeable surfaces, rain gardens, and other water sensitive measures in landscape treatments
  - d. using materials and colours that:
    - i. have a high solar reflectance index (lighter colours) on roofs, facades, glazing or ground surfaces subject to their purpose and aligned to orientation and exposure to sunlight
    - ii. where it may cause nuisance due to glare or reflection do not exceed 20% reflectivity (darker colours).

**13.3.8. Access and Parking**

**Objectives**

- O8. To ensure developments reduce private motor vehicle use, minimise traffic impacts and encourage sustainable transport.
- O9. To enhance the public domain, improve pedestrian experience and safety, and limit the number of vehicle access points throughout the Precinct.

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- O10. To reduce private vehicle ownership through unbundled parking, car share schemes and decoupled parking, where on-site car parking is provided.
- O11. To ensure development provides facilities for electric vehicles.
- O12. To future proof infrastructure to support increased take-up of electric vehicles.
- O13. To ensure vehicle parking and servicing areas are designed to:
  - a. reduce their visual impact on the public domain
  - b. support all vehicle types anticipated by development including service vehicles
  - c. maximise potential adaptation at a future point in time when less parking is required.
- O14. To ensure bike riders have sufficient, accessible and secure parking.

**Controls**

- C8. Travel plans are to include the following:
  - a. baseline travel demand and mode share estimates from established similar developments
  - b. targets for reduced private motor vehicle trips and an increased mode share for sustainable transport
  - c. actions to be implemented to achieve the mode shift targets, with a written commitment from the property owner to implement them
  - d. a process for monitoring and review of actions and targets
  - e. a guide for residents and visitors associated with the development to assist with the mode shift
  - f. on-site carshare schemes
  - g. subsidised bicycle purchase and quality bicycle parking and associated end-of-trip facilities
  - h. provision of peak period shuttle buses.
- C9. Vehicular access is located to:
  - a. reduce the number of access points, as far as practicable
  - b. consolidate vehicle access and reduce the number of crossovers to a maximum of one or one-way pair per site.
- C10. Provision of private vehicle parking:
  - a. is listed on a separate title (unbundled) from the development (i.e. separated from dwelling, commercial units and building ownership)
  - b. is decoupled from the development, as relevant
  - c. includes car share vehicle(s) that:
    - i. are located either on-site or on the street at the discretion of Council
    - ii. do not result in the maximum car parking rates being exceeded
    - iii. are publicly available and readily accessible at all times.

Note: **Unbundled parking** means parking that is separated from the cost or rent of a dwelling, commercial units and building ownership.

**Car share scheme** means a scheme in which any car share operator provides vehicles for shared use and hires those vehicles exclusively to members of the scheme for occasional use for short periods of time, on demand and on a pay-as-you go basis.

**Decoupled parking** means provision of off-site car parking, usually in the form of consolidated car parking in close proximity to the development to satisfy the parking requirements.

- C11. Provide Level 1 or faster electric vehicle (EV) ready to use (includes cabling, power outlet or charging head) car parking spaces, at a rate of:
  - a. 20% for resident spaces
  - b. 10% for visitor spaces.
- C12. Design electric infrastructure services (distribution boards, conduits and cabling) to ensure 100% of all parking spaces have:
  - a. sufficient energy and capacity, preferably from renewable sources
  - b. reticulated fixed charging facilities
  - c. any future EV charger does not require a cable of more than 50m from the parking space to the EV-ready connection.
- C13. On-site ground level exposed car parking is not provided, and parking areas:
  - a. are not open structures that are visible from the public domain
  - b. where below ground, do not protrude:
    - i. above ground level at any point along street frontages
    - ii. into setback areas that are identified as landscape areas
  - c. do not impede the provision of viable vegetation
  - d. provide sufficient manoeuvring space to allow vehicles to enter and exit the site in a forward direction
  - e. are designed in a manner that encourages opportunities for adaptation to other uses over time.
- C14. Bicycle parking:
  - a. is provided at the rate of:
    - i. for residents – 1 space per dwelling
    - ii. for visitors – 1 space per 10 dwellings
  - b. is in accessible and visible locations for residents and visitors
  - c. is secure through provision of bike cages for residents and bike stands for visitors
  - d. is provided with ready-to-use electric charging points at a minimum rate of 1 per 2 bicycle spaces
  - e. where there are multiple parking areas, facilities are distributed equally across all locations.

### 13.3.9. Heritage

#### Objectives

- O15. To ensure development:
  - a. respects the significance of Heritage Items in the locality
  - b. in the vicinity of Heritage Items is designed and sited to minimise impacts on the significance of the item.

#### Controls

- C15. To ensure development responds to historic built form in the locality by:

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- a. for Heritage Items – conserving and enhancing the significance, character, fabric and features of these buildings and conforming with the Burra Charter
- b. for all other buildings – respects the items by:
  - i. appropriately siting and designing new development
  - ii. ensuring new development does not physically overwhelm or dominate the items
  - iii. using sympathetic materials, colours and finishes that reflect and harmonise with original materials to maintain the character of the items.

**13.3.10. Built form**

**Objectives**

- O16. To provide a high-quality and well-designed built form that:
- a. supports intended uses
  - b. strengthens the residential character of the area
  - c. protects the amenity of nearby residential development
  - d. consolidates vehicle access locations
  - e. enhances the public domain for pedestrians
  - f. has clearly defined and accessible residential entries that are visible from the street
  - g. incorporates lighting that contributes to the quality and safety of the night-time residential environment
  - h. results in a high amenity internal living environment by taking an integrated and innovative approach to:
    - i. the orientation of development and individual dwellings
    - ii. maximise solar access and cross ventilation
    - iii. addressing road noise impacts.

**Controls**

- C16. Building design:
- a. includes architectural features and façade articulation to reduce apparent building bulk
  - b. retains privacy and solar access to nearby residential development
  - c. minimises vehicle crossovers
  - d. locates pedestrian entries:
    - i. on the primary street frontage so they are visible from the street
    - ii. at the same level as the street to maximise accessibility for all users, or
    - iii. where flood constraints limit at-grade entrances, suitable alternatives are explored to maximise visibility and street surveillance and ensure accessibility for all users
  - e. where incorporating external lighting it:
    - i. is integrated into the building design
    - ii. is energy efficient, high quality, durable and low maintenance
    - iii. does not cause nuisance or hazard to occupants of the building or nearby buildings
    - iv. minimises light spill into the night sky
    - v. supports street lighting to enhance safety and security

- f. results in comfortable and enjoyable internal environments through using a variety of integrated built form design, construction techniques and acoustic measures to ameliorate noise and other impacts including but not limited to:
  - i. materials and glazing
  - ii. angled walls and modulated surfaces
  - iii. solid balconies and winter gardens
  - iv. screens, louvers and hopper windows
  - v. locating single aspect dwellings away from the north and west street frontages
  - vi. incorporating light wells, atriums and building articulation to enhance solar access and air movement.

**13.3.11. Building materials and finishes**

**Objectives**

- O17. To provide building materials, fittings and finishes that are high quality, sustainable and complement the locality.
- O18. To reduce building waste by effectively re-using or recycling building materials where demolition or deconstruction of existing structures is required to facilitate development.

**Controls**

- C17. Building materials, fittings and finishes:
  - a. are durable, high-quality, textured and complement materials used in the locality
  - b. are sustainable with low embodied carbon such as:
    - i. replacement of Portland cement with supplementary cementitious materials (SCMs) in concrete (i.e., 30% SCM across all pre-cast and in-situ cement)
    - ii. high recycled content in steel
    - iii. timber framing instead of steel framing
    - iv. cross laminate timber
  - c. incorporate recycled materials, where possible.
- C18. The Deconstruction Plan demonstrates that the majority of demolished building materials, excluding hazardous materials, are integrated into the design and construction of development by re-using on site or through appropriate recycling.

### 13.3.12. Landscaping

#### Objectives

- O19. To ensure on-site landscaping:
- a. includes species native to the area
  - b. is suited to the location
  - c. provides habitat to enhance biodiversity
  - d. positively contributes to water management and is waterwise
  - e. contributes to mitigating urban heat
  - f. is durable and low maintenance.

#### Controls

- C19. Landscaping Strategy demonstrates that landscape:
- a. is provided in dedicated setbacks
  - b. includes:
    - i. water sensitive urban design solutions
    - ii. trees and supporting vegetation
    - iii. greening opportunities including green roofs and walls, pergolas with climbers, podiums, planters, lawns and gardens, rain gardens and permeable pavement
    - iv. 50% native species.

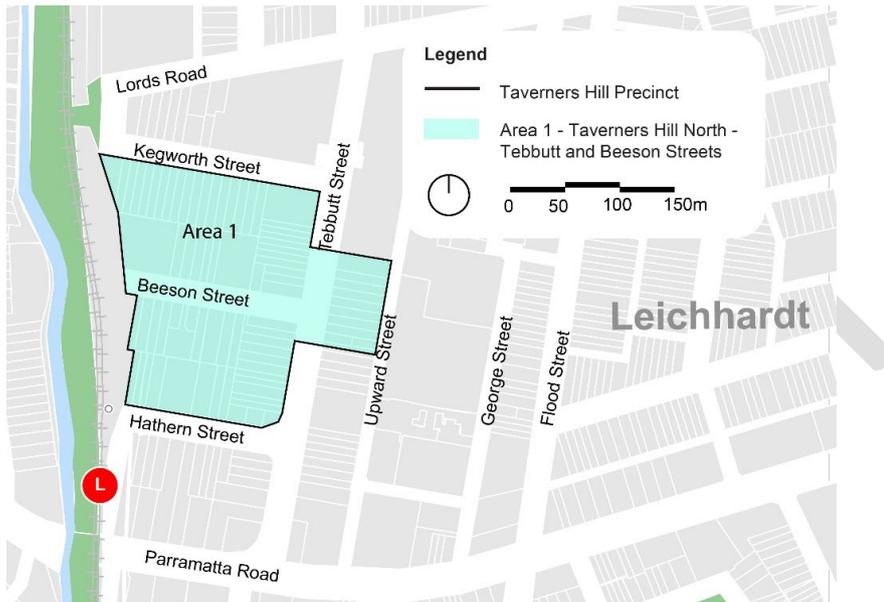
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### 13.4. Area 1 – Taverners Hill North: Tebbutt and Beeson Streets

#### 13.4.1. Application

Section 13.4 applies to Area 1 – Taverners Hill North: Tebbutt and Beeson Streets as shown in Figure 4.

**Figure 4:** Area 1 – Taverners Hill North: Tebbutt and Beeson Streets



#### 13.4.2. Desired future character

The Desired future character for Area 1 supplements and should be read in conjunction with the Desired Future Character Statement detailed in Section 13.3 for the Taverners Hill Precinct.

Area 1 – Taverners Hill North: Tebbutt and Beeson Streets:

- Has delivered high quality and suitably scaled residential development that responds to the local context.
- Has increased residential densities and housing diversity in the location.
- Is supported by lot amalgamation suited to intended uses and has avoided lots being isolated from redevelopment opportunities
- Enhanced public domain and streetscape increases amenity and safety for all users.
- Residents benefit from building design that maximises their amenity while protecting the amenity of nearby residential developments.
- The landscape character of the area is maintained and enhanced through:
  - retention of mature trees on-site and in the public domain, as much as possible
  - incorporating landscaped front and rear gardens.

### 13.4.3. Lot amalgamation

#### Objectives

O20. To ensure lot amalgamation promotes the orderly redevelopment of land for intended residential uses and identified built form.

#### Controls

C20. Lot amalgamation:

- a. results in a lot size of 720m<sup>2</sup> and has a primary street frontage width of 20m, or
- b. meets the following criteria:
  - i. does not isolate surrounding lots from redevelopment
  - ii. achieves required landscape areas and communal open space
  - iii. provides required setbacks
  - iv. consolidates vehicle access and reduces their impact on pedestrian movement paths
  - v. provides appropriate access for servicing and waste management
  - vi. facilitates basement parking, where on-site parking is provided.

### 13.4.4. Built form

#### Objectives

O21. To deliver the preferred building type and align building height, design and layout to suit:
 

- a. the local context and protect the amenity of nearby residential development
- b. lot pattern and depth, street frontage and access arrangements.

#### Controls

C21. Built form complies with the number of storeys, building envelope, scale and site layout requirements detailed in Table 1.

### 13.4.5. Landscaping

#### Objectives

O22. To maintain and enhance the landscaped and leafy character of the area.

#### Controls

C22. Maintain and enhance the landscape character by:

- a. providing a landscaping in the required front, side and rear setbacks
- b. minimising driveways and crossovers
- c. retaining existing on-site vegetation, especially mature trees, as much as possible.

Note: Landscaping requirements should be read in conjunction with Section 13.3.2 Streetscape and public domain, 13.3.7 Sustainability and resilience and 13.3.12 Landscaping.

**Table 1:** Built form – storeys, building envelope, scale and site layout requirements

Criteria/Location	Kegworth, Tebbutt and Beeson Streets	Beeson, Tebbutt and Hathern Streets	Tebbutt and Upward Streets
Maximum storeys	3-storey	4-storey	6-storey
Minimum floor to floor	First storey, ground floor – 4m (including topography allowance) Second storey and above – 3.2m		
Minimum front setback to primary street frontage	4m		4m to Tebbutt Street
Street wall	2-storey	3-storey	4-storey street wall to Tebbutt Street only
Minimum above ground front setback to primary street frontage	Additional 3m for the 3 <sup>rd</sup> storey	Additional 3m for the 4 <sup>th</sup> storey	Additional 3m for 5 <sup>th</sup> and 6 <sup>th</sup> storey fronting Tebbutt Street
Minimum rear setback	11m		13.5m to Upward Street
Minimum above ground setback to secondary street frontage	Not applicable		Additional 10m for 5 <sup>th</sup> and 6 <sup>th</sup> storey fronting Upward Street
Minimum side setback	3m		3m and additional 1.5m for 5 <sup>th</sup> and 6 <sup>th</sup> storey

Note: Variation to built form controls may be considered on site-specific basis where the proposed development demonstrates achievement of the Precinct wide (13.3.2) and Area 1 (13.4.2) Desired Future Character and Precinct wide (13.3.10) and Area 1 (13.4.4) Built form Objectives and delivers better outcomes.

## 9.49. Parramatta Road Corridor – Taverners Hill Precinct

### 9.49.1. Application

Part 9 Strategic Context, Section 9.49 Parramatta Road Corridor – Taverners Hill Precinct applies:

- to that part of Taverners Hill Precinct shown as Area 2 – Taverners Hill South: Old Canterbury Road and Thomas Street on **Figure 1: Parramatta Road Corridor – Taverners Hill Precinct Land Application Map**, and
- where development seeks to rely on the Incentives Floor Space Ratio Map, Incentives Height of Buildings Map and Clause X.X of the Inner West LEP 2022.

Where development does not seek to rely on the Incentives provisions, Part 9, Section 9.49 does not apply. In this circumstance, relevant provisions of this DCP apply.

Taverners Hill Precinct comprises of two Areas. As detailed above, this Section applies to Area 2 – Taverners Hill South: Old Canterbury Road and Thomas Street.

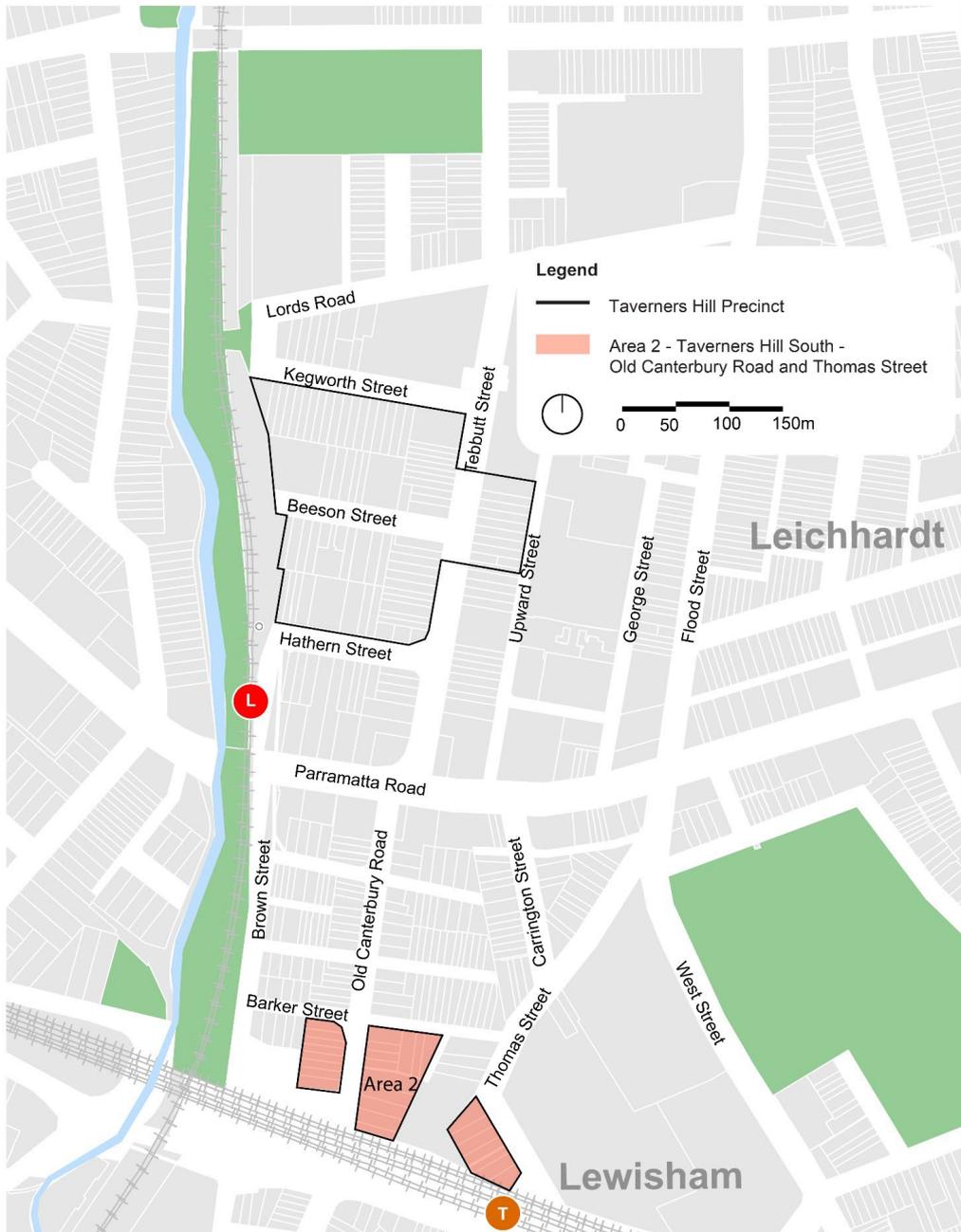
Where seeking to rely on Incentives provisions, all development will achieve the Desired Future Character, Objectives and Controls detailed in:

- **Section 9.49.3** that applies to all Areas in the Taverners Hill Precinct, and
- **Section 9.49.4** that applies to Area 2 – Taverners Hill South: Old Canterbury Road and Thomas Street.

