

The Cyprus Club Planning Proposal

58-76 Stanmore Road,
2-20 Tupper Street and
1,3-9 Alma Avenue

Draft Development Control Plan Framework



Purpose of the Draft DCP Framework

This *Draft Development Control Plan (DCP) Framework* document has been prepared to accompany the Planning Proposal (PP 2021/2911) to address the conditions of the gateway determination for the redevelopment of the Cyprus Club, Stanmore.

The purpose of the Draft DCP Framework is to establish the key principles for future development, and to provide an overview of the proposed site specific DCP controls to support the Planning Proposal for the site.

This Framework outlines the vision and principles that will guide the key planning controls that will inform a detailed site specific DCP for the site.

A detailed site specific DCP for the site will be prepared in consultation with Council staff following exhibition of the amended Planning Proposal and this Framework document.

Contents

- 1.The Site
- 2.Vision
- 3.Existing Character
- 4.Desired Future Character
- 5.The Master Plan
- 6.Land Use
- 7.Precinct-specific planning controls
- 8.Built Form Typology
 - a)Heights
 - b)Floor Space Ratio
 - c)Setbacks
- 9.Open Space
 - a)Landscape
 - b)Public Open Space
- 10.Access and Connectivity
 - a)Vehicular Access
 - b)Pedestrian Access and Connectivity
 - c)Road Widening
- 11.Solar Access
- 12.Heritage
- 13.Sustainability
- 14.Waste Management
- 15.Operation of Sydney Airport
- 16.Noise and Vibration
- 17.Contamination

The Site

- The Site is a rectangular shape and has a total area of 9,129 sqm. The total frontage to Stanmore Road is 66.915 m. The frontage to Alma Avenue is approximately 134 m and the frontage to Tupper Street is approximately 113 m.
- The Site contains the existing Cyprus Club which is a part three and part four storey building in the northeast corner of the site. The club contains bars, a restaurant and function rooms. The car parking area for the club is located at grade on the western portion of the site.
- The Site also includes 6 residential properties and an electrical substation.
- There are several mature trees throughout the car parking area. Vehicle access to the car park is via Alma Avenue. The southern portion of the site is currently a vacant grassed area.
- The Site is located approximately 4 km south-west of Sydney CBD and within 800m walking distance to Stanmore Railway Station.
- There are multiple bus stops within 500 m of the site serviced by a variety of routes which link to local centres and railway stations and to the Sydney CBD.
- The surrounding area is characterised by mainly residential development, supported by local schools and commercial core type land uses located on Enmore Road .



Vision

To create a connected community full of life with the Cyprus Community Club assets at the heart, where moments, memories and experiences are created and remembered respecting the history of the non-for-profit community club from 1929.

The Masterplan brings a range of (residential, commercial, retail, the Club and well-designed larger apartments) together to create an aspirational and desirable intergenerational precinct with significant community benefits and an outstanding legacy for the Cyprus Community Club.

An innovative housing model, will position the development as unique and a first of its kind for Sydney and subsequently, place the Community Club and the Inner West Council as forward-thinking organisations.

The demographics and richness of the social groups of Stanmore and the surrounding suburbs indicate that, the local community is yearning for a new and invigorated precinct that will bring all generations together through entertainment, arts, culture, retail and lifestyle. This vision entails four principles essential to meeting Sustainability, Environmental and Social components of the proposal which include.

- 1.- The Cyprus Club, the benefits derived from the planning proposal will give back to the community via supporting several groups in need of assistance
- 2.- Continuing the legacy of The Cyprus Community Club which commenced in 1929, and set the foundations for the next century, with the club taking an absolute centrepiece on the site and owning the land into the future
- 3- A true multigenerational community – spaces and places for all ages
- 4.- Creation of larger apartments to create high levels of compliance with the ADG guidelines whilst promoting well over above regulatory requirements in relation to sustainability



Sketch of Public Plaza (Source: PA Studios)

Existing Character

The Site is bound by Stanmore Road towards the north, Alma Avenue towards the southwest and Tupper Street towards the southeast. Stanmore Road is the main local road which connects the Site to the surrounding Enmore commercial core.

The Site contains the existing Cyprus Club which is a part three and part four storey building in the northeast corner of the Site. The club contains bars, a restaurant and function rooms. The car parking area for the club is located at grade on the western portion of the Site. The Site also includes 6 residential properties and an electrical substation.

The immediate surrounding locality consists of predominately low-medium density residential purposes towards the north, south, east and west. The Site is located immediately south of the Kingston South Heritage Conservation Area and near several local heritage items.

The wider locality includes a range of public open space, educational establishments, medium density residential development and the commercial centres of Stanmore, Enmore and Newtown.

Stanmore Railway Station is located approximately 560 m to the north-west of the Site. There are also multiple bus stops within 500 m of the Site serviced by a variety of routes which link to local centres and railway stations and to the Sydney CBD. The Site is located approximately 4 km south-west of Sydney CBD and within 800 m walking distance to Stanmore Railway Station.



Locality Map (Base Source: Six Maps)

Desired Future Character

The Cyprus Club precinct will be an active mixed-use locality, supporting the redevelopment of the existing Cyprus Club. The Site will support residential and commercial floor space to promote diverse housing choice and enable the community to be supported by commercial tenancies (retail). The precinct will improve open space opportunities and provide green infrastructure upgrades to support and enhance the existing streetscape. The precinct is to accommodate and respect the surrounding heritage characteristics through its built form. The Precinct will achieve this through the following :

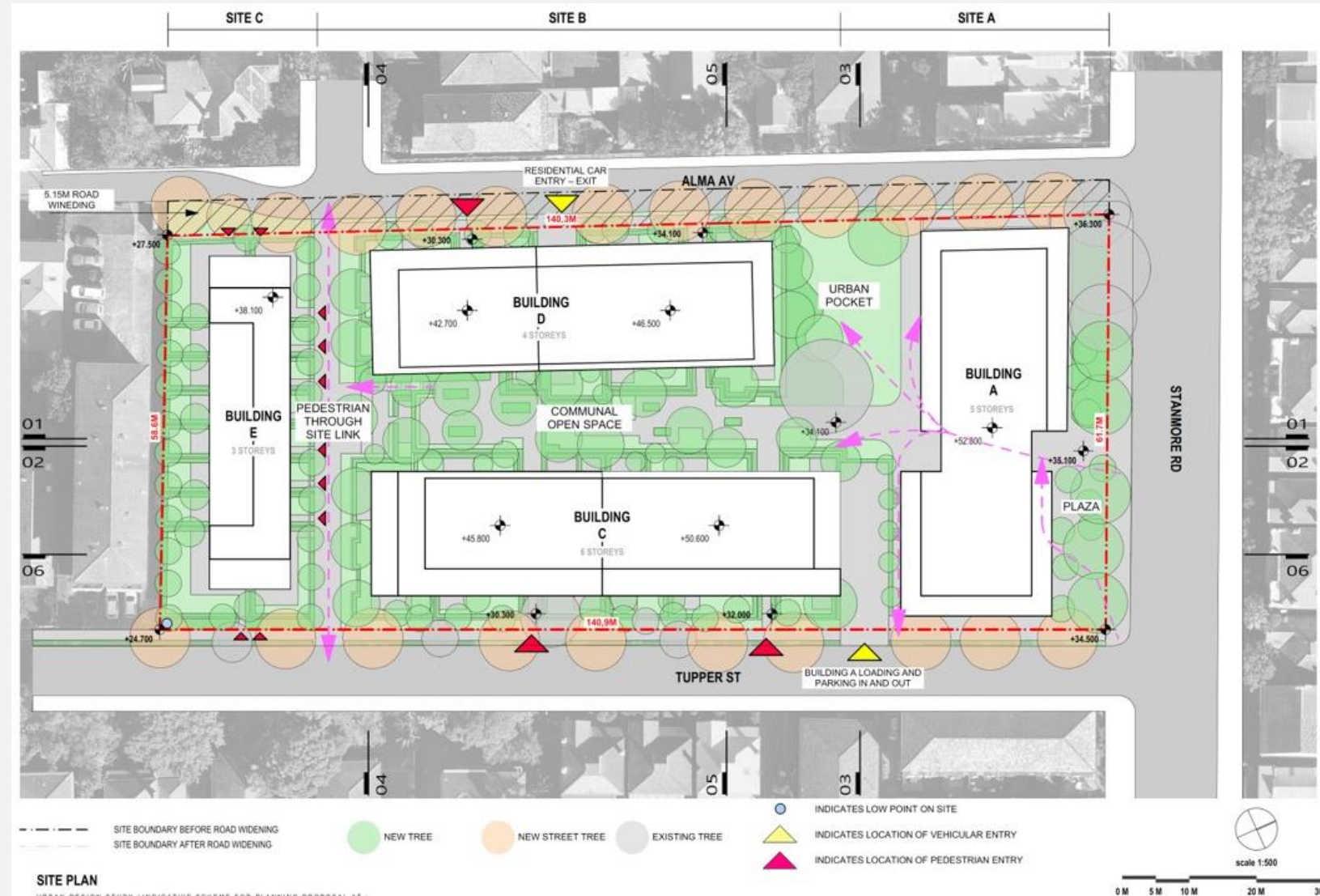
- Increasing development density in an inner-urban area with high levels of residential amenity and good access to employment, educational, cultural facilities and public transport
- Increasing the sustainable utilisation of existing operational urban land and local infrastructure
- Securing the ongoing viability of an enduring community club facility
- Minimising environmental impacts - primarily maintaining appropriate solar access to existing neighbours, managing traffic generation
- Achieving a positive urban character and contextual response in terms of building bulk and scale
- Providing inherent public benefit in the form of new publicly accessible open spaces, lanes and the proposed plaza
- Enabling a compatible built form with the surrounding heritage characteristics



Sketch of Public Plaza (Source: PA Studios)

The Master Plan

- Provides an overall master plan for the site indicating: land uses; building siting and footprints; open space and through site links; vehicle access points.
- Maintains Cyprus Club's activities as a key element of the Site
- Ensures high levels of residential amenity
- Provides connectivity using through site links
- Encourages social interaction
- Ensures complementary land uses within the site
- Is sympathetic to surrounding properties and heritage character of locality