These Sections supplement and should be read in conjunction with relevant provisions of this DCP.

Where Part 9, Section 9.49 applies and there is an inconsistency between this Section and other provisions of this DCP, this Section prevails.

Figure 1: Parramatta Road Corridor – Taverners Hill Precinct Land Application Map



## 9.49.2. Context

### Parramatta Road Corridor Urban Transformation Strategy

Parramatta Road Corridor – Taverners Hill Precinct is one of eight Precincts of the Parramatta Road Corridor Urban Transformation Strategy (PRUCS). PRUCS is the NSW Government’s 30-year plan setting out how the Parramatta Road Corridor will grow and bring new life to local communities living and working along the Corridor.

The vision for Parramatta Road Corridor is:

**A high-quality multi-use corridor with improved transport choices, better amenity and balanced growth of housing and jobs.**

The vision is supported by seven principles:

1. **Housing choice and affordability**  
Plan for diversity in housing types to accommodate a wide range of community needs, including affordable, family, student and seniors housing.
2. **Diverse and resilient economy**  
Plan for and position the corridor to attract new businesses and support existing business that create a diversity of jobs and promote jobs closer to home.
3. **Accessible and connected**  
Reshape and better connect places and movement networks to better serve customers and encourage sustainable travel.
4. **Vibrant community places**  
Promote quality places and built form outcomes to transform the corridor over time.
5. **Green spaces and links**  
Embellish existing open space and provide for new open spaces that support the recreational needs of the community and encourage active and healthy lifestyles.
6. **Sustainability and resilience**  
Create liveable local Precincts along the corridor that are sustainable, resilient and make Sydney a better place.
7. **Delivery**  
Deliver, drive, facilitate and monitor action.

### PRUCS and Inner West

Four of the eight PRUCS Precincts are within Inner West Council local government area. These include:

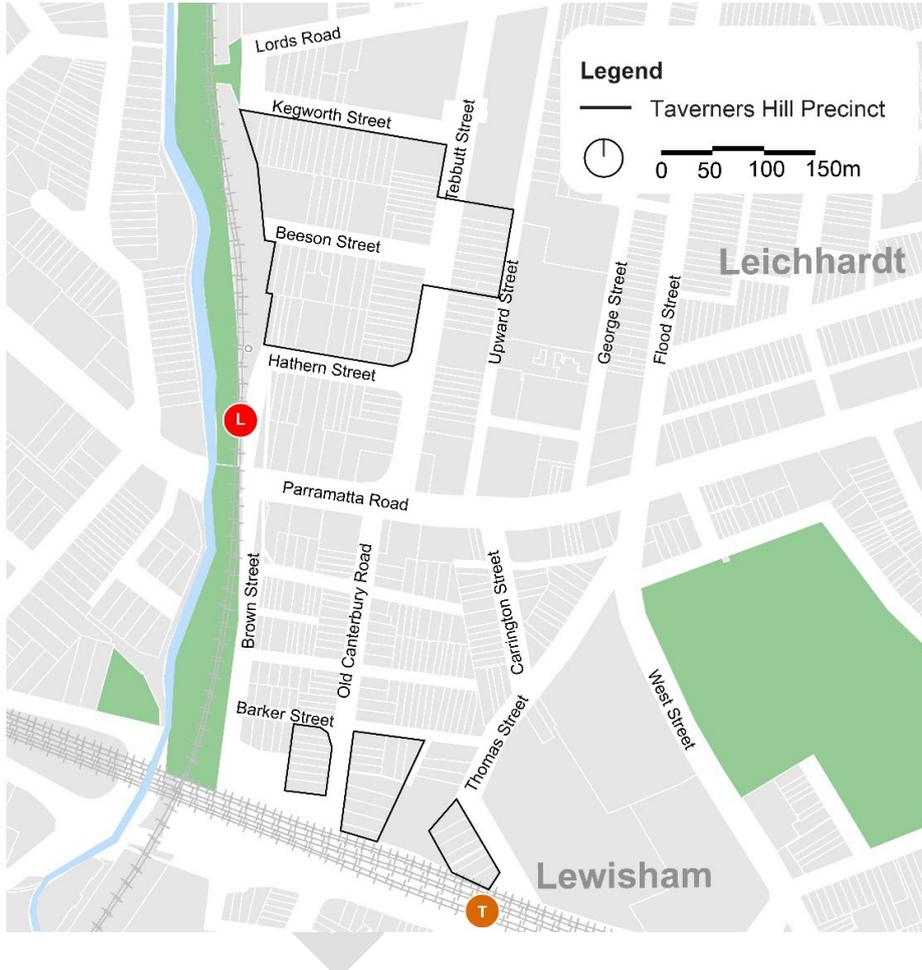
- Part of **Kings Bay/ Croydon Precinct** in Croydon. The remaining areas of Kings Bay/ Croydon Precinct are in Burwood and Canada Bay local government areas.
- **Taverners Hill Precinct** that extends from Petersham in the east to Summer Hill in the west and includes areas in Leichhardt and Lewisham. **Area 2 –Taverners Hill South: Old Canterbury Road and Thomas Street**, being the subject of this Section.
- **Leichhardt Precinct** in the suburbs of Leichhardt and Petersham.
- Part of **Camperdown Precinct**. The remaining area of Camperdown Precinct is in City of Sydney local government area.

9.49.3. Taverners Hill Precinct

9.49.3.1. Application

Section 9.49.3 applies to the entire Taverners Hill Precinct as identified in Figure 2.

Figure 2: Taverners Hill Precinct



### 9.49.3.2. Desired future character

Taverners Hill Precinct:

- Is strengthened by strategically located new housing that:
  - serves the needs of people of all ages, abilities and incomes
  - is well located to public transport and open space.
- People enjoy a public domain that:
  - is safe, well design and landscaped
  - has increased access to nearby public open space and sports facilities, and the GreenWay linking the Bay Run in the north and Cooks River in the south.
- Lot amalgamation has optimised redevelopment opportunities and made efficient use of land.
- Living environments are sustainable and comfortable as a result of:
  - buildings having a high standard environmental performance
  - integrated water management
  - building design, landscape and materials reducing urban heat effects
  - good facilities for active transport and access to public transport
  - catering for electric charging infrastructure.
- Residents benefit from building design that maximises their amenity while protecting the amenity of nearby residential development.
- Development has respected the historic fabric and character of the area.
- Old Canterbury Road and Tebbutt Street form a north-south movement link between Market Place on Marion Street and Lewisham Station at Thomas Street.
- Reliance on private vehicles has reduced due to:
  - reducing on-site car parking provision
  - setting maximum car parking rates instead of requiring minimum car parking
  - implementing new models such as unbundled parking and shared car use
  - leveraging proximity to public transport networks, including Light Rail at Taverners Hill and Marion, proximity to Lewisham Station, multiple bus routes and rapid transport on dedicated lanes on Parramatta Road.

### 9.49.3.3. Connectivity and accessibility

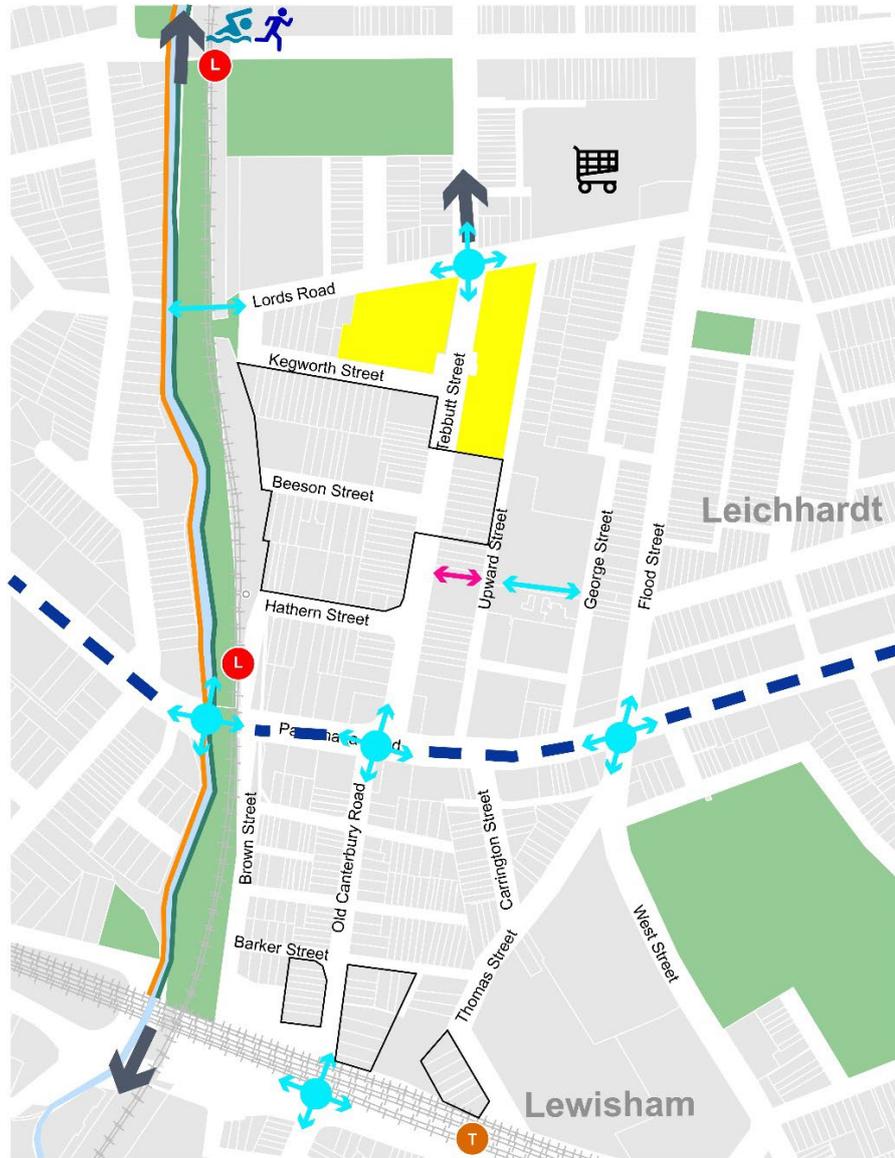
#### Objectives

- O1. To increase connectivity and accessibility across the Precinct for pedestrians and bike riders by enhancing links to key locations.

#### Controls

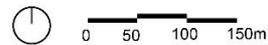
- C1. Built form and streetscape treatments reinforce the connections identified in Figure 3: Taverners Hill Precinct connectivity and accessibility map, including:
  - a. Parramatta Road and Marion Street
  - b. Taverners Hill and Marion Light Rail Stations
  - c. Parramatta River, The Bay Run and Leichhardt Aquatic Centre
  - d. Market Place and other local shopping areas
  - e. Hawthorne Canal GreenWay
  - f. Lewisham Station.

Figure 3: Taverners Hill Precinct connectivity and accessibility map



Legend

- |                         |                                |                           |
|-------------------------|--------------------------------|---------------------------|
| Taverners Hill Precinct | Connectivity Attractions       | Shopping                  |
| Open Space              | Existing Key Connection Points | Lightrail Station         |
| Canal                   | Proposed Key Connection Points | Railway Station           |
| Cycle Link              | Rapid Transport Route          | Leichhardt Aquatic Centre |
| Greenway                | School                         | Bay Run                   |



## 9.49.3.4. Streetscape and public domain

### Objectives

- O2. To improve the amenity and safety of the streetscape in a manner that:
- a. contributes to the street character and intended land uses
  - b. ensures built form interfaces well with the streetscape
  - c. reduces street clutter and improves the visual amenity of the public domain
  - d. upgrades verges and pedestrian movement paths
  - e. contributes to mitigating urban heat through kerb-side landscaping
  - f. protects existing street trees, in the verge or roadway
  - g. positively contributes to water management and is waterwise
  - h. results in a durable and low maintenance public domain.

### Controls

- C2. Development contributes towards enhancing the streetscape for the extent of the street frontage by:
- a. providing a pedestrian movement path that is:
    - i. a minimum of 1.5m wide
    - ii. clear of obstacles
  - b. integrating pedestrian and vehicular entries into the streetscape design
  - c. providing a kerb-side permeable landscaped area for the remaining width of the verge that:
    - i. acts as a soft barrier between pedestrians and traffic lanes
    - ii. retains and protects existing street trees, in the verge or roadway, and includes new trees where appropriate
    - iii. includes low-level ground cover species that are hardy and suited to the location, can average 0.9m in height (excludes ground level mown grass) and are a minimum of 50% native
    - iv. includes water sensitive urban design solutions
    - v. does not impede walkers, bike riders and vehicles in traffic lanes or designated on-street parking and does not interfere with sight lines
    - vi. is designed to include, as required, public transport infrastructure, street signage and lighting
    - vii. upgrades the kerb to the required standard for the location, or
  - d. where the verge width does not allow for the required movement path width and a separate landscaped area that is sufficient to sustain vegetation – design the movement path to integrate landscape treatments and/or street tree planting or provide landscape buildouts extending into the road reserve.

Note: Refer to *Inner West Public Domain Design Guide (202X)* for details of road types, footpath area functions and finishes.

**9.49.3.5. Development utility infrastructure**

**Objectives**

- O3. To reduce the street clutter, provide opportunity for viable street trees and enhance the public domain.
- O4. To locate and design mechanical plant and essential services in a way that:
  - a. improves the visual amenity of the public domain
  - b. does not conflict with landscaping or street tree planting
  - c. is located outside the public domain.

**Controls**

- C3. Relocate existing overhead cables underground, and where possible, co-locate with other underground services.
- C4. Mechanical plant and essential services equipment are:
  - a. contained within the property
  - b. located off the primary street frontage, or
  - c. where on the primary street frontage are located behind the building line and screened from view
  - d. integrated with the building and landscape design.

**9.49.3.6. Lot amalgamation**

**Objectives**

- O5. To promote efficient use of land and orderly redevelopment by:
  - a. avoiding isolating lots and reducing development potential
  - b. providing for intended uses and built form outcomes that make a positive contribution to the streetscape.

**Controls**

- C5. Lot amalgamation does not result in isolated lots that are impractical for redevelopment.

Note: Refer to additional controls in Section 9.49.4.3.

**9.49.3.7. Sustainability and resilience**

**Objectives**

- O6. To achieve a high standard of environmental building performance that:
  - a. reduces greenhouse gas emissions and water use
  - b. will result in comfortable living environments.
- O7. To reduce urban heat island effect through incorporating and integrating a range of mechanisms that collectively mitigate impacts, including:
  - a. green infrastructure in the form of landscape and surface treatments that incorporate water storage and treatment while reducing water usage
  - b. trees that offer shade to built form, hard surfaces and vegetation
  - c. building materials and colours that contribute to energy efficiency and thermal comfort, and minimise nuisance caused by glare or heat radiation.

**Controls**

- C6. Building Environmental Performance Report or BASIX certificate demonstrates that the development:
  - a. achieves a reduction in greenhouse gas emissions and water use
  - b. will result in a comfortable living environment
  - c. includes passive design features such as optimal orientation, increased insulation, effective shading, cross ventilation and lower solar absorptance on external surface finishes
  - d. optimises rooftop solar photovoltaic systems
  - e. achieves full electrification of utilities including cooking, heating and hot water (heat pumps)
  - f. achieves an average thermal performance of 7-star NatHERS
  - g. incorporates ceiling fans in bedrooms and living rooms.
  
- C7. Mitigate urban heat island effect by:
  - a. achieving required tree canopy through:
    - i. site layout maximising retention of existing mature trees
    - ii. including advanced containerised trees (great than 200 litre) of a species that within 10 years will achieve 50% of their potential at maturity
    - iii. incorporating trees and vegetation across various storeys (roof tops, terraces, atriums and the like), in addition to or where necessary, as an alternative to ground level planting
    - iv. a combination of the above that collectively achieve, or exceed, tree canopy requirements
  - b. integrating green roofs and walls as a component of the landscape and built form design specifically in northern and western facing locations
  - c. incorporating permeable surfaces, rain gardens, and other water sensitive measures in landscape treatments
  - d. using materials and colours that:
    - i. have a high solar reflectance index (lighter colours) on roofs, facades, glazing or ground surfaces subject to their purpose and aligned to orientation and exposure to sunlight
    - ii. where it may cause nuisance due to glare or reflection do not exceed 20% reflectivity (darker colours).