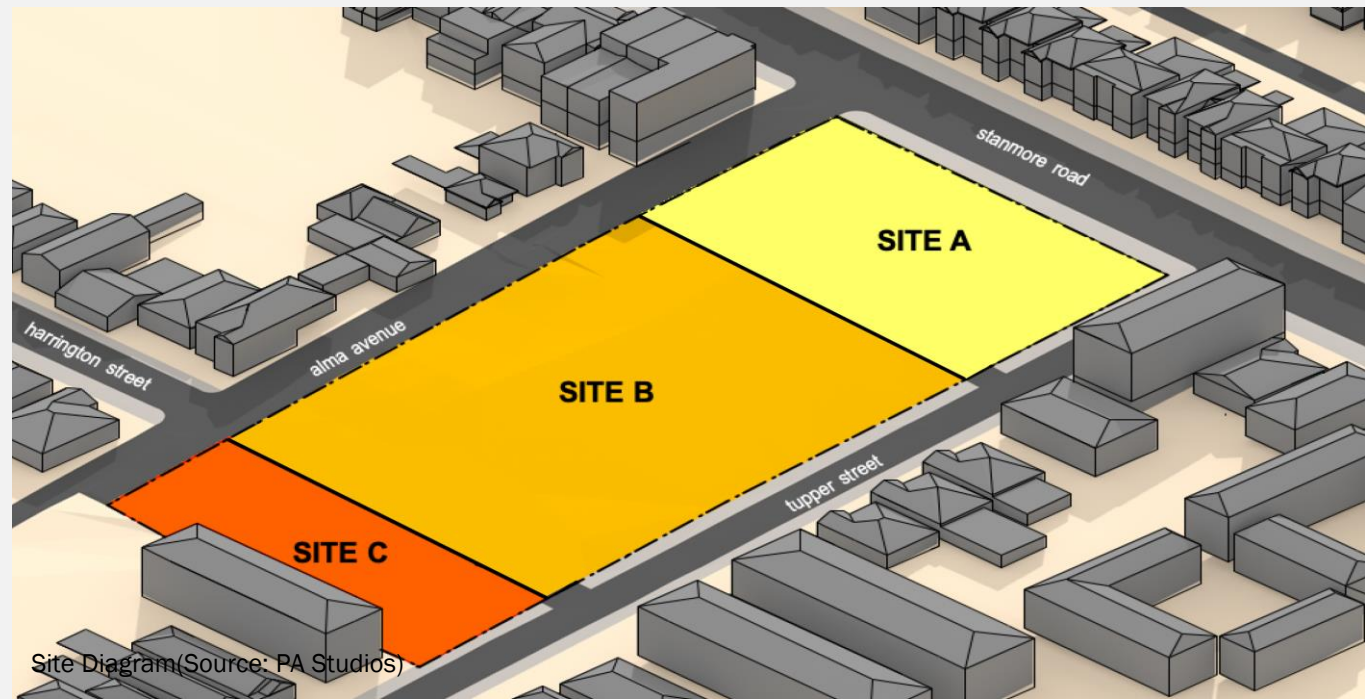
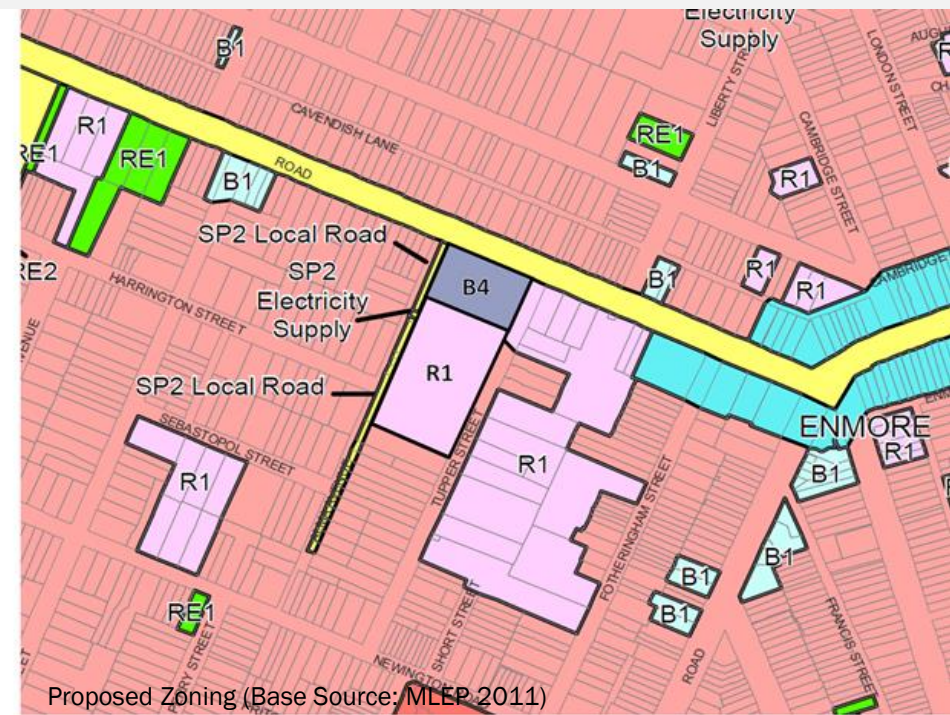


Site Plan (Source: PA Studios)

Land Use

- Site A: B4 Mixed Use zone. Land use to be 'registered club', 'commercial premises' and 'residential flat building'(RFB). RFBs are permitted via a proposed Additional Permitted Use in Schedule 1 of the MLEP 2011
- Site A to include a minimum 1500 sqm GFA of registered club and commercial
- Site B: R1 General Residential zone. Land use to be 'RFB' consistent with R1 land use table per the MLEP 2011
- Site C: R1 General Residential zone. Land use to be townhouses ('multi dwelling housing') consistent with R1 land use table per the MLEP 2011

Zone	
B1	Neighbourhood Centre
B2	Local Centre
B4	Mixed Use
B5	Business Development
B6	Enterprise Corridor
B7	Business Park
IN1	General Industrial
IN2	Light Industrial
R1	General Residential
R2	Low Density Residential
R3	Medium Density Residential
R4	High Density Residential
RE1	Public Recreation
RE2	Private Recreation
SP1	Special Activities
SP2	Infrastructure
W1	Natural Waterways
W2	Recreational Waterways