**9.49.3.8. Access and Parking**

**Objectives**

- O8. To ensure developments reduce private motor vehicle use, minimise traffic impacts and encourage sustainable transport.
- O9. To enhance the public domain, improve pedestrian experience and safety, and limit the number of vehicle access points throughout the Precinct.
- O10. To reduce private vehicle ownership through unbundled parking, car share schemes and decoupled parking, where on-site car parking is provided.
- O11. To ensure development provides facilities for electric vehicles.

- O12. To future proof infrastructure to support increased take-up of electric vehicles.
- O13. To ensure vehicle parking and servicing areas are designed to:
  - a. reduce their visual impact on the public domain
  - b. support all vehicle types anticipated by development including service vehicles
  - c. maximise potential adaptation at a future point in time when less parking is required.
- O14. To ensure bike riders have sufficient, accessible and secure parking.

**Controls**

- C8. Travel plans are to include the following:
  - a. baseline travel demand and mode share estimates from established similar developments
  - b. targets for reduced private motor vehicle trips and an increased mode share for sustainable transport
  - c. actions to be implemented to achieve the mode shift targets, with a written commitment from the property owner to implement them
  - d. a process for monitoring and review of actions and targets
  - e. a guide for residents and visitors associated with the development to assist with the mode shift
  - f. on-site carshare schemes
  - g. subsidised bicycle purchase and quality bicycle parking and associated end-of-trip facilities
  - h. provision of peak period shuttle buses.
- C9. Vehicular access is located to:
  - a. reduce the number of access points, as far as practicable
  - b. consolidate vehicle access and reduce the number of crossovers to a maximum of one or one-way pair per site.
- C10. Provision of private vehicle parking:
  - a. is listed on a separate title (unbundled) from the development (i.e. separated from dwelling, commercial units and building ownership)
  - b. is decoupled from the development, as relevant
  - c. includes car share vehicle(s) that:
    - i. are located either on-site or on the street at the discretion of Council
    - ii. do not result in the maximum car parking rates being exceeded
    - iii. are publicly available and readily accessible at all times.

Note: **Unbundled parking** means parking that is separated from the cost or rent of a dwelling, commercial units and building ownership.

**Car share scheme** means a scheme in which any car share operator provides vehicles for shared use and hires those vehicles exclusively to members of the scheme for occasional use for short periods of time, on demand and on a pay-as-you go basis.

**Decoupled parking** means provision of off-site car parking, usually in the form of consolidated car parking in close proximity to the development to satisfy the parking requirements.

- C11. Provide Level 1 or faster electric vehicle (EV) ready to use (includes cabling, power outlet or charging head) parking spaces, at a rate of:

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- a. 20% for resident spaces
  - b. 10% for visitor spaces.
- C12. Design electric infrastructure services (distribution boards, conduits and cabling) to ensure 100% of all parking spaces have:
- a. sufficient energy and capacity, preferably from renewable sources
  - b. reticulated fixed charging facilities
  - c. any future EV charger does not require a cable of more than 50m from the parking space to the EV-ready connection.
- C13. On-site ground level exposed car parking is not provided, and parking areas:
- a. are not open structures that are visible from the public domain
  - b. where below ground, do not protrude:
    - i. above ground level at any point along street frontages
    - ii. into setback areas that are identified as landscape area
  - c. do not impede the provision of viable vegetation
  - d. provide sufficient manoeuvring space to allow vehicles to enter and exit the site in a forward direction
  - e. are designed in a manner that encourages opportunities for adaptation to other uses over time.
- C14. Bicycle parking:
- a. is provided at the rate of:
    - i. for residents – 1 space per dwelling
    - ii. for visitors – 1 space per 10 dwellings
  - b. is in accessible and visible locations for residents and visitors
  - c. is secure through provision of bike cages for residents and bike stands for visitors
  - d. is provided with ready-to-use electric charging points at a minimum rate of one per two bicycle spaces
  - e. where there are multiple parking areas, facilities and distributed equally across all locations.

### 9.49.3.9. Heritage

#### Objectives

- O15. To ensure development:
- a. respects the significance of the Heritage Items and Heritage Conservation Areas
  - b. in the vicinity of Heritage Items and Heritage Conservation Areas is designed and sited to minimise impacts on the significance of the item or area.

#### Controls

- C15. To ensure development responds to the Heritage Conservation Area and Heritage Items by:
- a. for Heritage Items – conserving and enhancing the significance, character, fabric and features of these buildings and conforming with the Burra Charter
  - b. for all other buildings – respects the Heritage Conservation Area and Heritage Items by:
    - i. appropriately siting and designing new development
    - ii. ensuring new development does not physically overwhelm or dominate the heritage significance of items or area

- iii. using sympathetic materials, colours and finishes that reflect and harmonise with original materials to maintain the character of the items or area.

**9.49.3.10. Built form**

**Objectives**

- O16. To provide a high-quality building design that:
  - a. supports intended uses
  - b. strengthens the residential character of the area
  - c. protects the amenity of nearby residential development
  - d. consolidates vehicle access locations
  - e. enhances the public domain for pedestrians
  - f. has clearly defined and accessible residential entries that are visible from the street
  - g. incorporates lighting that contributes to the quality and safety of the night-time residential environment
  - h. results in a high amenity internal living environment by taking an integrated and innovative approach to:
    - i. the orientation of development and individual dwellings
    - ii. maximising solar access and cross ventilation
    - iii. addressing road and rail noise impacts.

**Controls**

- C16. Building design:
  - a. includes architectural features and façade articulation to reduce apparent building bulk
  - b. retains privacy and solar access to nearby residential development
  - c. minimises vehicle crossovers
  - d. locates entries:
    - i. on the primary street frontage so that it is visible from the street
    - ii. at the same level as the street to maximise accessibility for all users
  - e. where incorporating external lighting it:
    - i. is integrated into the building design
    - ii. is energy efficient, high quality, durable and low maintenance
    - iii. does not cause nuisance or hazard to occupants of the building or nearby buildings
    - iv. minimises light spill into the night sky
    - v. supports street lighting to enhance safety and security
  - f. results in comfortable and enjoyable internal environments through using a variety of integrated built form design, construction techniques and acoustic measures to ameliorate noise and other impacts including but not limited to:
    - i. materials and glazing
    - ii. angled walls and modulated surfaces
    - iii. solid balconies and winter gardens
    - iv. screens, louvers and hopper windows
    - v. locating single aspect dwellings away from the north and west street frontages
    - vi. incorporating light wells, atriums and building articulation to enhance solar access and air movement.

**9.49.3.11. Building materials and finishes**

**Objectives**

- O17. To provide building materials, fittings and finishes that are high quality, sustainable, complement the locality and where adjoining the railway line are protected from effects of electrolysis.
- O18. To reduce building waste by effectively re-using or recycling building materials where demolition or deconstruction of existing structures is required to facilitate development.

**Controls**

- C17. Building materials, fittings and finishes:
  - a. are durable, high-quality, textured and complement materials used in the locality
  - b. structurally address potential impacts of electrolysis resulting from proximity to the electric railway line
  - c. are sustainable with low embodied carbon such as:
    - i. replacement of Portland cement with supplementary cementitious materials (SCMs) in concrete (i.e., 30% SCM across all pre-cast and in-situ cement)
    - ii. high recycled content in steel
    - iii. timber framing instead of steel framing
    - iv. cross laminate timber
  - d. incorporate recycled materials, where possible.
- C18. The Deconstruction Plan demonstrates that the majority of demolished building materials, excluding hazardous materials, are integrated into the design and construction of development by re-using on-site or through appropriate recycling.

**9.49.3.12. Landscaping**

**Objectives**

- O19. To ensure on-site landscaping:
  - a. includes species native to the area
  - b. is suited to the location
  - c. provides habitat to enhance biodiversity
  - d. positively contributes to water management and is waterwise
  - e. contributes to mitigating urban heat
  - f. is durable and low maintenance.

**Controls**

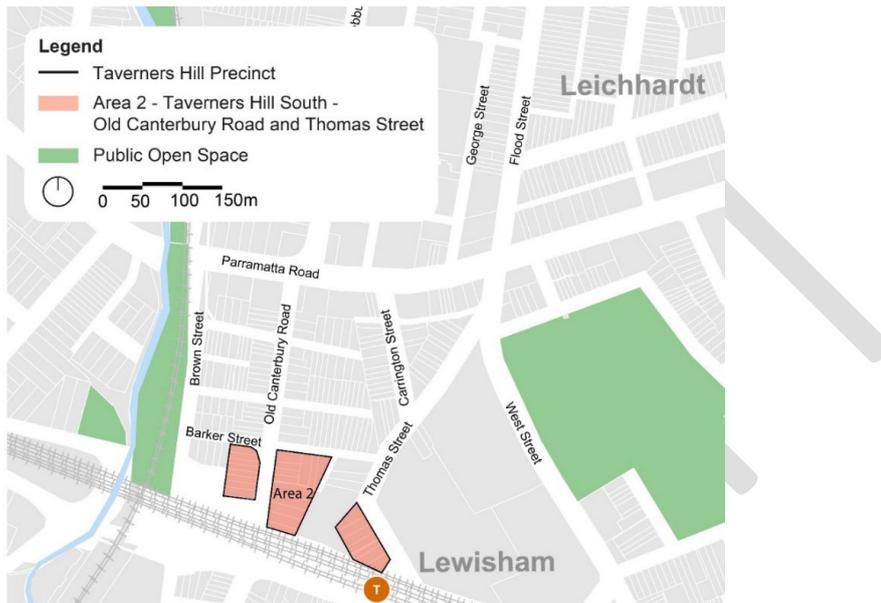
- C19. Landscaping Strategy demonstrates that landscape:
  - a. is provided in dedicated setbacks
  - b. includes:
    - i. water sensitive urban design solutions
    - ii. trees and supporting vegetation
    - iii. greening opportunities including green roofs and walls, pergolas with climbers, podiums, planters, lawns and gardens, rain gardens and permeable pavement
    - iv. 50% native species.

### 9.49.4. Area 2 – Taverners Hill South: Old Canterbury Road and Thomas Street

#### 9.49.4.1. Application

Section 9.49.4 applies to Area 2 - Taverners Hill South: Old Canterbury Road and Thomas Street as shown in Figure 4.

Figure 4: Area 2 - Taverners Hill South: Old Canterbury Road and Thomas Street



#### 9.49.4.2. Desired future character

The Desired future character for Area 2 supplements and should be read in conjunction with the Desired Future Character Statement detailed in Section 9.49.3 for the Taverners Hill Precinct.

Area 2 - Taverners Hill South: Old Canterbury Road and Thomas Street:

- Has delivered high quality and suitably scaled residential development that responds to the local context.
- Has increased residential densities and housing diversity in the location.
- Is supported by lot amalgamation suited to intended uses and has avoided lots being isolated from redevelopment opportunities.
- Enhanced public domain and streetscape increases amenity and safety for all users.
- Residents benefit from building design that maximises their amenity while protecting the amenity of nearby residential developments.
- The landscape character of the area is maintained and enhanced through:
  - retention of mature trees on-site and in the public domain, as much as possible
  - incorporating landscaped front and rear gardens.

9.49.4.3. Lot amalgamation

Objectives

O20. To ensure lot amalgamation promotes the orderly redevelopment of land for intended residential uses and identified built form.

Controls

C20. Lot amalgamation:

- a. aligns to Figure 5: Area 2 – Taverners Hill South preferred lot amalgamation pattern, or
- b. where a. is not achievable:
  - i. results in a lot size of 720m<sup>2</sup> and has a street frontage of 20m, or
  - ii. meets the following criteria:
    - does not isolate surrounding lots from redevelopment
    - achieves required landscape areas and communal open space
    - provides required setbacks
    - consolidates vehicle access and reduces their impact on pedestrian movement paths
    - provides appropriate access for servicing and waste management
    - facilitates basement parking, where on-site parking is provided.

Figure 5: Area 2 - Taverners Hill South preferred lot amalgamation pattern



9.49.4.4. Built form

Objectives

- O21. To deliver the preferred building type and align building height, design and layout to suit:
- the local context and protect the amenity of nearby residential development
  - lot pattern and depth, street frontage and access arrangements.

Controls

- C21. Built form complies with the number of storeys, building envelope, scale and site layout requirements detailed in Table 1.

**Table 1: Built form – storeys, building envelope, scale and site layout requirements**

Criteria/Location	Old Canterbury Road West and Barker Street East	Old Canterbury Road East	Thomas Street
Refer to:		Figure 6: Northern site and Figure 7 – Southern site	Figure 8
Maximum storeys	3-storey	5-storey	4-storey
Minimum floor to floor	First storey, ground floor – 4m (including topography allowance) Second storey and above – 3.2m		
Minimum front setback to primary street frontage	4m	3m for public domain improvements and additional 3m to front building line	6m
Street wall	2-storey		
Minimum above ground front setback to primary street frontage	Additional 3m for 3 <sup>rd</sup> storey	Additional 3m for 3 <sup>rd</sup> , 4 <sup>th</sup> and 5 <sup>th</sup> storey	Additional 3m for 3 <sup>rd</sup> and 4 <sup>th</sup> storey
Minimum rear setback	7m	6m	From 4m increasing to 12m
Minimum side setbacks	3m	Northern site – 6m to Barker Street and 3m to southern boundary Southern site – 3m to northern boundary 5m to southern boundary	6m
Minimum above ground side setback	Not applicable	Additional 3m for 3 <sup>rd</sup> , 4 <sup>th</sup> and 5 <sup>th</sup> storey	Additional 3m for 3 <sup>rd</sup> and 4 <sup>th</sup> storey
Vehicle access location	Old Canterbury Road or	Northern Site - Barker Street	Thomas Street

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	Barker Street	Southern Site – Old Canterbury Road	
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**Note:** Variation to built form controls may be considered on site specific basis where the proposed development demonstrates achievement of the Precinct wide (9.49.3.2) and Area 2 (9.49.4.2) Desired Future Character and Precinct wide (9.49.3.10) and Area 2 (9.49.4.4) Built form Objectives and delivers better outcomes.

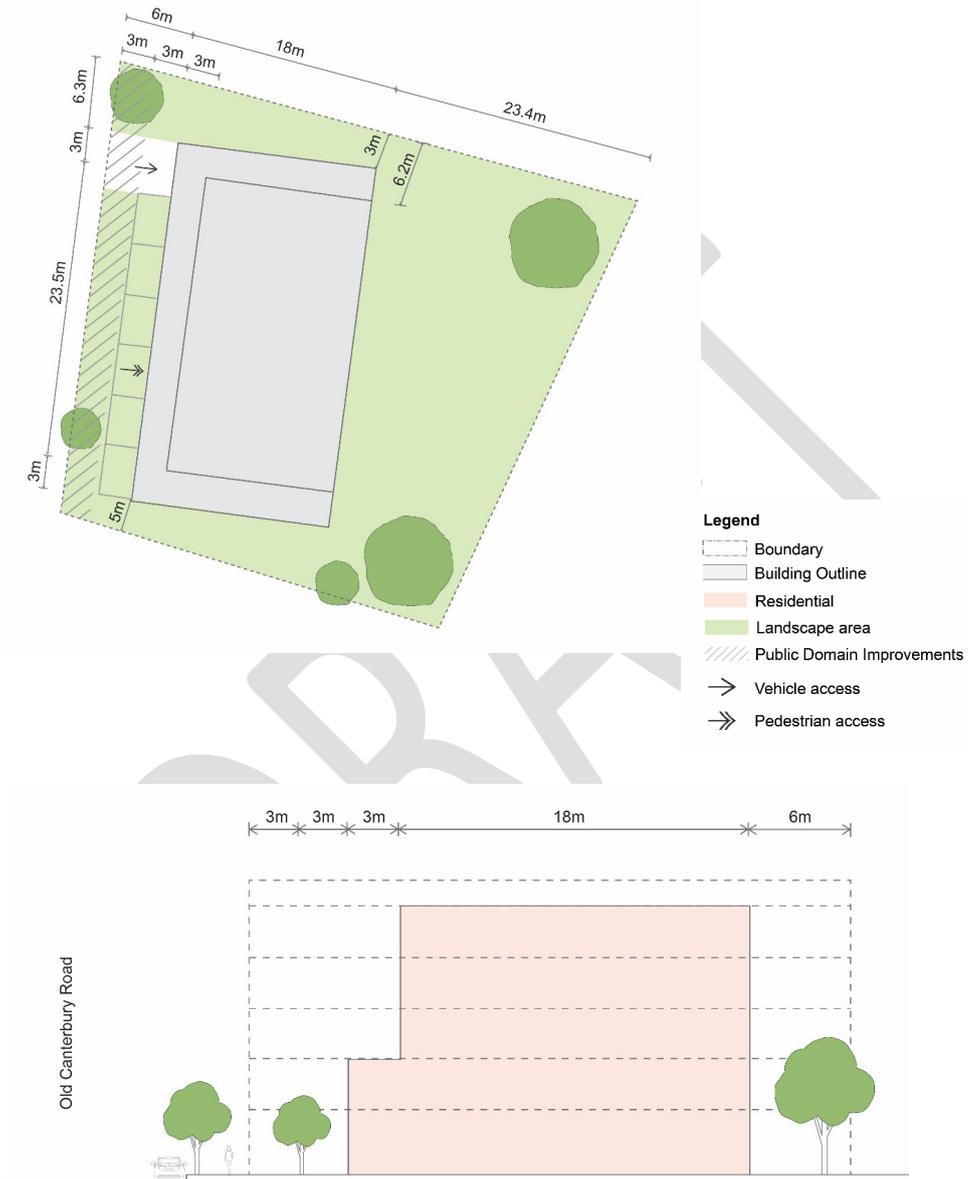
Figure 6: Old Canterbury Road East, northern site – indicative solution that achieves storeys, building envelope, scale and site layout – plan and section