Built Form Typology

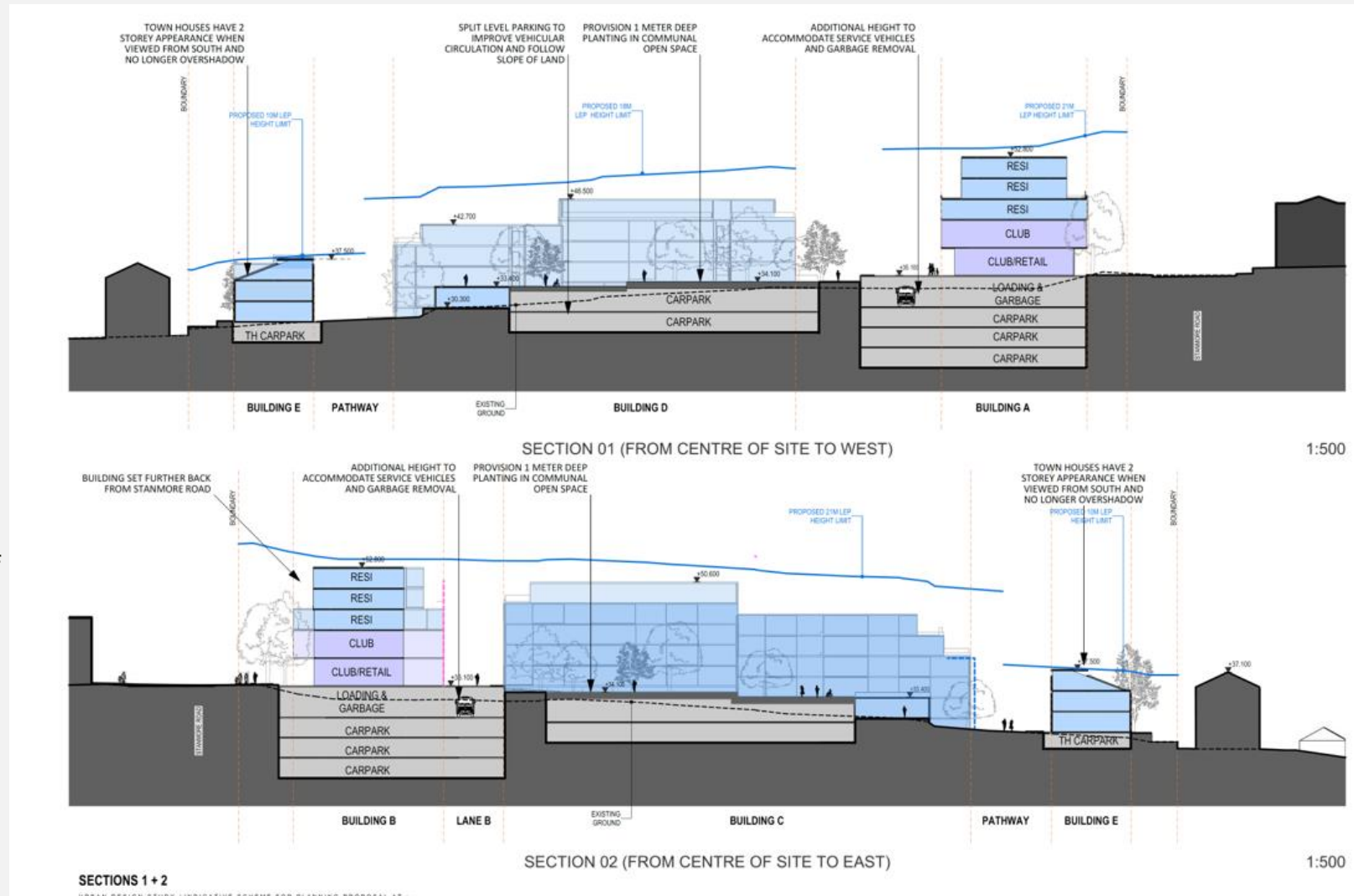
- Numeric development standards including heights and setbacks
- Controls will ensure the proposed buildings reflect the natural slope of the land by stepping down the slope
- New buildings are of exceptional design quality
- New residential development provides good amenity for residents and does not adversely impact on existing surrounding development
- Variations in parapet walls and/or roof form are required to create a varied skyline
- The architectural expression and articulation of buildings are to emulate the fine-grained built form of the surrounding area



Proposed Height of Building Map (Base Source: MLEP 2011)

Building Heights

- Heights are to be expressed in storeys
- Building A - maximum 5 storeys with the top storey setback a minimum of 3 metres on all sides from the external wall of the floor below
- Building B - maximum 5 storeys with top storey setback a minimum 3 metres on all sides from the external wall of the floor below
- Building C - maximum 6 storeys with the top storey set back a minimum of 3 metres from the Tupper Street external wall of the floor below and a minimum of 6 metres from the Lane A external wall of the floor below
- Building D - maximum 4 storeys with the top storey setback a minimum of 3 metres from the Alma Avenue external wall of the floor below
- Building E - maximum 3 storeys



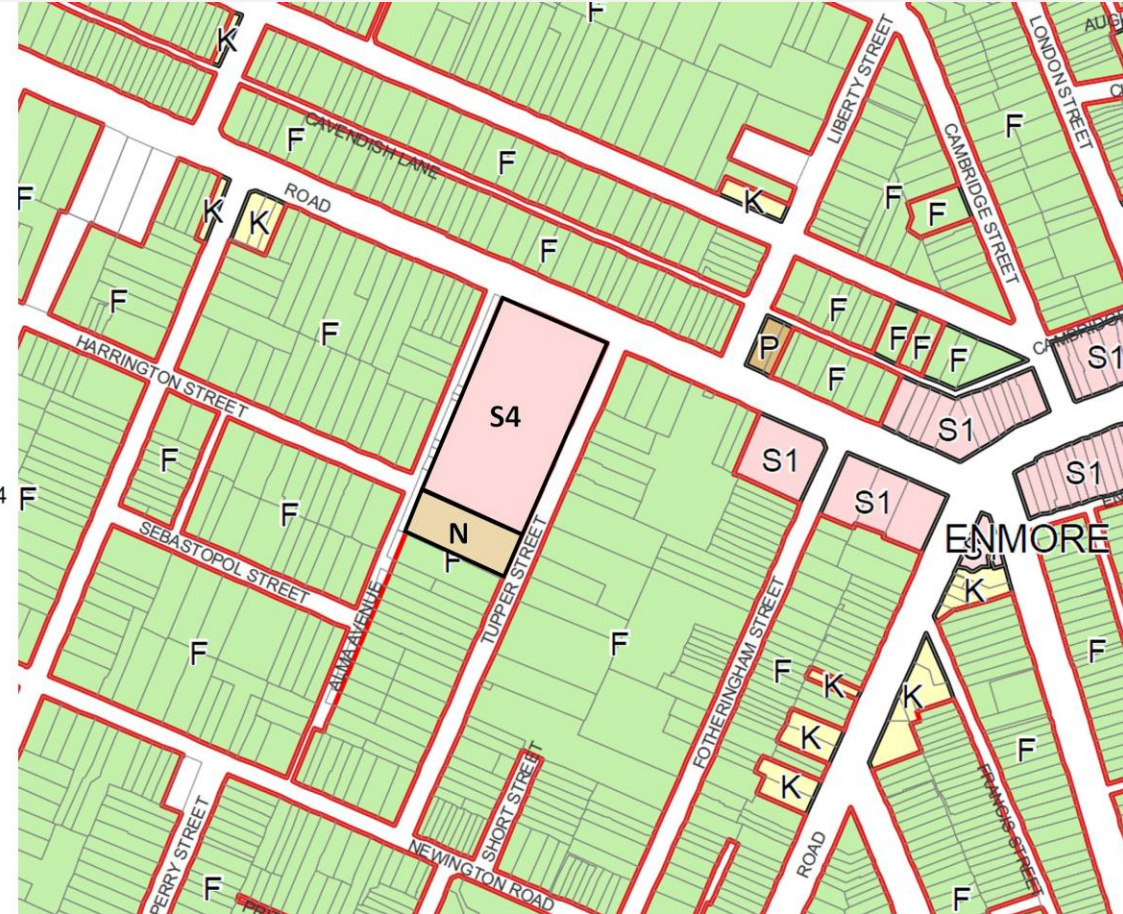
Indicative building heights (Source: PA Studios)

Floor Space Ratio

- The maximum FSR for any development must be consistent with the standards prescribed in the MLEP 2011 Floor Space Ratio Map
- The FSR controls for the precinct have been determined to achieve the desired future built form

Maximum Floor Space Ratio (n:1)

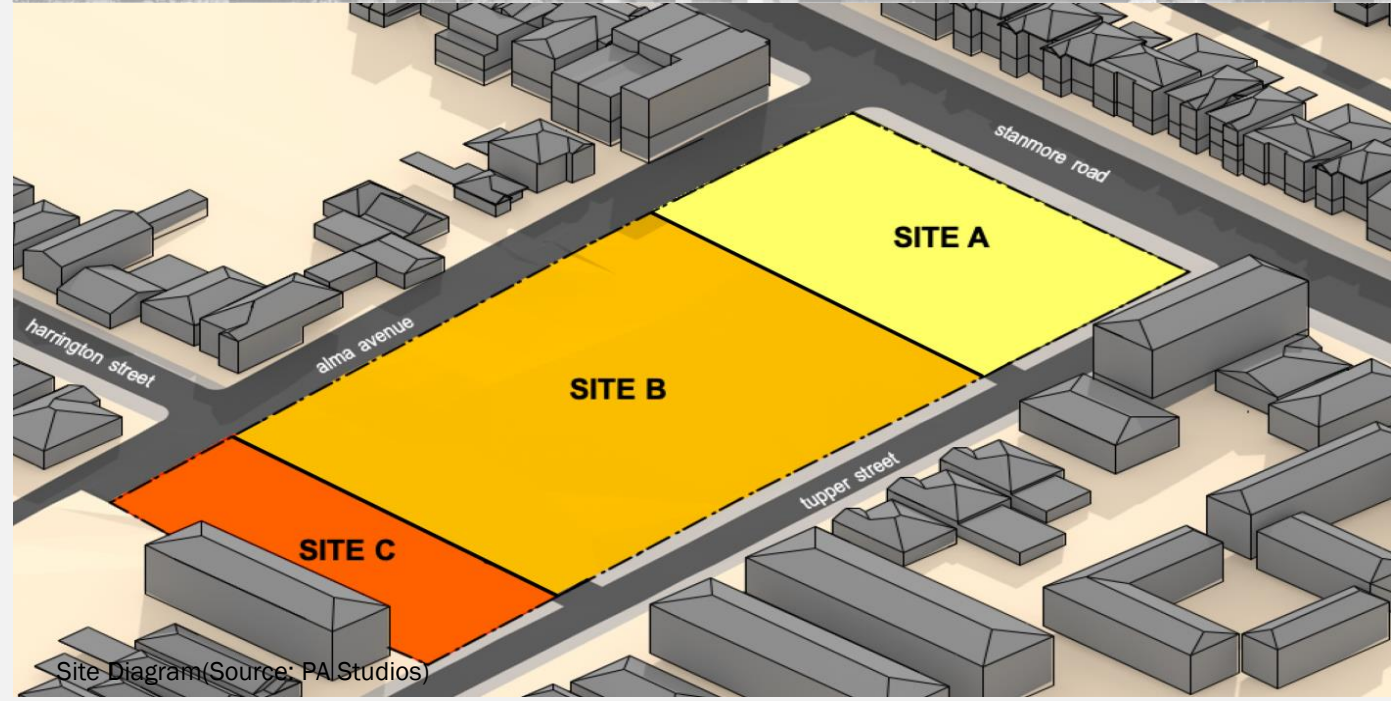
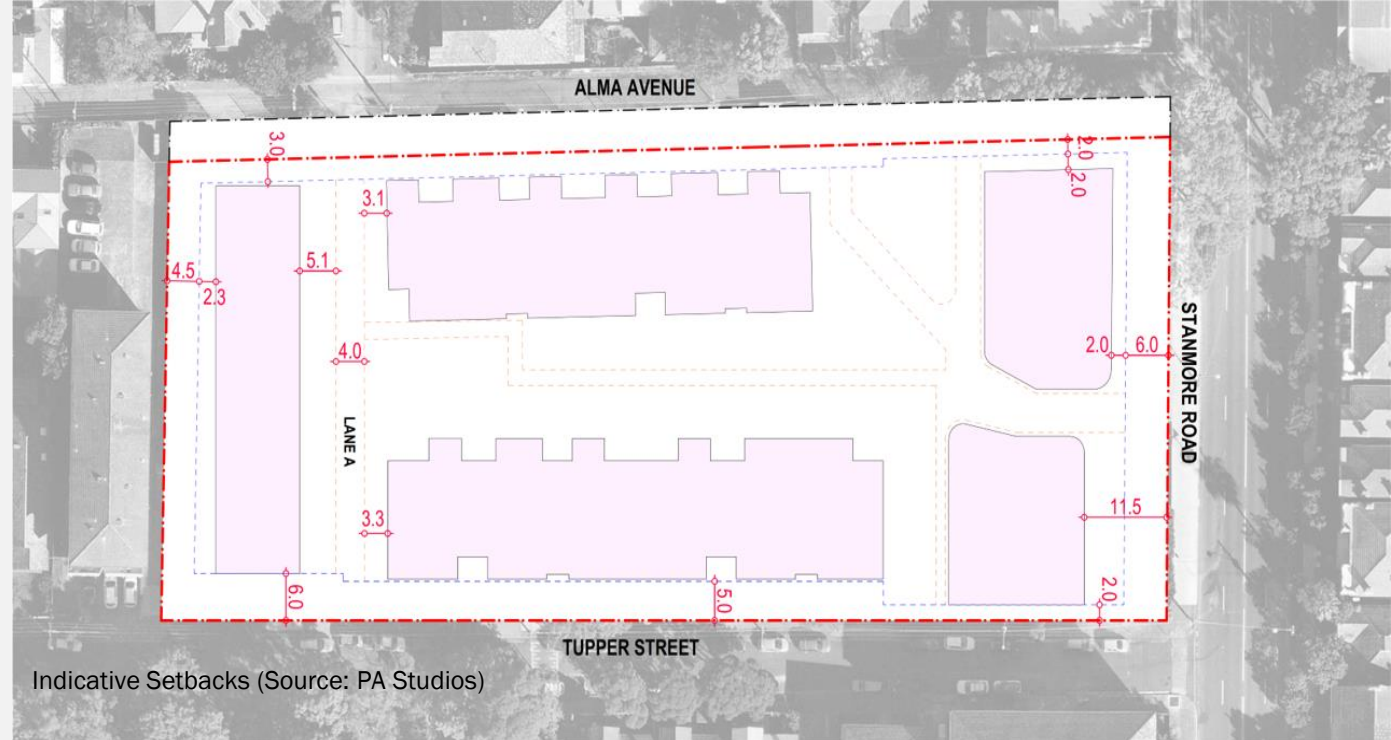
F	0.60	S1	1.50	T6	2.40
H	0.70	S2	1.60	U1	2.50
I	0.75	S3	1.70	U2	2.60
J	0.80	S4	1.75	U3	2.70
K	0.85	S5	1.80	U4	2.80
L	0.90	S6	1.85	V1	3.00
M	0.95	S7	1.90	V2	3.10
N	1.00	T1	2.00	V3	3.30
O	1.10	T2	2.05	V4	3.40
P	1.20	T3	2.10	W1	3.50
Q	1.30	T4	2.20	W2	3.70
R	1.40	T5	2.30		Refer to clause 4.4