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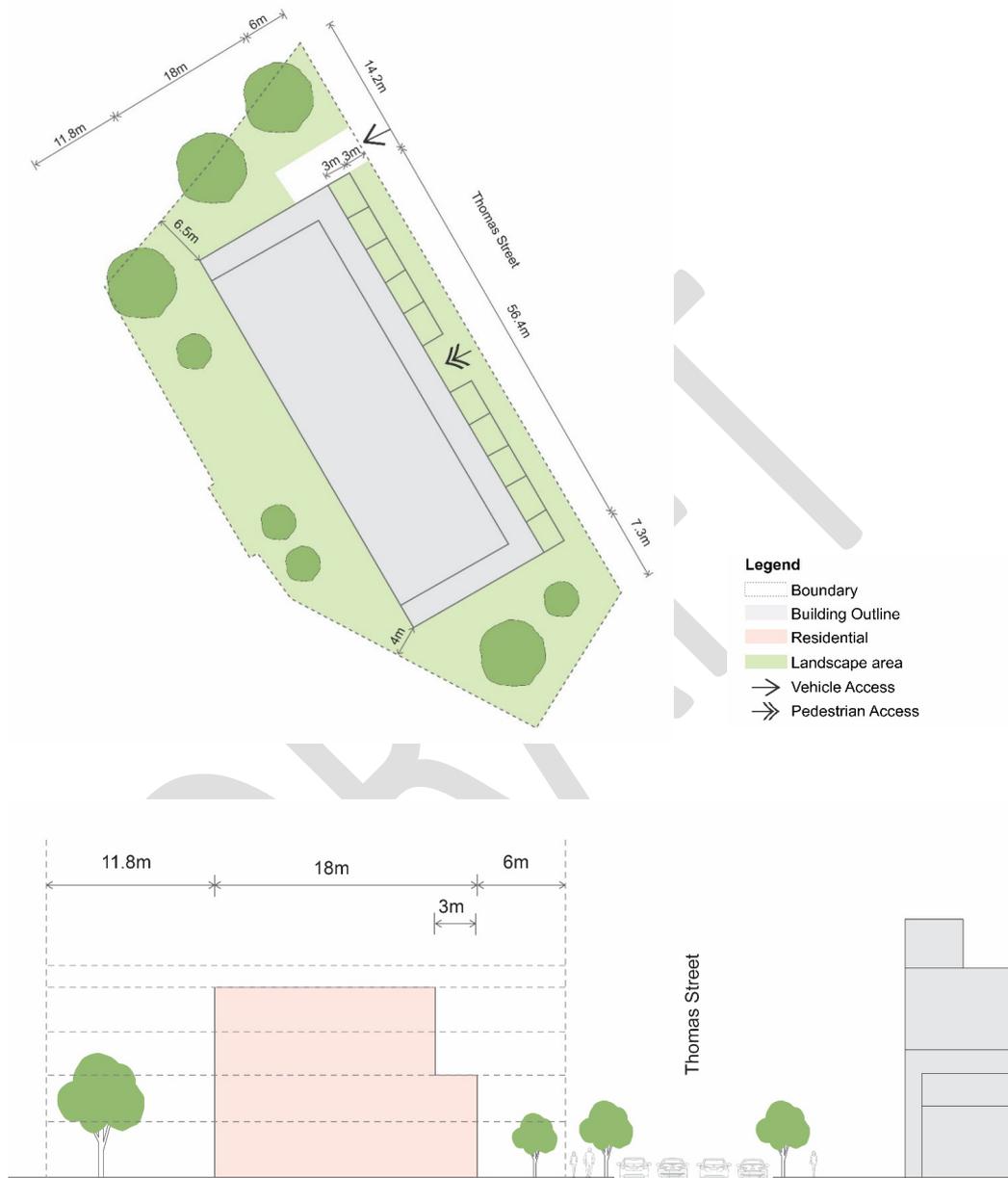
Figure 7: Canterbury Road East, southern site indicative solution that achieves storeys, building envelope, scale and site layout – plan and section



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Figure 8: Thomas Street – indicative solution that achieves storeys, building envelope, scale and site layout – plan and section



**9.49.4.5. Landscaping****Objectives**

- O22. To maintain and enhance the landscaped and leafy character of the area.
- O23. To extend the public domain and enhance pedestrian amenity on Old Canterbury Road east.

**Controls**

- C22. Maintain and enhance the landscape character by:
- providing landscaping in the required front, side and rear setbacks
  - minimising driveways and crossovers
  - retaining existing on-site vegetation, especially mature trees, as much as possible.
- C23. Provide streetscape and pedestrian movement improvements on Old Canterbury Road east that contributes towards an enhanced public domain.

Note: Landscaping requirements should be read in conjunction with Section 9.49.3.2 Streetscape and public domain, 9.49.3.7 Sustainability and resilience and 9.49.3.12 Landscaping.

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## 14. Parramatta Road Corridor – Kings Bay/ Croydon Precinct

### 14.1. Application

Chapter D, Section 14 Parramatta Road Corridor: Kings Bay/ Croydon/ Croydon Precinct applies:

- to the land identified in **Figure 1: Parramatta Road Corridor: Kings Bay/ Croydon Precinct Land Application Map**, and
- where development seeks to rely on the Incentives Floor Space Ratio Map, Incentives Height of Buildings Map and Clause XX of the Inner West LEP 2022.

Where development does not seek to rely on the incentive provisions, Chapter D, Section 14 does not apply. In this circumstance, relevant provisions of this DCP apply.

Kings Bay/ Croydon Precinct comprises three Areas that are identified on Figure 1. Each Area has varying functions and intended outcomes. The Areas are:

- Area 1 – Kings Bay/ Croydon: Parramatta Road Employment
- Area 2 – Kings Bay/ Croydon: Dalmar Street
- Area 3 – Kings Bay/ Croydon: Opportunity Sites.

Where seeking to rely on Incentives provisions, all development will achieve the Desired Future Character, Objectives and Controls detailed in:

- **Section 14.3** that applies to all Areas in the Kings Bay/ Croydon Precinct, and as applicable
- **Section 14.4** that applies to Area 1 – Kings Bay/ Croydon: Parramatta Road Employment, or
- **Section 14.5** that applies to Area 2 – Kings Bay/ Croydon: Dalmar Street, or
- **Section 14.6** that applies to Area 3 – Kings Bay/ Croydon: Opportunity Sites.

These Sections supplement and should be read in conjunction with relevant provisions of this DCP.

Where Part D, Section 14 applies and there is an inconsistency between this Section and the other provisions of this DCP, this Section prevails.

Figure 1: Parramatta Road Corridor: Kings Bay/ Croydon Precinct Land Application Map



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## 14.2. Context

### Parramatta Road Corridor Urban Transformation Strategy

Parramatta Road Corridor – Kings Bay/ Croydon Precinct is one of eight Precincts of the Parramatta Road Corridor Urban Transformation Strategy (PRUCUTS). PRUCUTS is the NSW Government’s 30-year plan setting out how the Parramatta Road Corridor will grow and bring new life to local communities living and working along the Corridor.

The vision for Parramatta Road Corridor is:

**A high-quality multi-use corridor with improved transport choices, better amenity and balanced growth of housing and jobs.**

The vision is supported by seven principles:

1. **Housing choice and affordability**  
Plan for diversity in housing types to accommodate a wide range of community needs, including affordable, family, student and seniors housing.
2. **Diverse and resilient economy**  
Plan for and position the corridor to attract new businesses and support existing business that create a diversity of jobs and promote jobs closer to home.
3. **Accessible and connected**  
Reshape and better connect places and movement networks to better serve customers and encourage sustainable travel.
4. **Vibrant community places**  
Promote quality places and built form outcomes to transform the corridor over time.
5. **Green spaces and links**  
Embellish existing open space and provide for new open spaces that support the recreational needs of the community and encourage active and healthy lifestyles.
6. **Sustainability and resilience**  
Create liveable local Precincts along the corridor that are sustainable, resilient and make Sydney a better place.
7. **Delivery**  
Deliver, drive, facilitate and monitor action.

### PRUCUTS and Inner West

Four of the eight PRUCUTS Precincts are within Inner West Council local government area. These include:

- Part of **Kings Bay/ Croydon Precinct** in Croydon that extends from Lang Street, Croydon in the west to Iron Cove Creek in the east. The remaining areas of Kings Bay/ Croydon Precinct are in Burwood and Canada Bay local government areas. The Inner West section of **Kings Bay/ Croydon Precinct**, being the subject of Part D, Section 14 of this DCP.
- **Taverners Hill Precinct** that extends from Petersham in the east to Summer Hill in the west and includes areas in Leichhardt and Lewisham.
- **Leichhardt Precinct** in the suburbs of Leichhardt and Petersham.
- Part of **Camperdown Precinct**. The remaining area of Camperdown Precinct is in City of Sydney local government area.

### 14.3. Kings Bay/ Croydon Precinct

#### 14.3.1. Application

Section 14.3 applies to the entire Kings Bay/ Croydon Precinct as identified in Figure 2.

**Figure 2:** Kings Bay/ Croydon Precinct



#### 14.3.2. Desired future character

Kings Bay/ Croydon Precinct:

- Parramatta Road is a productive economic corridor that attracts investment, new businesses and employment opportunities.
- Housing within the Precinct is well located and diverse, serving the needs of people of all ages, abilities and incomes.
- People enjoy a public domain that is safe, well-designed and landscaped.
- Pedestrians and bike riders benefit from:
  - increased connections between Parramatta Road and Dalmar Street
  - links to the open space along Iron Cove Creek.
- Lot amalgamation has optimised redevelopment opportunities and made efficient use of land.
- Living and working environments are sustainable and comfortable as a result of:
  - buildings having a high standard of environmental performance
  - integrated water management
  - building design, landscape and materials reducing urban heat effects
  - good facilities for active transport and access to public transport
  - catering for electric charging infrastructure.
- The built form is high quality, suitably scaled, transitions to neighbouring areas and meet the needs of intended uses.

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- Residents benefit from building design that maximises their amenity while protecting the amenity of nearby residential development.
- Consolidated, access locations reduce vehicle movements to and from Parramatta Road and across the Precinct.
- Reliance on private vehicles has reduced to support sustainable living through:
  - reducing on-site car parking provision for origin and destination locations
  - setting maximum car parking rates instead of requiring minimum car parking
  - implementing new models such as unbundled parking and shared car use
  - leveraging proximity to public transport networks, including Sydney Metro at Five Dock Croydon Station and rapid transport on dedicated lanes on Parramatta Road.

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14.3.3. Connectivity and accessibility

Objectives

- O1. To increase connectivity and accessibility across the Precinct for pedestrians and bike riders by enhancing links between workplaces and residential areas to key locations.
- O2. To enhance local connectivity between Dalmar Street and Parramatta Road.

Controls

- C1. Built form and streetscape treatments reinforce the connections identified in Figure 3: Kings Bay/ Croydon Precinct connectivity and accessibility map, including:
  - a. Spencer Street urban village centre
  - b. Sydney Metro Five Dock Station
  - c. Croydon Railway Station
  - d. Iron Cove Creek, Parramatta River and the Bay Run
  - e. public open space and recreation facilities in the locality
  - f. Queens Road cycling link.
- C2. Where a desired through-site link is identified on Figure 3, lot amalgamation and development contribute to mid-block connections to increase connectivity between Dalmar Street and Parramatta Road.

Figure 3: Kings Bay/ Croydon Precinct connectivity and accessibility map



## 14.3.4. Streetscape and public domain

### Objectives

- O3. To improve the amenity and safety of the streetscape in a manner that:
- a. contributes to the street character and intended land uses
  - b. is supported by built form that interfaces well with the streetscape
  - c. reduces street clutter and improve the visual amenity of the public domain
  - d. upgrades verges and pedestrian movement paths
  - e. contributes to mitigating urban heat through kerb-side landscaping
  - f. protects existing street trees, in the verge or roadway
  - g. positively contributes to water management and is waterwise
  - h. results in a durable and low maintenance public domain.

### Controls

- C3. Development contributes towards enhancing the streetscape for the extent of the street frontage by:
- a. providing required built form setbacks and a built form that interfaces well with the streetscape
  - b. providing a pedestrian movement path that is:
    - i. a minimum of 1.8m wide on Parramatta Road and intersecting streets for a length of 20m, or
    - ii. a minimum of 1.5m wide on all other streets
  - c. ensuring that pedestrian movement area is clear of obstacles
  - d. integrating pedestrian and vehicular entries into the streetscape design
  - e. providing a kerb-side permeable landscaped area for the remaining width of the verge that:
    - i. acts as a soft barrier between pedestrians and traffic lanes
    - ii. retains and protects existing street trees, in the verge or roadway, and includes new trees where appropriate
    - iii. includes low-level ground cover species that are hardy and suited to the location, can average 0.9m in height (excludes ground level mown grass) and are a minimum of 50% native
    - iv. includes water sensitive urban design solutions
    - v. does not impede walkers, bike riders, vehicles in traffic lanes or designated on-street parking and does not interfere with vehicle sight lines
    - vi. is designed to include, as required, public transport infrastructure, street signage and lighting
    - vii. upgrades the kerb to the required standard for the location, or
  - f. where the verge width does not allow for the required movement path width and a separate landscaped area that is sufficient to sustain vegetation - design the movement path to integrate landscape treatments and/or street tree planting or provide landscaped buildouts extending into the road reserve.

#### Notes:

1. Refer to *Inner West Public Domain Design Guide (202X)* for details of road types, footpath area functions and finishes.

2. Refer to Figure 4 for examples of streetscape and building setback landscaping.
3. Controls related to built form and landscaping as detailed in Sections 14.4, 14.5 and 14.6 as relevant to the development location.

**Figure 4:** Examples of streetscape verge and façade planting



**14.3.5. Development utility infrastructure**

**Objectives**

- O4. To reduce street clutter, provide opportunity for viable street trees and enhance the public domain.
- O5. To locate and design mechanical plant and essential services in a way that:
  - a. improves the visual amenity of the public domain
  - b. does not conflict with landscaping or street tree planting
  - c. is located outside the public domain.

**Controls**

- C4. Relocate existing overhead cables underground, and where possible, co-locate with other underground services.
- C5. Mechanical plant and essential services equipment are:
  - a. contained within the property

- b. located off the primary street frontage, or
- c. where on the primary street frontage - located behind the building line and screened from view
- d. integrated with the building and landscape design.

### 14.3.6. Lot amalgamation

#### Objectives

- O6. To promote efficient use of land and orderly redevelopment by:
  - a. avoiding isolating lots and reducing development potential
  - b. providing intended uses and built form outcomes that make a positive contribution to the streetscape.

#### Controls

- C6. Lot amalgamation:
  - a. does not result in isolated lots that are impractical for redevelopment due to the scale and intensity desired for the area
  - b. combines narrow lots and lots in fragmented ownership.

Note: Refer to additional Controls in Section 14.4.3 for Area 1 or 14.5.3 for Area 2.

### 14.3.7. Sustainability and resilience

#### Objectives

- O7. To achieve a high standard of environmental building performance that:
  - a. reduces greenhouse gas emissions and water use
  - b. results in comfortable living and working environments.
- O8. To reduce urban heat island effect through incorporating and integrating mechanisms that collectively mitigate the impacts, including:
  - a. green infrastructure in the form of landscape and surface treatments that incorporate water storage and treatment while reducing water usage
  - b. trees that offer shade to the built form, hard surfaces and vegetation
  - c. building materials and colours that reduce heat impacts, contribute to energy efficiency and thermal comfort, and minimise nuisance caused by glare or heat radiation.

#### Controls

- C7. Building Environmental Performance Report or BASIX certificate demonstrates that the development:
  - a. achieves a reduction in greenhouse gas emissions and water use
  - b. results in comfortable living and working environments
  - c. includes passive design features such as optimal orientation, increased insulation, effective shading, cross ventilation and lower solar absorptance external surface finishes
  - d. optimises rooftop solar photovoltaic systems
  - e. achieves full electrification of utilities including cooking (other than in commercial kitchens), heating and hot water (heat pumps)
  - f. for residential development:
    - i. achieves an average thermal performance of 7-star NatHERS

- ii. incorporates ceiling fans in bedrooms and living rooms.
- C8. Mitigate urban heat island effect by:
  - a. achieving required tree canopy through:
    - i. site layout maximising retention of existing mature trees
    - ii. including advanced containerised trees (greater than 200 litre) of a species that within 10 years will achieve 50% of their potential at maturity
    - iii. incorporating trees and vegetation across various storeys (roof tops, terraces, atriums, and the like), in addition to, or where necessary, as an alternative to ground level planting
    - iv. a combination of the above that collectively achieve, or exceed, tree canopy requirements
  - b. integrating green roofs and walls as a component of the landscape and built form design specifically in northern and western facing locations
  - c. incorporating permeable surfaces, rain gardens, and other water sensitive measures in landscape treatments
  - d. using materials and colours that:
    - i. have a high solar reflectance index (lighter colours) on roofs, facades, glazing or ground surfaces subject to their purpose and aligned to orientation and exposure to sunlight
    - ii. where it may cause nuisance due to glare or reflection do not exceed 20% reflectivity (darker colours).

**14.3.8. Access and parking**

**Objectives**

- O9. To ensure developments reduce private motor vehicle use, minimise traffic impacts and encourage sustainable transport.
- O10. To enhance the public domain, improve pedestrian experience and safety, and limit the number of vehicle access points throughout the Precinct.
- O11. To reduce private vehicle ownership through unbundled parking, car share schemes and decoupled parking, where on-site car parking is provided.
- O12. To ensure development provides facilities for electric vehicles.
- O13. To future proof infrastructure to support increased take-up of electric vehicles.
- O14. To ensure vehicle parking, servicing and loading areas are designed to:
  - a. reduce their visual impact on the public domain
  - b. support all vehicle types anticipated by development including service vehicles and loading areas
  - c. maximise potential adaptation at a future point in time when less parking is required.
- O15. To ensure delivery areas prioritise servicing outside peak pedestrian activity for key streets.
- O16. To ensure bike riders have sufficient accessible and secure parking.
- O17. To provide on-site workers facilities for employment generating uses that encourage active transport commuting, healthy workplaces and cater for worker needs.