Proposed Floor Space Ratio Map (Base Source: MLEP 2011)

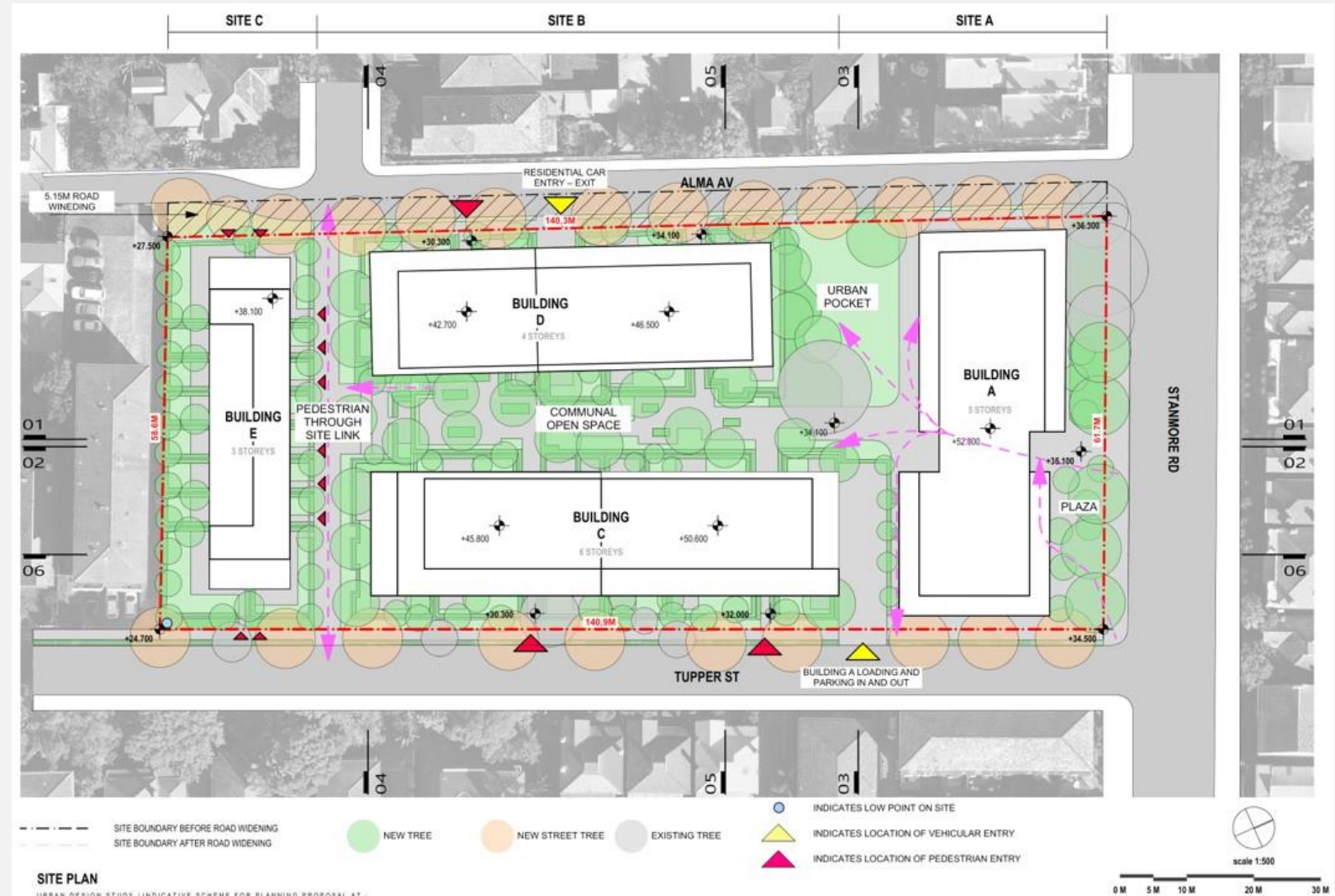
Setbacks

- Site A: minimum setback of 2 metres from Alma Avenue (after road widening) and Tupper Street. Minimum 6 metre setback from Stanmore Road
- Site B: minimum 5 metres to Tupper Street. Minimum 3 metres to Alma Avenue (after road widening)
- Site C: minimum 4.5 metre setback to Tupper Street. Minimum 3 metres to Alma Avenue (after road widening)



Landscape

- Landscaping, particularly deep soil plantings, across the site, will be maximized
- A range of spaces will be provided across the site to allow for different forms of recreation
- Canopy tree planting to be incorporated along street frontages
- Landscaping to incorporate appropriate mix of native species to promote biodiversity outcomes
- Significant trees on the site to be retained
- Open space to deliver deep soil landscaping and soft landscaping spaces
- Provide opportunities for tree planting to occur on the streets or within front setbacks
- The design will maximise areas for soft landscaping and deep soil within the site consistent with the ADG



Site Plan (Source: PA Studios)

Public Open Spaces

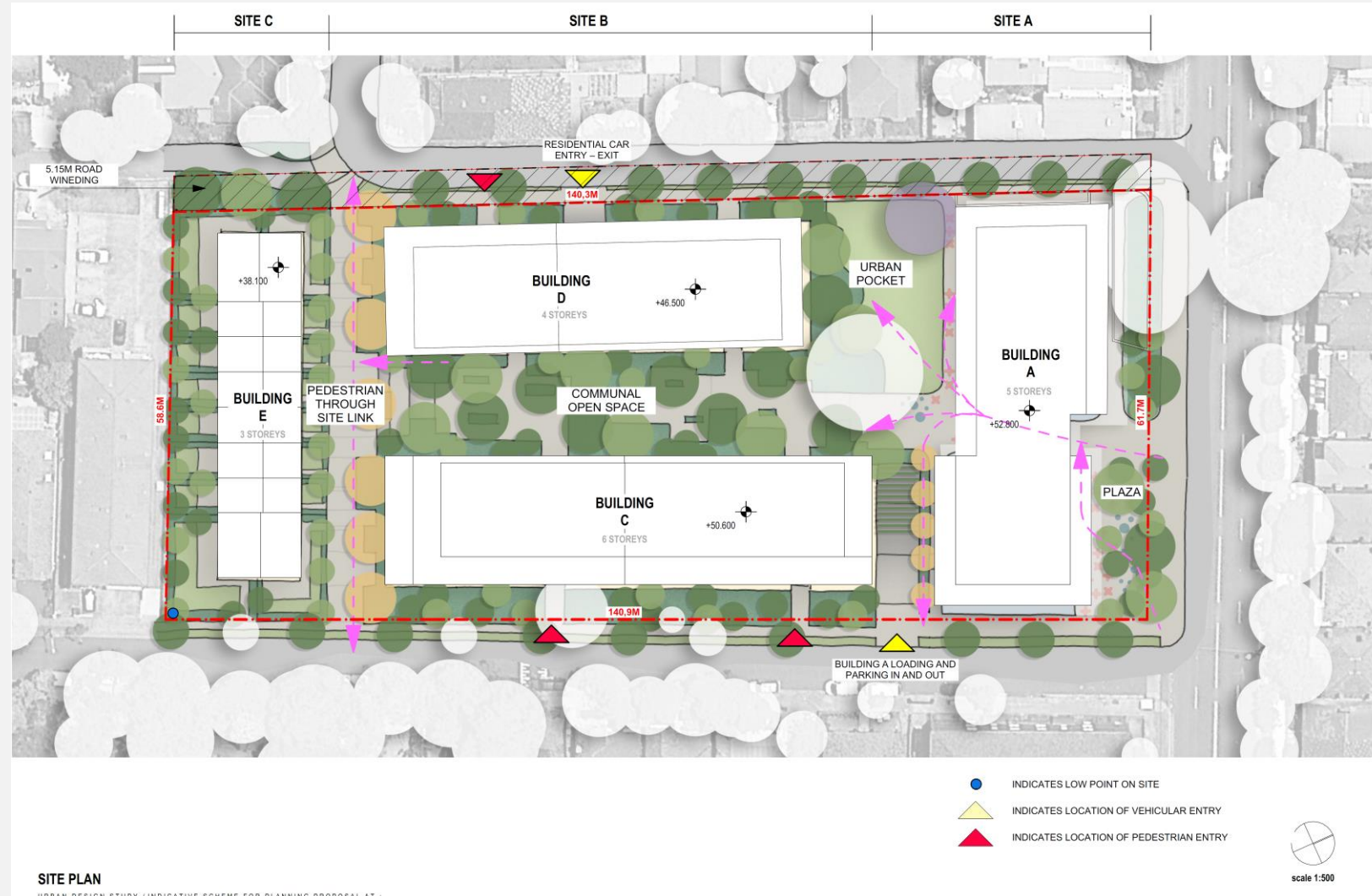
- Include a new Urban Pocket Park off Alma Street. To be landscaped and fronted by active uses.
- Include a new north facing plaza or “Plateia” facing Stanmore Road. To incorporate landscaping and outdoor dining opportunities.
- The pocket park and plaza designs to be configured to provide intimacy of the human scale amenity inclusions including seating, large canopy shade tree plantings and the addition of softscape planting.



Indicative public open space plan (Source: PA Studios)

Pedestrian Access and Connectivity

- Controls will reinforce the importance of Pedestrian connections
- The Site shall provide publicly accessible Pedestrian links to improve permeability through the Site and with connections to surrounding neighbourhoods
- This includes north-south linkages from Stanmore Road and east-west linkages between Tupper Street and Alma Avenue
- The Central Communal Open Space shall be publicly accessible and connected to through-site links



Site Plan (Source: PA Studios)