## Controls

- C9. Travel plans are to include the following:
- a. baseline travel demand and mode share estimates from established similar developments
  - b. targets for reduced private motor vehicle trips and an increased mode share for sustainable transport
  - c. actions to be implemented to achieve the mode shift targets, with a written commitment from the property owner and/or business operator to implement them
  - d. a process for monitoring and review of actions and targets
  - e. a guide for residents, employees and visitors associated with the development to assist with the mode shift
  - f. public transport subsidies for workers for commuter and for-work trips and parking charges for workers who commute by car and/or payments to employees who don't
  - g. on-site carshare schemes and memberships, and priority parking for multiple occupancy vehicles, e.g. employees who car pool
  - h. subsidised bicycle purchase and quality bicycle parking and associated end-of-trip facilities
  - i. provision of peak period shuttle buses, relocation allowances and flexible working hours
- C10. Vehicular access is located to:
- a. reduce the number of access points to and from Parramatta Road
  - b. use secondary streets or rear accessways and laneways
  - c. consolidate vehicle access to reduce the number of crossovers through a maximum of one driveway per site or one-way pair.

**Note:** Refer to additional Controls relevant to specific Areas within the Precinct in Sections 14.4, 14.5 and 14.6.

- C11. Provision of private vehicle parking:
- a. is listed on a separate title (unbundled) from the development (i.e. separated from dwelling, commercial units and building ownership)
  - b. is decoupled from the development, as relevant
  - c. includes car share vehicle(s) that:
    - i. are located either on-site or on the street at the discretion of Council
    - ii. do not result in the maximum car parking rates being exceeded
    - iii. are publicly available and readily accessible at all times.

**Note:** **Unbundled parking** means parking that is separated from the cost or rent of a dwelling, commercial units and building ownership.

**Car share scheme** means a scheme in which any car share operator provides vehicles for shared use and hires those vehicles exclusively to members of the scheme for occasional use for short periods of time, on demand and on a pay-as-you go basis.

**Decoupled parking** means provision of off-site car parking, usually in the form of consolidated car parking in close proximity to the development to satisfy the parking requirements.

- C12. Where shared use of car parking spaces is included, they are determined on a case-by-case basis dependant on anticipated tenancies/uses.

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- C13. Provide electric vehicle (EV) ready to use (including cabling, power outlet or charging head) car parking spaces:
- a. for non-residential development – Level 3, or faster, at a rate of 10% for all spaces – dedicated and visitor
  - b. for residential development – Level 1, or faster, at a rate of:
    - i. 20% for resident spaces
    - ii. 10% for visitor spaces.
- C14. Design electric infrastructure services (distributions boards, conduits and cables) to ensure:
- a. sufficient energy and capacity, preferably from renewable sources
  - b. reticulated fixed charging facilities cater for a minimum of:
    - i. for non-residential development - 50% of all parking spaces
    - ii. for residential development – 100% of all parking spaces
  - c. any future EV charger does not require a cable of more than 50m from the parking space to the EV-ready connection.
- C15. On-site ground level exposed car parking is not provided, and parking areas:
- a. are concentrated below ground or sleeved by other uses
  - b. are not open structures that are visible from the public domain
  - c. where below ground:
    - i. do not protrude:
      - above ground level at any point along street frontages
      - into setbacks areas that are identified as landscape areas
    - ii. are designed to facilitate break out walls, where required
  - d. do not impede the provision of viable vegetation
  - e. are designed to accommodate all vehicles anticipated by the development
  - f. provide sufficient manoeuvring space to allow vehicles to enter and exit the site in a forward direction
  - g. are designed in a manner that encourages opportunities for adaptation for other uses over time.
- C16. Service delivery times are restricted to avoid conflict with peak pedestrian periods on key streets, where practical.
- C17. Bicycle parking:
- a. complies with the minimum requirements detailed in Table 1
  - b. is in accessible and visible locations for residents, workers and visitors
  - c. is secured through provision of bike cages for residents and workers or bike stands for visitors
  - d. is provided with ready-to-use electric charging points at a minimum rate of:
    - i. for non-residential development - 1 per 4 bicycle spaces
    - ii. for residential development – 1 per 2 bicycle spaces
    - iii. where there are multiple parking areas, facilities are distributed equally across all locations.

**Table 1: Minimum bicycle parking**

Development type	Resident/Worker	Visitor
Residential	1 space per dwelling	1 space per 10 dwellings
Office	1 space per 150m <sup>2</sup> GFA	1 space per 400m <sup>2</sup> GFA
Retail	1 space per 250m <sup>2</sup> GFA	2 spaces + 1 per 100m <sup>2</sup> GFA
Industrial	1 per 10 staff	1 space per 500m <sup>2</sup> GFA

C18. On-site workers facilities:

- a. comply with the minimum requirements detailed in Table 2
- b. are in secure locations
- c. where more than one shower/change cubicle is required, separate and equal numbers of male and female facilities are provided.

**Table 2: Minimum worker facilities for all employment generating uses**

Anticipated number of workers	Personal lockers	Showers and change cubicles
0-49	1 per 2 workers	1 unisex
50 - 99	1 per 3 workers	2
100-199	1 per 4 workers	4
200+	1 per 5 workers	+ 1 per 200 workers

### 14.3.9. Active street frontages

#### Objectives

O18. Active street frontages are provided to reinforce the vitality and liveliness of the public domain.

O19. Active street frontages are provided:

- a. with ground floor frontages being pedestrian orientated and of a high design quality to add vitality to streets
- b. by incorporating frequent pedestrian entries that open towards the street.

#### Controls

C19. Provide active street frontages by including the following uses at street level:

- a. shops, commercial premises and other employment uses
- b. commercial and residential lobbies and reception areas
- c. public buildings or community facilities.

C20. Active street frontages contribute to the liveliness and vitality of streets by:

- a. providing a minimum of 70% of the ground floor frontage as transparent glazing with an unobstructed view from the adjacent footpath to at least a depth of 6m within the building
- b. maximising entries, display windows, customer service areas and key activities to provide pedestrian interest and interaction
- c. minimising blank walls, fire escapes, service doors, plant and equipment hatches

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- d. providing elements of visual interest, such as display cases, or creative use of materials where fire escapes, service doors and equipment hatches cannot be avoided
- e. providing a high standard of finish and appropriate level of architectural detail for building facades
- f. providing passive surveillance to enhance safety and security
- g. providing ground floor pedestrian entry at the same level as the street to maximise accessibility for all users
- h. not including driveways and service entries
- i. if including security measures, using grilles or screens that are fitted internally and are a minimum of 60% perforated/transparent when closed.

**14.3.10. Built form**

**Objectives**

- O20. To provide a high-quality and well-designed built form that:
  - a. supports intended land uses
  - b. promotes a positive image for businesses
  - c. is of a bulk and scale and has site layout that complements the local context
  - d. minimises adverse amenity impacts
  - e. enhances the public domain for pedestrians
  - f. incorporates lighting that contributes to the quality and safety of the night-time urban environment, is sustainable and easy to maintain
  - g. does not create nuisance or hazard from glare, noise and odour for pedestrians, motorists, or occupants of nearby buildings.
- O21. To ensure development for residential purposes achieves a high-quality living environment and mitigates urban hazards by taking an integrated and innovative approach to:
  - a. address road noise and air quality impacts
  - b. the orientation of development and individual dwellings
  - c. minimise the need for mechanical ventilation and heating or cooling
  - d. protect the amenity of nearby residential developments.

**Controls**

- C21. Building design:
  - a. includes architectural features and façade articulation to reduce apparent building bulk
  - b. emphasises building corners at intersections
  - c. does not result in overshadowing or loss of privacy
  - d. locates pedestrian entries:
    - i. on the primary street frontage and visible from the street
    - ii. at the same level as the street to maximise accessibility for all users
  - e. where incorporating external lighting it:
    - i. is integrated into the building design and highlights distinctive architectural features
    - ii. is energy efficient, high quality, durable and low maintenance
    - iii. does not cause nuisance or hazard to occupants of the building or nearby buildings
    - iv. minimises light spill into the night sky
    - v. supports street lighting to enhance safety and security
  - f. negates adverse noise and odour emissions from activities, plant or equipment.

- C22. Residential development results in comfortable and enjoyable internal environments through:
- a. meeting the required standards for residential development near busy roads
  - b. using a variety of integrated built form design and construction and acoustic solutions to ameliorate negative amenity impacts including but not limited to:
    - i. material and glazing choices
    - ii. angled walls and modulated surfaces
    - iii. solid balconies and winter gardens
    - iv. screens, louvres and hopper windows
    - v. locating single aspect dwellings away from the north and west street frontages
    - vi. incorporating light wells, atriums and internal articulation to enhance sun capture and air movement
  - c. retaining privacy and solar access while improving noise impacts for nearby residential developments.

### 14.3.11. Building materials and finishes

#### Objectives

- O22. To provide building materials, fittings and finishes that are high quality, sustainable and complement the locality.
- O23. To reduce building waste by effectively re-using or recycling building materials where demolition or deconstruction of existing structures is required to facilitate new development.

#### Controls

- C23. Building materials, fittings and finishes:
- a. are durable, of high-quality, textured, and complement materials used in the locality
  - b. are sustainable with low embodied carbon such as:
    - i. replacement of Portland cement with supplementary cementitious materials (SCMs) in concrete (i.e., 30% SCM across all pre-cast and in-situ cement)
    - ii. high recycled content in steel
    - iii. timber framing instead of steel framing
    - iv. cross laminate timber
  - c. incorporate recycled materials, where possible.
- C24. The Deconstruction Plan demonstrates that the majority of demolished building materials, excluding hazardous materials, are integrated into the design and construction of development by re-using on-site or through appropriate recycling.

### 14.3.12. Landscaping

#### Objectives

- O24. To ensure on-site landscaping:
- a. includes species native to the area
  - b. is suited to the location
  - c. provides habitat to enhance biodiversity
  - d. positively contributes to water management and is waterwise
  - e. contributes to mitigating urban heat
  - f. is durable and low maintenance.

**Controls**

C25. Landscaping Strategy demonstrates that the landscape:

- a. is provided in dedicated setbacks
- b. includes:
  - i. water sensitive urban design solutions
  - ii. trees and supporting vegetation
  - iii. greening opportunities including green roofs and walls, pergolas with climbers, podiums, planters, lawns and gardens, rain gardens and permeable pavement
  - iv. 50% native species.

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## 14.4. Area 1 – Kings Bay/ Croydon: Parramatta Road Employment

### 14.4.1. Application

Section 14.4 applies to Area 1 – Kings Bay/ Croydon: Parramatta Road Employment as shown in Figure 5.

**Figure 5:** Area 1 – Kings Bay/ Croydon: Parramatta Road Employment



### 14.4.2. Desired future character

The Desired Future Character for Area 1 supplements and should be read in conjunction with the Desired Future Character Statement detailed in Section 14.3 for the Kings Bay/ Croydon Precinct.

Kings Bay/ Croydon: Parramatta Road Employment:

- Is supported by lot amalgamation that uses land efficiently, is suited to intended uses and has avoided lots being isolated from future redevelopment.
- Development provides suitable floorplates and flexible spaces that accommodate a mix of medium to large format businesses.
- New accessways and use of existing laneways and side streets reduce vehicle access to and from Parramatta Road.
- Footpath upgrades, landscaping and reduced street clutter enhance the public domain, attracting pedestrians and businesses and creating a vibrant place for people.
- Development siting:
  - provides setback to Parramatta Road and intersecting streets allowing for landscaping to soften the built form
  - facilitates substantial landscaping along the rear boundary adjoining residential development and providing for rear access

- increases accessibility by providing for desired through-site links, in identified locations.
- Its built form:
  - is cohesive and presents a consistent street wall that defines Parramatta Road
  - provides active frontages that positively interact with the street through ground and mezzanine floors that are glazed and visually accessible
  - has protected solar access, privacy and amenity for nearby residential uses.

**14.4.3. Lot amalgamation**

**Objectives**

O25. To facilitate lot amalgamation that:

- a. results in lots of a sufficient size to deliver intended land uses
- b. reduces, or removes, where possible, direct access to or from Parramatta Road.

**Controls**

C26. Lot amalgamation:

- a. aligns with Figure 6: Area 1 – Kings Bay/ Croydon: Parramatta Road Employment preferred lot amalgamation pattern where a specific lot pattern is identified, or
- b. in other circumstances, achieves a minimum street frontage of 25m
- c. does not result in battle-axe lots
- d. facilitates access from intersecting streets or existing or proposed rear accessways.

Note: Refer to additional Controls in 14.3.6 Lot amalgamation.

**Figure 6:** Area 1 – Kings Bay/ Croydon: Parramatta Road Employment preferred lot amalgamation pattern



**14.4.4. Built form****Objectives**

- O26. To ensure building height:
- is appropriate for the location and anticipated land uses while protecting the amenity of adjoining residential development to the south
  - provides consistent street wall to Parramatta Road that is suited to the street proportions and defines and reinforces the street edge.
- O27. To ensure storey height:
- at the ground level, allows for a variety of uses, the potential for a mezzanine and flexibility to cater for change over time
  - above ground, is suited to employment land uses.
- O28. To ensure building floorplates are of sufficient size to support intended medium to large scale uses.
- O29. To provide a front setback that:
- facilitates a landscaped green edge along the front façade
  - extends across all storeys, including basement, so that the built form accommodates landscape areas
  - allows for public domain improvements along Parramatta Road.
- O30. To provide rear setbacks that:
- support a range of vehicular movements expected by the development
  - provide adequate landscaping to create a buffer between employment and residential uses
  - facilitate the provision of a continuous rear access easement between Lang and Scott Streets
  - increase in depth aligned to building height to provide a built form transition, and protect solar access, amenity and privacy to residential properties to the south.
- O31. To provide side setbacks, as applicable, that:
- meet the side property boundary line, or
  - facilitate connectivity and accessibility from Dalmar Street to Parramatta Road where desired through-site links are identified, or
  - on corner lots of Lang, Byron and Scott Streets:
    - reinforce the visual prominence of the street corner
    - provide opportunity to enhance the public domain through landscaping, street tree planting and footpath upgrades.

Note: Refer to Figure 3: Kings Bay/ Croydon Precinct Connectivity and Accessibility Map for location of desired through-site links.

- O32. To provide shelter for pedestrians at key activity locations.

**Controls**

C27. Building height:

- a. does not exceed four storeys
- b. provides a four-storey street wall to Parramatta Road.

C28. At ground level floor to floor height is a minimum of 5m.

Note: Floor to floor heights include a slope/topography allowance.

C29. Building floorplates:

- a. at the ground floor support employment uses and street activation
- b. limit ground floor use for services, storage and other business needs, and where required locate these to the rear of the building
- c. are larger in scale and designed to provide flexibility and ability to adapt to different uses.

C30. Front setback is 1.5m across all storeys and including the basement to facilitate public domain improvements and landscaping.

C31. Rear setback:

- a. between Lang and Byron and Byron and Scott Streets includes an accessway as detailed in Figure 8: Access Easement for the full extent of the rear setback
- b. is minimum of 9m at ground level:
  - i. includes a 3m wide landscaped area to the southern boundary that is unobstructed by any basement structure
  - ii. provides a 6m wide area for vehicle movement, or
  - iii. between Croydon Road and Scott Street:
    - facilitates an extension of Sophia Lane, where possible
    - includes a 3m wide landscaped area to the southern boundary that is unobstructed by any basement structure, or
    - north of Sophia Lane, being 624 to 636 Parramatta Road include a 3m wide landscaped area on the southern boundary that is unobstructed by any basement structure
- c. increases by 5m per floor above the second storey.

C32. Side setbacks, as applicable, are:

- a. zero; or
- b. 2m if the lot is identified as the location of a desired through-site link on Figure 4: Precinct Connectivity and Accessibility Map in Section 14.3.3, or
- c. on corner lots – built form design is splayed at the corner by a minimum of 3 x 3m.

C33. Building design incorporates awnings:

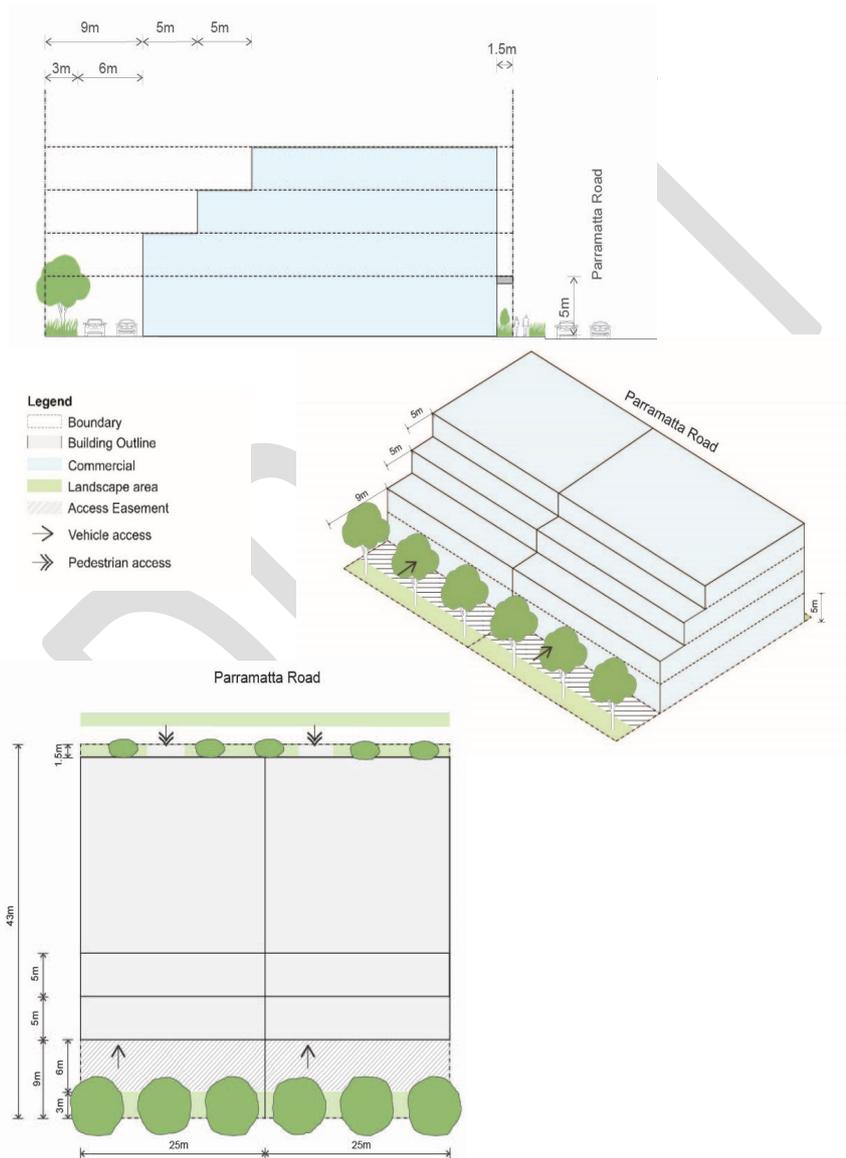
- a. at entries and lobbies
- b. that extend to the front property boundary
- c. do not impact on landscaping or provision of street trees
- d. cantilever from the top of the ground floor.

Notes:

1. Read built form requirements in conjunction with 14.3.4 Streetscape and public domain, 14.3.9 Active street frontages, 14.3.10 Built form, 14.3.11 Building materials and finishes, and 14.3.12 Landscaping

2. Refer to Figure 7 for indicative built form bulk and scale.
3. Floor to floor heights include a slope/topography allowance.
4. Variation to built form controls may be considered on site specific basis where the proposed development demonstrates achievement of the Precinct wide (14.3.2) and Area 1 (14.4.2) Desired Future Character and Precinct wide (14.3.10) and Area 1 (14.4.4) Built form Objectives and delivers better outcomes.

**Figure 7:** Indicative built form bulk and scale including storeys, floor heights, setbacks landscape areas and access – section, plan and axonometric views



**Figure 8: Access Easement**



**14.4.5. Landscaping**

**Objectives**

- O33. To ensure on-site landscaping:
- a. in the front setback - softens the building façade and enhances pedestrian amenity on Parramatta Road
  - b. in the rear setback - achieves a vegetated visual barrier between employment uses and residential development to the south.

**Controls**

- C34. Provide appropriate landscaping:
- a. in the front setback - is designed as an integral element of the built form design and does not cause impediment to the pedestrian movement area
  - b. in the rear setback:
    - i. includes deep soil areas that have a minimum dimension of 3m
    - ii. includes tree planting at a rate of 1 medium tree per 8 metres of lot width that will result in a continuous tree canopy appearance when viewed from residential development to the south.

**Notes:**

1. A medium tree has a minimum 8 metre diameter canopy at maturity.
2. Landscaping requirements should be read in conjunction with Section 14.3.4 Streetscape and public domain, 14.3.7 Sustainability and resilience, and 14.3.12 Landscaping.

## 14.5. Area 2 – Kings Bay/ Croydon: Dalmar Street

### 14.5.1. Application

Section 14.5 applies to Area 2 – Kings Bay/ Croydon: Dalmar Street as shown in Figure 9.

**Figure 9:** Area 2 – Kings Bay/ Croydon: Dalmar Street



### 14.5.2. Desired future character

The Desired Future Character for Area 2 supplements and should be read in conjunction with the Desired Future Character Statement detailed in Section 14.3 for the Kings Bay/ Croydon Precinct.

Kings Bay/ Croydon: Dalmar Street:

- Has delivered suitably scaled residential development that responds to the local context and provides a transition from the employment uses on Parramatta Road to the lower density residential area south of Dalmar Street.
- Footpath upgrades, landscaping and reduced street clutter enhance the public domain.
- Accessibility is increased by providing through-site links, in identified locations.
- Redevelopment is supported by lot amalgamation suited to intended uses and avoids lots being isolated from development opportunities.
- Development has increased housing diversity in the location.
- Residents benefit from building design that maximise their amenity while protecting solar access, privacy and amenity of existing residential development to the south.
- The existing street character is retained through landscaped front garden setbacks.

14.5.3. Lot amalgamation

Objectives

O34. To ensure lot amalgamation promotes the orderly redevelopment of land for intended residential uses and identified built form.

Controls

C35. Lot amalgamation:

- a. aligns to Figure 10: Area 2 – Kings Bay/ Croydon: Dalmar Street preferred lot amalgamation pattern, or
- b. where a. is not achievable:
  - i. results in a minimum lot size of 720m<sup>2</sup> and a minimum street frontage as detailed in Table 3, or
  - ii. meets the following criteria:
    - does not isolate surrounding lots from redevelopment
    - achieves required landscape areas and communal open space
    - provides required setbacks
    - consolidates vehicle access and reduces their impact on pedestrian movement paths
    - provides appropriate access for servicing and waste management
    - facilitates basement parking, where on-site parking is provided.

Figure 10: Area 2 – Kings Bay/ Croydon: Dalmar Street preferred lot amalgamation pattern



## 14.5.4. Built form

### Objectives

- O35. To deliver the preferred building type and align building design and layout to lot pattern and depth, street frontage and access arrangements.
- O36. To provide an appropriate built form that:
  - a. has an appropriate height for its context and provides a transition between employment uses to the north and low scale residential to the south of Dalmar Street
  - b. is responsive to the desired future character of the area and does not result in adverse amenity impacts on adjacent properties.

### Controls

- C36. Lot layout and building types are as identified in Figure 11: Residential building types based on:
  - a. Type 1A - Narrow lot infill with lot depth of approximately 46m
  - b. Type 1B - Narrow lot infill with lot depth of approximately 56m
  - c. Type 1C - Narrow lot infill - Croydon Road corner lot
  - d. Type 2 - Row house – between Byron and Scott Streets.

**Figure 11: Residential building types**



- C37. Built form:
  - a. complies with the number of storeys, building envelope, scale and site layout requirements detailed in Table 3
  - b. where identified as a location for a desired through-site link on Figure 3: Precinct Connectivity and Accessibility Map in Section 14.3.4 provide an additional side setback of 2m.

**Table 3: Built form – storeys, building envelope, scale and site layout requirements**

Criteria/type	Type 1 (A and B)	Type 1C	Type 2
Description	Narrow lot infill	Narrow lot infill corner of Croydon Road and Dalmar or West Streets	Row house between Byron and Scott Streets
Minimum street frontage	20m	35m	30m
Maximum storeys	3-storey	4-storey	3-storey
Street wall	2-storey	<ul style="list-style-type: none"> <li>4-storey street wall to Croydon Road, and</li> <li>3-storey to West Street or Dalmar Street</li> </ul>	2-storey
Floor to floor storey heights	First storey, ground floor – 4m Second storey and above – 3.2m		
Minimum above ground setback	Additional 3m for the 3rd storey	Additional 3m for the 4th storey fronting West or Dalmar Streets	Additional 3m for the 3rd storey
Minimum front, corner and rear setbacks	6m		
Minimum side setback	3m	6m	6m for corner lots, otherwise 3m

Note: Variation to built form controls may be considered on site-specific basis where the proposed development demonstrates achievement of the Precinct wide (14.3.2) and Area 2 (14.5.2) Desired Future Character and Precinct wide (14.3.10) and Area 2 (14.5.4) Built form Objectives and delivers better outcomes.

**14.5.5. Landscaping**

**Objectives**

O37. To maintain and enhance a landscaped character that relates to the existing streetscape.

**Controls**

- C38. Maintain and enhance the landscape character by:
- providing a 6m setback for a landscaped front garden
  - minimising driveways and crossovers
  - retaining existing vegetation, especially mature trees, as much as possible.

Note: Landscaping requirements should be read in conjunction with 14.3.4 Streetscape and public domain, 14.3.7 Sustainability and resilience, and 14.3.12 Landscaping.

## 14.6. Area 3 – Kings Bay/ Croydon: Opportunity Sites

### 14.6.1. Application

Section 14.6 applies to Area 3 –Kings Bay/ Croydon: Opportunity Sites as shown in Figure 12.

Area 3 comprises:

- Opportunity Site 1: 612–624 Parramatta Road and 210 Croydon Road
- Opportunity Site 2: 590–610 Parramatta Road and 235–237 Croydon Road

**Figure 12:** Area 3 – Kings Bay/ Croydon: Opportunity Sites



### 14.6.2. Desired future character

The Desired Future Character for Area 3 supplements and should be read in conjunction with the Desired Future Character Statement detailed in Section 14.3 for the Kings Bay/ Croydon Precinct.

Kings Bay/ Croydon: Opportunity Sites:

- Reininvorate these key locations in the Precinct.
- Enliven the important intersection at the corner of Parramatta Road and Croydon Road.
- Have active employment uses on the ground floor with residential uses above.
- Are supported by lot amalgamation that use land efficiently, is suited to intended uses and avoids lots being isolated from redevelopment.
- There is an enhanced public domain due to increased setbacks, footpath upgrades, landscaping and reduced street clutter.
- Consolidated access from Croydon Road and rear lanes reduces vehicle access to and from Parramatta Road.
- The adaptive reuse of the Electricity Substation at 590 Parramatta Road, Croydon has conserved the heritage significance, character, fabric and features of the heritage listed building.

- Its built form:
  - is high-quality architecture
  - is cohesive and presents a consistent street wall that defines Parramatta Road and is appropriately scaled to transition along Croydon Road
  - positively interacts with the street through ground and mezzanine floors that are highly glazed and visually accessible.

**14.6.3. Lot amalgamation**

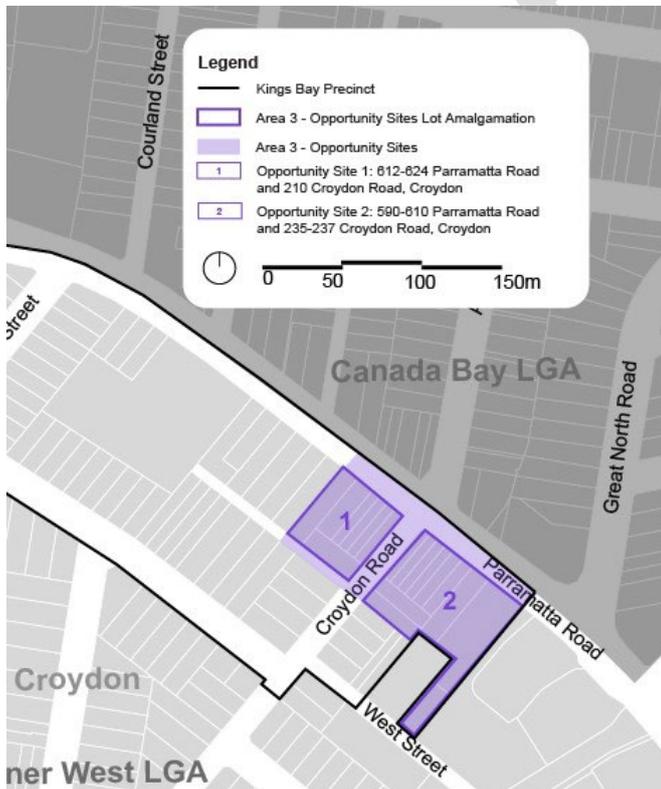
**Objectives**

O38. To ensure lot amalgamation promotes the orderly redevelopment of land for intended uses and does not isolate or prevent redevelopment of lots.

**Controls**

C39. Lot amalgamation aligns to Figure 13: Area 3 – Kings Bay/ Croydon: Opportunity Sites preferred lot amalgamation pattern.

**Figure 13:** Area 3 – Kings Bay/ Croydon: Opportunity Sites preferred lot amalgamation pattern



**14.6.4. Built form**

**Objectives**

- O39. To ensure building height:
  - a. is appropriate for the location and anticipated land uses while protecting the amenity of nearby residential development
  - b. provides consistent street wall on Croydon Road and Parramatta Road that is suited to the street proportions and defines and reinforces the street edge.
- O40. To provide a ground level storey height that allows for a variety of uses, the potential for a mezzanine and provides flexibility to adapt to changing market conditions over time.
- O41. To ensure storey height above ground floor is suited to support residential uses.
- O42. To ensure ground floor and basement setbacks:
  - a. support intended building typologies
  - b. provide a landscaped area on the Parramatta Road building frontage
  - c. on the corner of Croydon Road:
    - i. reinforce the visual prominence of the street corner
    - ii. provide for public domain enhancement of Croydon Road.
- O43. To ensure above ground floor setbacks are provided to:
  - a. reduce the bulk and scale of development
  - b. retain solar access, amenity and privacy for nearby residential properties
  - c. retain the prominence and landmark qualities of the heritage item.
- O44. To provide appropriate employment uses on the ground floor that:
  - a. are compatible with the residential uses above
  - b. are separated from residential uses through subdivision
  - c. safeguard the provision and viability of business uses
  - d. provide large floor plates and high ceilings to ensure functionality and flexibility in accommodating a diverse range of employment uses.
- O45. To provide clearly defined and accessible non-residential and residential lobbies and entries.
- O46. To improve the interface of the public domain and the built form for pedestrians and residents by:
  - a. providing shelter at key activity locations
  - b. ensuring visual access to the interior.
- O47. To minimise land use conflict between employment and residential uses by:
  - a. ensuring acceptable residential amenity and the ongoing viability of employment uses on site and on surrounding sites
  - b. providing a built form design with appropriate construction techniques and acoustic measures that attenuates noise and vibration between residential and employment uses.

**Controls**

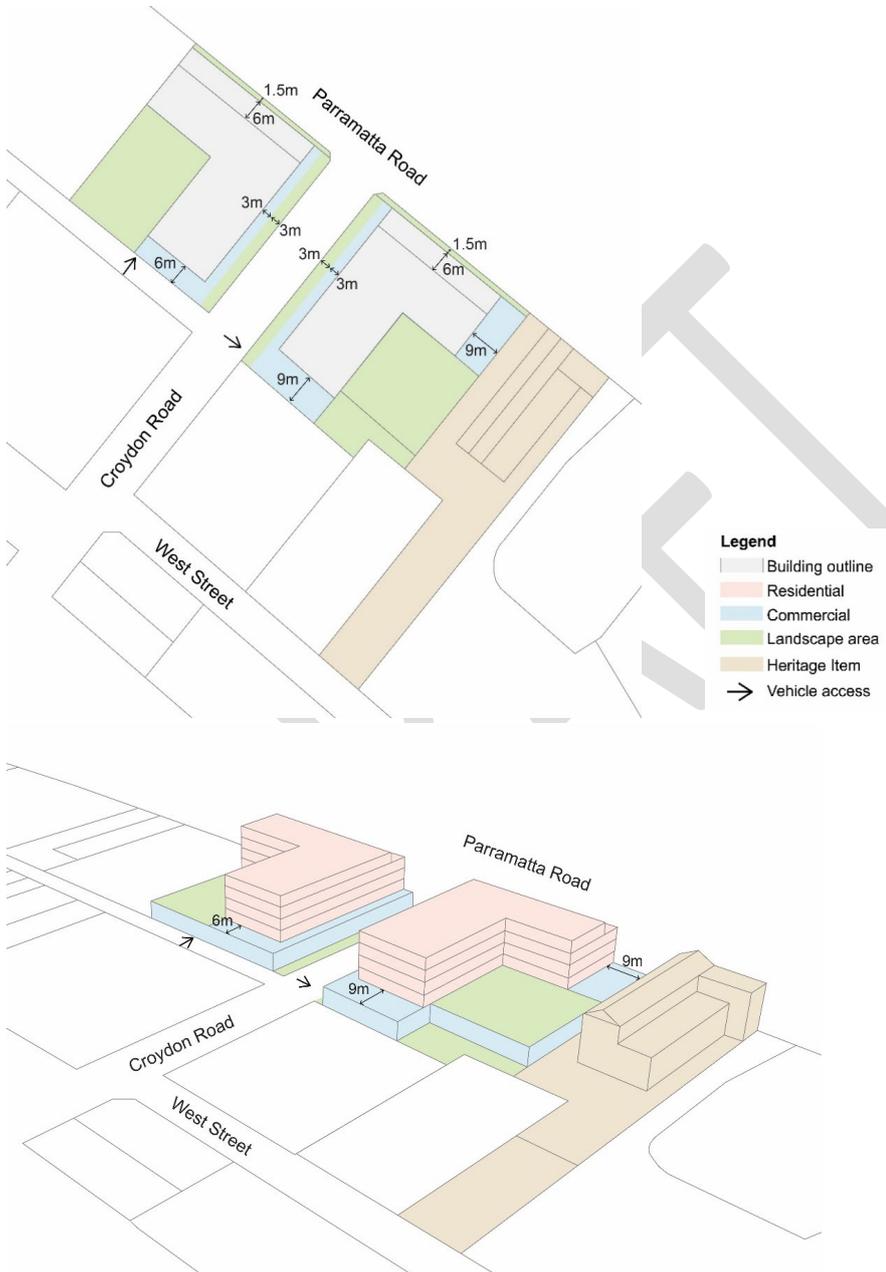
- C40. Building height:
  - a. is equivalent to 5 storeys
  - b. has a street wall of 4 storeys to Parramatta Road and 1 storey to Croydon Road
  - c. for 590 Parramatta Road, Croydon retains its existing height to Parramatta Road frontage with any additions clearly distinguishable from the fabric of the heritage item.
- C41. Ground level floor to floor height is a minimum of 5m for employment uses.
- C42. First storey and above is a minimum of 3.2m for residential uses.
- C43. Built form setbacks at the ground and basement levels are at a minimum of:
  - a. to Parramatta Road – 1.5m
  - b. to Croydon Road – 3m
  - c. for 590 Parramatta Road – retain zero front setback
  - d. side and rear – zero.
- C44. Above ground setbacks:
  - a. facilitate built form articulation, separation distances and communal open space
  - b. are a minimum of:
    - i. to Parramatta Road – additional 6m for the 5th storey
    - ii. to Croydon Road – additional 3m above the 1st storey
    - iii. for Opportunity Site 1 to the rear laneway – additional 6m above the 1<sup>st</sup> storey
    - iv. for Opportunity Site 2 – additional 9m above the 1<sup>st</sup> storey where adjoining residential land to the south or the Heritage Item at 590 Parramatta Road Croydon
- C45. Building design facilitates employment uses on the ground floor that:
  - a. are compatible with residential uses
  - b. activate the street frontage
  - c. provide suitable floorplates
  - d. limit ground floor use for services, storage and other business needs, and where required locate these to the rear of the building
  - e. are larger in scale and designed to provide flexibility and ability to adapt to different uses
  - f. include a stratum subdivision scheme to delineate land use separation, ownership structures and obligations to the overall building regarding requiring owners’ corporation consent for the submission of development applications and complying development certificates for employment uses separate from residential uses.
- C46. Locate:
  - a. the primary pedestrian access for ground floor employment uses on Parramatta Road
  - b. residential pedestrian access on Croydon Road
  - c. ground floor entries at the same level as the street to maximise accessibility for all users.
- C47. The building façade along Parramatta Road and Croydon Road incorporates awnings:
  - a. at entries and lobbies
  - b. that extend to the property boundary
  - c. do not impact on landscape areas or tree planting
  - d. cantilever from the top of the ground floor.

- C48. Acoustic Report demonstrates the adequacy of the design, construction methods and materials so that land use conflicts are minimised between employment and residential uses through, among other methods:
- a. implementing a minimum 400mm thick floor slab, or alternate treatment, for acoustic attenuation
  - b. incorporating construction methods and materials that insulate against noise and vibration transmission, on-site and from surrounding employment uses
  - c. designing and locating services and equipment (plant, goods lifts) to minimise amenity impacts.

Notes:

1. Read built form requirements in conjunction with 14.3.4 Streetscape and public domain, 14.3.9 Active street frontages, 14.3.10 Built form, 14.3.11 Building materials and finishes, and 14.3.12 Landscaping.
2. Figures 14 provides an indicative built form for Opportunity Sites.
3. Floor to floor heights include a slope/topography allowance.
4. Variation to built form controls may be considered on site specific basis where the proposed development demonstrates achievement of the Precinct wide (14.3.2) and Area 3 (14.6.2) Desired Future Character and Precinct wide (14.3.10) and Area 3 (14.6.4) Built form Objectives and delivers better outcomes.

**Figure 14:** Opportunity Sites 1 and 2 – indicative built form that achieves storeys, building envelope, scale and site layout requirements – plan and axonometric views



**14.6.5. Heritage****Objectives**

O48. To conserve and enhance the heritage significance of the Heritage Item at 590 Parramatta Road, Croydon and demonstrate that achievement of the floor space ratio and height of buildings incentives provisions will not have any negative impact on the item.

**Controls**

- C49. Heritage Impact Statement demonstrates that the development and any proposed works:
- a. within the heritage item site:
    - i. result in the positive adaptive reuse of the item in a manner that retains heritage significance and existing built form
    - ii. retains the existing built form fabric, façade, openings and windows, internal proportions and levels
    - iii. that all new building elements are appropriately sited and designed to not dominate the scale and character of the item
  - b. on Opportunity Site 2: 590–610 Parramatta Road, Croydon and 235–237 Croydon Road, Croydon:
    - i. appropriately site and design development to respect and respond to the item
    - ii. ensure development does not physically overwhelm or dominate the item
    - iii. using sympathetic materials, colours and finishes that reflect and harmonise with materials of the item.

**14.6.6. Vehicle and service access locations**

**Objectives**

- O49. To ensure vehicle and service access:
  - a. reduces vehicular movements to and from Parramatta Road
  - b. does not impact on achieving an active street frontage
  - c. prioritises pedestrian movement along Parramatta Road
  - d. does not result in an unsafe pedestrian or cycling environment
  - e. does not visually dominate the Parramatta Road streetscape
  - f. makes use of existing laneways, secondary streets and Croydon Road.

**Controls**

- C50. Vehicle and service access:
  - a. for Opportunity Site 1- the rear accessway laneway off Croydon Road
  - b. for Opportunity Site 2:
    - i. from Croydon Road
    - ii. creates a splayed corner at the intersection of Parramatta Road and Croydon Road to facilitate improvements for public transport access.

**14.6.7. Landscaping**

**Objectives**

- O50. To ensure landscaping is provided in the front setbacks along Parramatta Road and Croydon Road that:
  - a. softens the appearance of building façade
  - b. enhances pedestrian amenity
  - c. contributes to defining this important intersection.

**Controls**

- C51. Landscaping Strategy demonstrates that landscape:
  - a. in the front setback to Parramatta Road is a minimum of 1.5m, supports ground floor employment uses, and enhances amenity on Parramatta Road
  - b. is designed to prioritise pedestrian movement along Parramatta Road and Croydon Road specifically at business and residential entries
  - c. forms an integral element of the built form design including green roofs, terraces, walls and other features.

Note: Landscaping requirements should be read in conjunction with Section 14.3.4 Streetscape and public domain, 14.3.7 Sustainability and resilience and 14.3.12 Landscaping.



## Summary of Post Gateway changes to the Parramatta Road Corridor Planning Proposal and draft DCPs

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**Purpose**

On 20 October 2022, DPE issued a Gateway determination stating that the Planning Proposal should proceed through the statutory Local Environmental Plan (LEP) making process subject to conditions.

Conditions of the Gateway determination required amendments to the Planning Proposal to address technical changes relating to:

- Proposed land use approach
- Urban design issues in the Leichhardt and Taverners Hill Precincts
- Local Planning Direction relating to Flooding through additional flood impact assessment study
- High performance building requirements in line with the Sustainable Buildings SEPP 2022
- State infrastructure contributions
- Remove local provisions within the Proposal relating to:
  - provision of on-street rapid transit along Parramatta Road
  - tree canopy targets in public domain
  - additional heritage provision
  - community infrastructure contributions

The purpose of this document is to outline t changes made to the Planning Proposal in response to the studies completed to address Gateway conditions. There are consequential changes to the draft DCPs which have also been outlined in this report.

In the first section of this report, Table 1 outlines the findings of the flooding, urban design and sustainability studies and describes the corresponding changes to the Planning Proposal and DCPs.

Table 2 outlines all the Gateway conditions and changes made to the Planning Proposal/ draft DCPs in response to these Gateway conditions.



## Outcomes of Post-Gateway Technical studies

Table 1 – Outcomes of Post Gateway studies

<b>1. Flooding</b>
<p>Council engaged WMS Engineering to undertake a Flood Risk Impact Assessment (FIRA) in response of the Gateway condition 1(d) which required the Planning Proposal to address consistency with Local Planning Direction 4.1 Flooding. Detailed mapped outputs for the post-development scenario depicting flood depth, velocity, hazard and afflux can be found in Appendix 5 of the Planning Proposal.</p> <p>The flood modelling outlines both pre and post development flood modelling for various design options and made the following recommendations to inform the Planning Proposal:</p> <p><b>Leichhardt Precinct:</b></p> <ul style="list-style-type: none"> <li>The proposed development in the study area has minimal influence on flood patterns, demonstrating its compatibility with the region's flood dynamics. Development at the corner of Crystal Street and Petersham Lane has negligible impacts on existing water levels. These results underline the project's viability and effectiveness in managing flood risks, providing a strong foundation for sustainable urban development within the Leichhardt Precinct.</li> </ul> <p><b>Taverners Hill:</b></p> <p>North of Parramatta Road</p> <ul style="list-style-type: none"> <li>Flood behaviour within the study area to the north of Parramatta Road remains generally similar to that observed in the pre-development scenario. The key areas affected by flooding are the trapped low points in Upward Street and Tebbutt Street, and the area near the railway embankment downstream.</li> <li>The flood hazard afflux results indicate that there is very limited change in flood hazard classification within and in the vicinity of the study area as a result of the proposed development.</li> <li>Based on the above, the proposed building setbacks are considered acceptable from a flood risk perspective as there is no significant increase in risk to life and there is an improvement in flood conditions within the upstream area of the site.</li> </ul> <p>South of Parramatta Road:</p> <ul style="list-style-type: none"> <li>The Hawthorne Canal Flood Study indicates that this area is generally subject to limited flood risk, and therefore only a qualitative assessment (i.e., no modelling) was required to be undertaken.</li> <li>Sub-area 3 to the east of Old Canterbury Road is slightly flood affected for the same storm events, with floodwaters slightly inundating the south side of the area along the railway line due to lower topography. Flood depths range between 0.3 to 0.5 m, which is relatively low. It is recommended to adjust the proposed setback to a minimum of 4.9m on the southwest corner and 10.9 m on the southeast corner from the southern boundary.</li> </ul> <p><b>Kings Bay/ Croydon:</b></p> <ul style="list-style-type: none"> <li>For the PMF event, flood depths exceeding 2 meters were observed in the majority of the inundated areas near Iron Cove Creek. This was mainly due to the creek's water</li> </ul>



flow. Specific areas such as West Street, Sunbeam Avenue, and Parramatta Road experienced inundation depths of up to 2 meters.

- Under the conditions of the Probable Maximum Flood (PMF) storm event, the impact on both existing and proposed structures in the area becomes significant. There are established building blocks situated at the corner of Parramatta Road and Iron Cove Creek, alongside the proposed development at 223-225 Croydon Road.
- In most of the area, flood velocities remain below 0.5 m/s, indicating a lower impact category. However, specific areas like West Street near Iron Cove Creek have observed higher flood velocities exceeding 2 m/s, signifying a major impact category. This can be attributed to the water flow direction of this street towards Iron Cove Creek.
- The proposed development at the intersection of West Street and Croydon Road (223-225 Croydon Road) contributed to a change in hazard category, in part from H1 to H5.
- The proposed building layout in the areas of West Street, Sunbeam Avenue, and Croydon Road may have a slight impact on water levels in these areas during severe storms.
- Consequently, the combined development at the corner of West Street and Croydon Road (223 and 225 Croydon Road) is not recommended.
- Conversely, at the corner of Parramatta Road and Iron Cove Creek, a significant decrease of 0.2 meters in water level was observed on the north side of the building or along Parramatta Road. This decrease is concerning as it restricts flood flow during the PMF storm event in Iron Cove Creek. The presence of the proposed building at this location could potentially hinder the natural flow of floodwaters.
- Based on the above development at the intersection of Parramatta Road and Iron Cove Creek should be reduced/removed.
- Further design testing is required to determine the appropriate level of development/FSR on the Opportunity Site at the intersection of Parramatta Road and Iron Cove Creek with an increased setback. Consequently, the site is recommended to be removed from the Stage 1 Planning Proposal and be considered for a future LEP amendment.

Appendix 5 – Flood Studies has been replaced with the latest Flood Studies by WMS (2023).

**Implications for the Planning Proposal/ draft DCPs:**

**Planning Proposal**

- Updates to the Stage 1 Implementation area based on deletion of the following sites in Kings Bay/ Croydon precinct due to flooding impacts:
  - 223 and 225 Croydon Road, Croydon
  - Opportunity Site 3 – 582-584 Parramatta Road, Croydon
- Corresponding reduction in the proposed number of dwellings by 99 in Kings Bay/ Croydon precinct
- Remove Opportunity Site 3 from Part 2 Site-specific Local Provisions
- Updates to the response to Local Planning Direction 4.1 Flooding

**Maps:**

- Updates to Kings Bay/ Croydon staging map



- Remove the above-mentioned sites from LZN, FSR, HOB, KYS, OPS, LUT maps

**Draft DCPs:**

- Update Kings Bay/ Croydon Precinct DCP to remove the above-mentioned sites and associated diagram changes.
- Change setbacks for the following site in Taverners Hill Area:
  - Adjust setbacks for the site 45-53 Old Canterbury Road, Lewisham to a minimum of 5m on southwest corner

**2. Urban Design**

Council engaged Architectus to undertake the Urban Design Analysis for Parramatta Road Corridor Stage 1 for Leichhardt, Taverners Hill and Kings Bay/ Croydon Precincts. In response to the gateway conditions, specifically condition 1(c)i-iii, scope of work involved investigating specific sites in Taverners Hill and Kings Bay/ Croydon Precincts to justify the inconsistency between the Planning Proposal and PRCUTS with regard to proposed height of building (HOB) and floor space ratio (FSR).

Additional to this work, another review of all three precincts was undertaken to:

- incorporate the increased floor to floor heights for residential flat buildings in accordance with the National Construction Code updates (2023)
- address comments from the Inner West Architectural Excellence Design Review Panel in relation to establishing primary setbacks for a consistent street wall and to parapets of buildings in HCAs,
- review of sites against the ADG to address increased heights and where required solar and overshadowing testing,
- update to the new equivalent Employment Zones, and
- incorporate changes to built form, all diagrams, LEP and DCP maps and recommendations.

This work has resulted in revised HOBs for all sites in the Stage 1 Implementation area with additional height in the order of 0.5m to account for the NCC updates.

FSRs and heights for the following sites in Leichhardt Precinct have now been recalibrated as follows: (Detailed urban design testing provided in Appendix 2)

**Proposed Incentive FSRs:**

- Properties 1 McDonald Street, 74-76A Balmain Road reduced from 1.9:1 to 1.4:1
- Properties 93-97 Norton Street reduced from 3:1 to 2.7:1.
- Properties 2-8 McDonald Street reduced from 1.9:1 to 1.5:1.
- Properties 64-72 Balmain Road increased from 1.9:1 to 2.1:1.
- Properties 56-62 Balmain Road increased from 1.9:1 to 2.3:1.

**Proposed Incentive HOBs:**

- Properties 99 Norton Street, 8A-14 McDonald Street increased from 18m to 20.5m.
- Properties 2-8 McDonald Street, 64-72 Balmain Road increased from 18m to 21.5m.
- Properties 56-62 Balmain Road increased from 18m to 25m.
- Properties 1 McDonald Street and 74-76A Balmain Road reduced from 18m to 15.5m.
- Properties 93-97 Norton Street increased from 23m to 27m.



- Heritage Conservation Area along Parramatta Road – Minor changes to setback controls requiring minimum 3m setback from 2nd-5th storey and 9m from 6th storey (from boundary).

This has consequently resulted in reduction of proposed dwellings in Leichhardt Precinct by 57 from 764 to 707.

In addition to the above, this work found some minor modelling errors in the FSR calculations for Taverners Hill Precinct which have now been recalibrated as per below:

- Proposed FSR of block between Beeson and Kegworth Street reduced from 1:1 to 0.9:1
- Proposed FSR of block between Beeson and Hathern Street reduced from 1.4:1 to 1.2:1
- Proposed FSR of block between Tebbutt and Upward Street reduced from 1.5:1 to 1.4:1
- Proposed FSR of block to the west of Old Canterbury Road reduced from 1.1:1 to 1:1

This has resulted in a reduction of proposed dwellings by 45 in Taverners Hill precinct from 438 to 393.

Appendix 2 – Architectus Urban Design Study has been replaced with the latest Architectus Urban Design Study 2023.

**Implications for the Planning Proposal/ draft DCPs:**

**Planning Proposal**

- Updates to dwelling numbers in Leichhardt and Taverners Hill Precincts and overall dwelling numbers

**LEP Maps**

- Updates to existing and proposed Zoning to reflect Employment Zone Reforms
- Updated Incentive HOB Maps for all precincts
- Updated Incentive FSRs for sites in Leichhardt and Taverners Hill Precincts

**Draft DCPs**

- Updates to setbacks for Parramatta Road Heritage Conservation Area and associated diagram changes.

**3. Sustainability**

Council engaged WSP Pty Ltd to undertake a technical analysis relating to proposed sustainability provisions in Council's Parramatta Road Stage 1 Planning Proposal for Leichhardt, Taverners Hill and Kings Bay/ Croydon Precincts. This scope of this study aims to address DPE's gateway conditions relating to sustainability targets, including Condition 1(h), 1(i) and Condition 3(d).

The Planning Proposal was reviewed against the Sustainable Buildings SEPP, PRCUTS Planning & Development Guidelines and national best practice building policies to ensure the sustainability controls are relevant and aligned with best practice examples. Where the Study found that the proposed planning controls do not exceed the Sustainable Buildings SEPP or align with the Guidelines, alternative planning controls for the Planning Proposal and supporting DCPs were recommended to ensure the provisions are appropriate for



triggering the proposed incentive FSR and height controls and achieving the objective of delivering high-performing buildings.

The findings and recommendations from the Sustainability Study have been integrated into the Planning Proposal and supporting DCP Amendments. Additionally, the Sustainability Study has been included as Appendix 9 of the Planning Proposal.

**Implications for the Planning Proposal/ draft DCPs:**

**Planning Proposal**

- The proposed residential energy targets have been updated to align with the new BASIX 2022 tool and scoring methodology and increased in stringency where they do not exceed the Sustainable Buildings SEPP 2022.
- The thresholds for residential buildings 6-storeys and over have been updated to align with the Sustainable Building SEPP 2022.
- The proposed hotel energy targets have been increased to exceed the minimum requirements in the Sustainable Buildings SEPP 2022.
- The proposed retail energy targets have been increased to align with the PRCUT Planning & Design Guidelines.
- A new provision has been introduced to include serviced apartment energy and water targets aligned with the large commercial development definition.
- The thresholds triggering energy targets for additions to office and shopping centre developments have been amended from a percentage (50%) to lettable area (500m2 for office and 2500m2 for shopping centre)
- The proposed performance standards for non-residential development have been updated to use the term “office” rather than “commercial development”
- The proposed provision for workers facilities has been amended to satisfy the Gateway conditions.
- The proposed provision that considers reduced sustainability requirements for heritage items has been removed.
- The proposed overall precinct/zone-based tree canopy targets (including streets) have been removed.

Appendix 9 – Parramatta Road Corridor High Performance Buildings has been replaced with the WSP Sustainability Study 2023.

**Draft DCPs**

- Worker bicycle parking rates for industrial buildings have been updated to align with best practice controls.



### Response to Gateway Determination Conditions

Table 2 – Response to the Gateway Determination conditions

Gateway Conditions	Response
Prior to public exhibition, the planning proposal (including relevant appendices) is to be revised to address the matters set out below:	
(a) include an explanatory note that future development will be subject to state/regional infrastructure contributions in accordance with the implementation actions in the Parramatta Road Urban Corridor Transformation Strategy Implementation Update 2021	Completed – Refer to section 2.12 of the Planning Proposal.
(b) include a figure or figures in the planning proposal that clearly identify all land and sites that are subject to the planning proposal	Completed – Refer to Figure 1 in the Planning Proposal and LEP Maps package.
(c) address consistency with section 9.1 Direction 1.5 Parramatta Road Corridor Urban Transformation Strategy, including:	
i. the proposal seeks a height of 23m rather than 17m and a FSR of 3:1 rather than 1.9:1 for 97 Norton Street, Leichhardt. Appendix 12 to the planning proposal refers to the Urban Design Study, however it is unclear that the site is specifically discussed in the Urban Design Study;	Completed – Refer to Appendix 2 - Urban Design Study which has been updated to review the proposed controls for the North of Leichhardt Investigation Area. This has resulted in changes in the proposed FSR/HOBs for this area as also shown in Appendix 1 – LEP Maps Package.
ii. the proposal seeks a height of 23m rather than 17m and a FSR of 3:1 rather than 1:1 for 23 Norton Street, Leichhardt. Appendix 12 to the proposal acknowledges the FSR variation but not the height variation. Update Appendix 12 to the planning proposal to acknowledge the inconsistency and provide justification; and	Completed – Refer to Appendix 11 Section 8.0 Variations sought to PRCUTS.



Gateway Conditions	Response
<p>iii. part of 35-53 Old Canterbury Road, Lewisham is proposed to remain with a FSR of 1.1:1 and is identified on the proposed FSR incentive map as 1.1:1. The incentive FSR map is to be updated accordingly to remove the area from the map.</p>	<p>Completed – Refer to the Appendix 1 – LEP Maps Package.</p>
(d) address consistency with section 9.1 Direction 4.1 Flooding, including:	
<p>i. update the planning proposal to address relevant recommendations of the NSW Government's 2022 Flood Inquiry Report;</p>	<p>Additional flood modelling has been undertaken to address the Section 9.1 Local Planning Direction 4.1 and NSW Government's 2022 Flood Inquiry Report. Refer to Appendix 5.</p>
<p>ii. clearly address the requirements of Direction 4.1, providing clear assessment and consideration the level of flood hazard(s) that may impact the proposal; and</p>	<p>Completed – refer to Appendix 5. This work has resulted in removal of the below sites in Kings Bay/ Croydon Precinct as the proposed developments were resulting in the obstruction of the overland flow path between the existing properties with notable increases in flood depth, velocity and hazard during the PMF storm events.</p> <ul style="list-style-type: none"> <li>• 223 and 225 Croydon Road, Croydon</li> <li>• Opportunity Site 3 – 582-584 Parramatta Road, Croydon</li> </ul> <p>This change in the Planning Proposal area will result in a reduction of 99 dwellings from what was originally proposed in the Kings Bay/ Croydon precinct.</p>
<p>iii. remove references to outdated Direction 4.1 numbering.</p>	<p>Completed – references have been updated throughout the Planning Proposal.</p>
<p>(e) to contemplate the suitability of the use of the R1 General Residential and/or R4 High Density Residential zones under Inner West LEP 2022 to remove the need to rely upon 'residential flat buildings' as an additional permitted use for land</p>	<p>Completed – Refer to Part 3 Justification Section A Q1 Additional justification of proposed amendments.</p>



Gateway Conditions	Response
zoned R3 Medium Density Residential	
(f) remove the proposed additional heritage local provision	Completed – Refer to Section 2.3 of the Planning Proposal.  This is considered acceptable as IWLEP 2022 Clause 5.10 Heritage Conservation already stipulates heritage requirements.
(g) review and correct as required existing and proposed maximum building heights and floor space ratio provisions to ensure the planning proposal and proposed mapping are consistent	Completed – Adjustments have been made to the proposed maximum building heights and floor space ratio provisions in line with Architectus’s Urban Design study August 2023. Refer to Appendix 1 – LEP Maps Package.
(h) to include an assessment of the proposed sustainability provisions against State Environmental Planning Policy (Sustainable Buildings) 2022 (Sustainable Buildings SEPP). This must outline how the proposed incentive targets relate to the targets set out in the Sustainable Buildings SEPP	Completed – Refer to Appendix 9 – Sustainability Report. This report includes an assessment of proposed sustainability provisions against the Sustainable Buildings SEPP 2022. Adjustments have been made to the Planning Proposal’s proposed incentive targets in line with the recommendations of this work. Refer to Section 2.7 of the Planning Proposal for the proposed sustainability provisions.
(i) in relation to the proposed performance standards for non-residential development, update the proposal to use the development type term ‘office’ rather than ‘commercial development’ or provide justification as to why the term commercial development is preferred	Completed – Reference to ‘commercial development’ has been replaced with ‘office’ development. Refer to Table 10 in Section 2.7 of the Planning Proposal.
(j) include a table in the planning proposal that clearly demonstrates indicative zoning under the Department’s employment zones reforms	Completed – all employment zone references have been updated throughout the Planning Proposal.
(k) amend the proposed workers facilities provision to reframe it as an overarching clause setting out aims and objectives, the detailed	Completed – refer to Section 2.7 (iii) of the Planning Proposal.



Gateway Conditions	Response
requirements may be contained in a Development Control Plan (DCP)	This is a minor change and proposed new wording adequately covers off the objectives for workers/ end of trip facilities.
(l) remove references to the finalisation of the draft Design and Place State Environmental Planning Policy (SEPP) 2021	Completed – references to the draft Design and Place SEPP 2021 have been removed from the Planning Proposal.
(m) remove the proposed clause that considers reduced sustainability requirements for heritage items	Completed – refer to Section 2.7 of the Planning Proposal.
(n) remove the proposed overall precinct/zone-based tree canopy targets (including streets)	<p>Completed – refer to Section 2.8 of the Planning Proposal. Note that proposed site-based tree canopy targets are still included in the Planning Proposal.</p> <p>The removal of overall precinct/zone-based tree canopy targets (including streets) is considered acceptable as these cannot be assessed on a site-by-site basis at the Development Application stage. Site-by-site targets are more relevant and will continue to be included in the Planning Proposal.</p>
(o) remove the proposed incentive requirement for all car parking to be provided as unbundled parking in new developments	<p>Completed – requirement for unbundled parking has been removed and replaced with an overarching objective to encourage unbundled and decoupled car parking and car share schemes. Refer to Section 2.9 of the Planning Proposal.</p> <p>This requirement has now been transferred to the DCPs.</p>
(p) provide a plain English explanation of intent for the proposed community infrastructure contributions (CIC) clause for the Leichhardt Precinct, noting that the Department is unable to support a CIC levy that does not conform with the existing legislative framework for infrastructure funding under the Environmental Planning and Assessment Act 1979	<p>Completed – requirements for developments in Leichhardt precinct to make Community Infrastructure Contributions have been removed.</p> <p>DPE have advised that community infrastructure contributions must be delivered on-site, and any items within the existing public reserve (i.e. public road or public open space) must be delivered through alternative infrastructure contribution mechanisms. Within the Leichhardt Precinct, additional</p>

Gateway Conditions	Response
	through-site links will be secured by the LEP and DCP. The Planning Proposal requires the provision of through-site links of minimum dimensions in order to access FSR and height incentives. While these will be publicly accessible, they will not be dedicated to Council, and as such a Community Infrastructure Policy is not required to secure their delivery. All references to Value Sharing Study have also been consequently deleted from the Planning Proposal.
(q) remove the two proposed transport infrastructure provisions	Completed – refer to the Planning Proposal. Whilst this provision has been now deleted at DPE’s request, it is strongly recommended that NSW Government commits to introducing an on-street rapid transit system along Parramatta Road as required under PRCUTS.
(r) update the project timeline to reflect the progress of the planning proposal and Gateway timeframes	Completed – refer to Part 6 Project timeline of the Planning Proposal. Also updated in line with the DPE letter to Council dated August 2023.
(3) Prior to finalisation, the planning proposal to be updated to:	Note that these updates are required to be dealt prior to finalisation only. Where possible, Council officers have addressed these conditions as pre-exhibition.
a) address the Implementation Actions in the Parramatta Road Urban Corridor Transformation Strategy Implementation Update 2021 to:	
i. ensure the planning proposal aligns with any transport or infrastructure plan developed by the NSW Government; and	Completed. The Planning Proposal aligns with the NSW Government’s Future Transport Strategy.
ii. address the recommendations and outcomes of the Precinct-wide traffic studies.	Completed – refer to Table 1 of Appendix 10.
b) provide additional analysis demonstrating that the tree canopy	In progress – to be completed prior to finalisation.

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Gateway Conditions	Response
<p>targets (% of site area) and the deep soil target can be achieved on a site-by-site basis</p>	
<p>c) provide feasibility analysis considering the zoning, height and floor space ratio and other requirements for development including design excellence, affordable housing contributions, local and community infrastructure requirements and contributions, state or regional contributions and sustainability requirements. This updated analysis should also account for any amendments to the planning proposal that occur as part of the plan making process</p>	<p>In progress – to be completed prior to finalisation.</p>
<p>d) ensure that the thresholds for BASIX standards which trigger the incentive provisions are appropriate having regard to the Sustainable Buildings SEPP</p>	<p>Completed – refer to section 2.7 of the Planning Proposal and supporting Sustainability Study at Appendix 9 for further justification.</p>



## Conclusion

Below is a summary of key post-Gateway changes to the **Planning Proposal**:

### All precincts:

1. Updates to the Planning Proposal to remove references to:
  - a. Additional heritage provisions
  - b. Transport infrastructure provisions
  - c. Community Infrastructure Contributions for Leichhardt precinct
  - d. Reduced sustainability requirements for heritage items
  - e. Overall precinct/zone-based tree canopy targets (including streets)
2. Amended controls for the following sections:
  - a. High Performance Buildings and Improved Environmental Outcomes (Sustainability targets and workers facilities)
  - b. Urban Heat Mitigation (Tree canopy cover)
  - c. Car Parking (Unbundled Parking)
3. Updated references to new Employment Zones
4. Additional justification in relation to:
  - a. Proposed land use zoning approach R3 Medium Density Residential with 'residential flat buildings' as an additional permitted use for the Planning Proposal area
  - b. Section 9.1 Local Planning Direction 4.1 Flooding
  - c. Section 9.1 Local Planning Direction 1.5 Parramatta Road Corridor Urban Transformation Strategy (Implementation Update 2021)
5. Updated Incentive Height of Buildings (HOB) for all sites (no change to number of storeys)
6. Overall, reduction of proposed dwellings from 1717 to 1516 and decrease in number of proposed jobs from 2022 to 1944
7. Updated Project timeline

### Leichhardt:

8. Revised Incentive Floor Space Ratios (FSRs) and Height of Buildings (HOBs) for North Leichhardt Investigation Area including sites:
  - a. 93-99 Norton Street
  - b. 56-76A Balmain Road,
  - c. 2-14 McDonald Street

### Taverners Hill:

9. Revised Incentive FSRs and HOBs for the following blocks:
  - a. Between Beeson and Kegworth Street
  - b. Between Beeson and Hathern Street
  - c. Between Tebbutt and Upward Street
  - d. West of Old Canterbury Road

### Kings Bay/ Croydon:

10. Deletion of the following sites from the Planning Proposal area:
  - a. 223 and 225 Croydon Road, Croydon



- b. Opportunity Site 3 – 582-584 Parramatta Road, Croydon  
11. Remove Opportunity Site 3 from Part 2 Site-specific Local Provisions

Below is a summary of key post-Gateway changes to the **draft DCPs**:

The following changes were made in response to:

- Inner West Architectural Excellence Design Review Panel feedback of 5 April 2022, such as use of figures and floor to floor heights.
- Flood Impact Risk Assessment (WMS Engineering, 2023) advice relating to mitigating flood impact on a number of sites.
- Recommendations provided in the Sustainability Study (WSP, 2023) relating to bicycle parking for industrial developments.
- Recommended actions outlined in Parramatta Road Corridor Traffic and Transport Study (Camperdown, Taverners Hill, Leichhardt) and Kings Bay Transport Assessment (Cardno/Stantec, 2022) to include requirements for new development to prepare a sustainable transport plan and provide unbundled parking.
- General drafting changes to increase clarity of content.

**All precincts:**

1. Minor rewording of:

- C5. relating to utility infrastructure making clear this control applies to the primary street frontage.
- C9. To clarify that in relation to urban heat effects, lighter coloured materials have beneficial high solar reflectivity attributes and darker colours can increase glare and reflection.
- New Objective 09. and Control C9. for large development, as defined, to reduce private motor vehicle use, minimise traffic impacts and encourage sustainable transport.
- Minor amendment to O11. And C11. relating to private parking being updated to include car share and unbundled parking being on a separate title.
- C17. Relating to bicycle parking updated to include 1 space per 10 staff, rather than per m<sup>2</sup>, for industrial development.
- C23. Relating to building materials modified to remove reference to light reflectivity that is now contained in C9.

**Leichhardt Precinct:**

Area 1 – North of Parramatta Road

1. 14.1, Figure 1. Amended to introduce new Area 5 and Area 6 with text and figure updated.
2. Figure 3. Amended location of desired through site link between Norton Street and McDonald Street to boundary with Leichhardt Public School.
3. C30, C32, Figure 9 and Figure 10, Amended to reduce street wall height to 2 storeys and requirement for setbacks to parapet and 6<sup>th</sup> storey.
4. Area 3. Land included in Area 3 amended to remove 93-99 Norton Street. These form new Area 5. Consequent amendment to Figure 11.
5. C43. Dimension of urban plaza specified.

6. 14.7. New section inserted – Area 5 – Leichhardt: Norton Street North to reflect additional built form testing. This includes new controls (C50-C56) and Figures 21.
7. 14.8. Revised section – Area renumbered from Area 5 to Area 6. New controls (C57-C62) that respond to additional built form testing. New Figure 21 – Area 6 Leichhardt: Balmain Road.

Area 2 South of Parramatta Road

1. Figure 3. Amended location of desired through site link between Norton Street and McDonald Street to boundary with Leichhardt Public School.
2. C30, C32, Figure 9, Amended to reduce street wall height to 2 storeys and requirement for setbacks to parapet and 6<sup>th</sup> storey.
3. Figure 12. Legend amended to show easement on Petersham Lane.

**Taverners Hill Precinct:**

Area 1 – North of Parramatta Road

1. C19. Remove Figure 5 – preferred lot amalgamation pattern and update controls accordingly.
2. Minor amendment to C20. Table 1 to include reference to minimum/maximum measures and increase above ground floor-to-floor height from 3.1m to 3.2m.
3. Remove Figures 6, 7, 8 and 9 which repeat content in Table 1 and may limit built form design when taken literally.

Area 2 – South of Parramatta Road

1. Minor amendment to C20. Table 1 to include reference to minimum/maximum measures, increase above ground floor-to-floor height from 3.1m to 3.2m and increase of side setback from 3m to 5m for Old Canterbury Road east side, southern site to accommodate flood impacts.
2. Remove previous Figure 5 for Old Canterbury Road west and Barker Street east which repeat content in Table 3 and may limit built form design when taken literally.
3. Remove axonometric view from Figures 6 and 7.

**Kings Bay/ Croydon Precinct:**

1. Throughout - update content and maps to remove the following sites and any associated content due to flooding constraints:
  - Area 2 – Kings Bay: Dalmar Street amended to remove two lots on south-eastern corner of Croydon Road and West Street.
  - Area 3 – Kings Bay Opportunity Sites amended to remove Opportunity Site 3 – 582-584 Parramatta Road, Croydon.
2. Throughout - Remove reference to delivering an active green transport link along Iron Cove Creek. This directly relates to 582-584 Parramatta Road which no longer forms a part of the Land Application Area.
3. Minor amendment of:
  - C28. remove reference to floor-to-floor height for upper storeys.
  - C29. and C44. provide more flexibility about ground floor active employment uses.
  - C31. include controls to facilitate continuation of Sophie Lane through to Scott Street.
  - C34. include reference to deep soil requirements and tree size and spread.

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- C37. Table 3 – increase floor-to-floor height above ground floor from 3.1m to 3.2m and standardise minimum side setback, except on corner lots.
  - O41. and C41. updated to separate built form needs of ground floor and above ground levels and increase above ground floor-to-floor height to 3.2m.
4. Remove Figures 12, 13, 14 and 15 which repeat content in Table 3 and may limit built form design when taken literally.