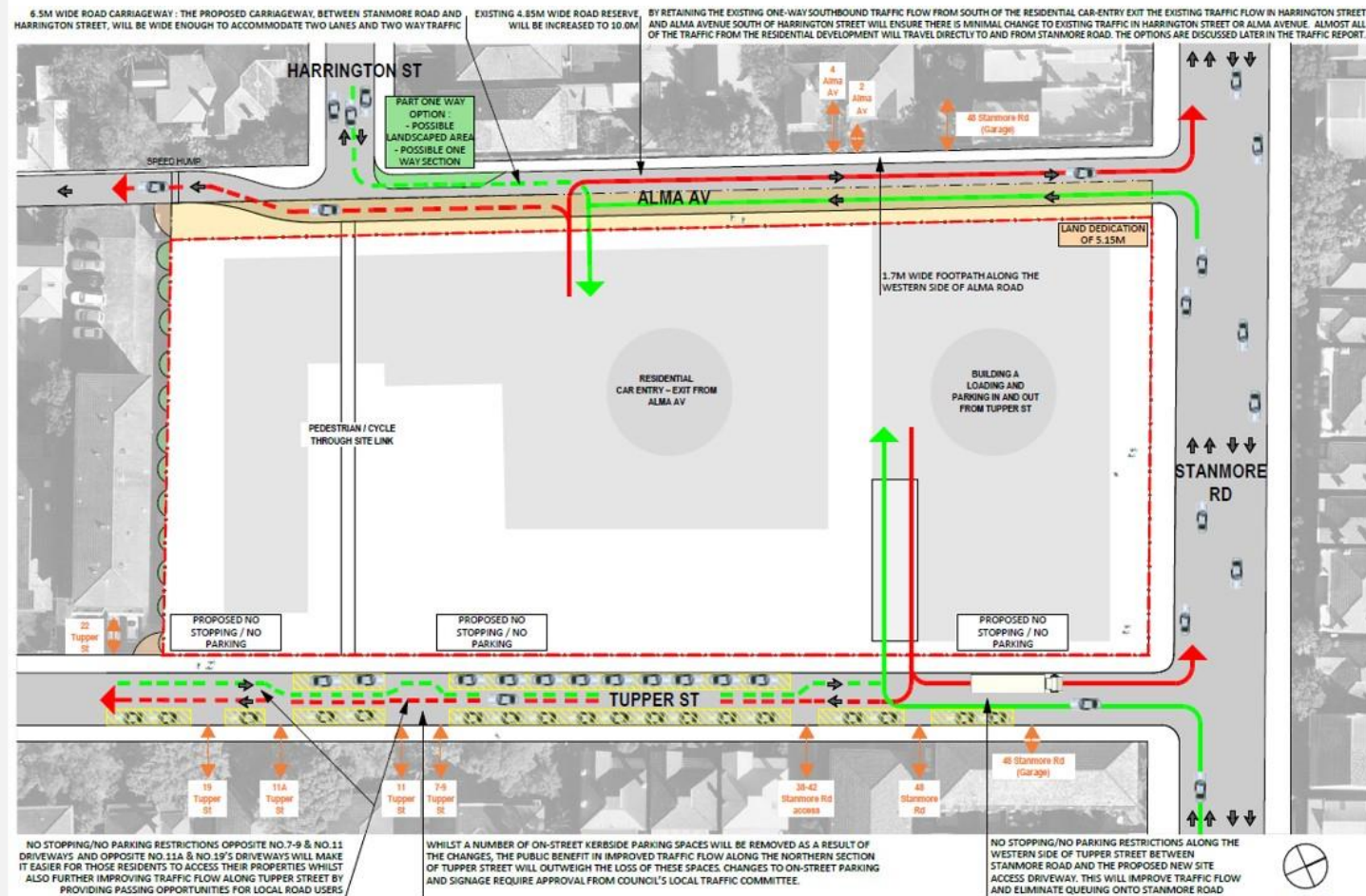
Vehicle Access and Car Parking

Vehicle Access

- Vehicular access to Site A (club and retail/commercial space) to be provided from Tupper Street
- Vehicular access to Site B (residential apartments) and Site C (townhouses) to be provided midway from Alma Avenue

Car Parking

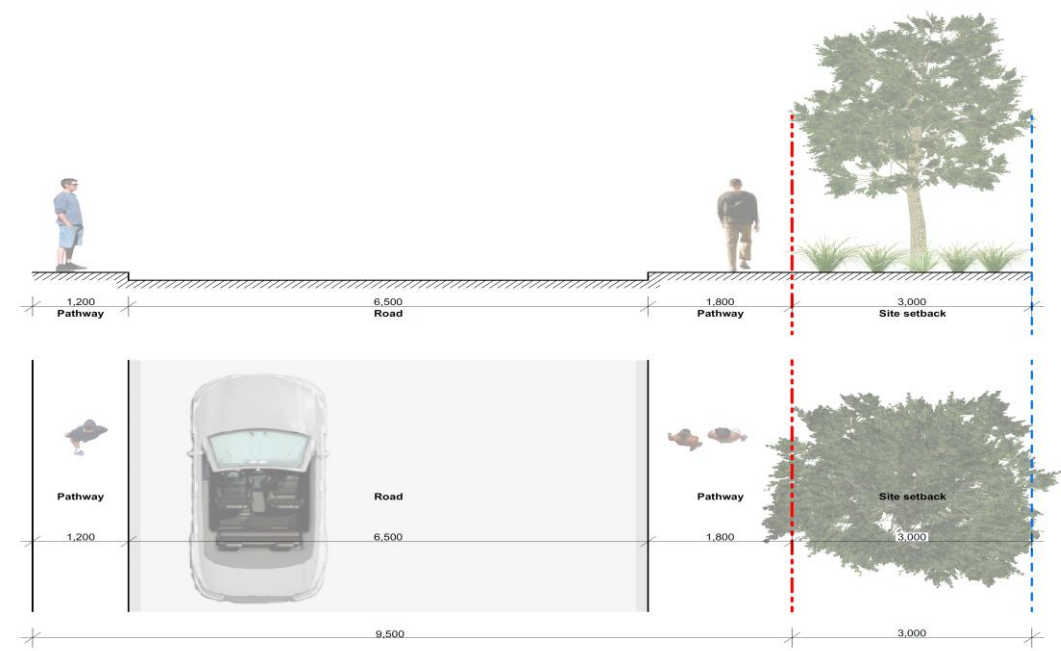
- Off-street parking to be provided in basement parking areas
- Parking rates are to be provided based on a traffic study aiming to minimise local area impacts.
- Travel demand measures such as Green Travel Plans to be required



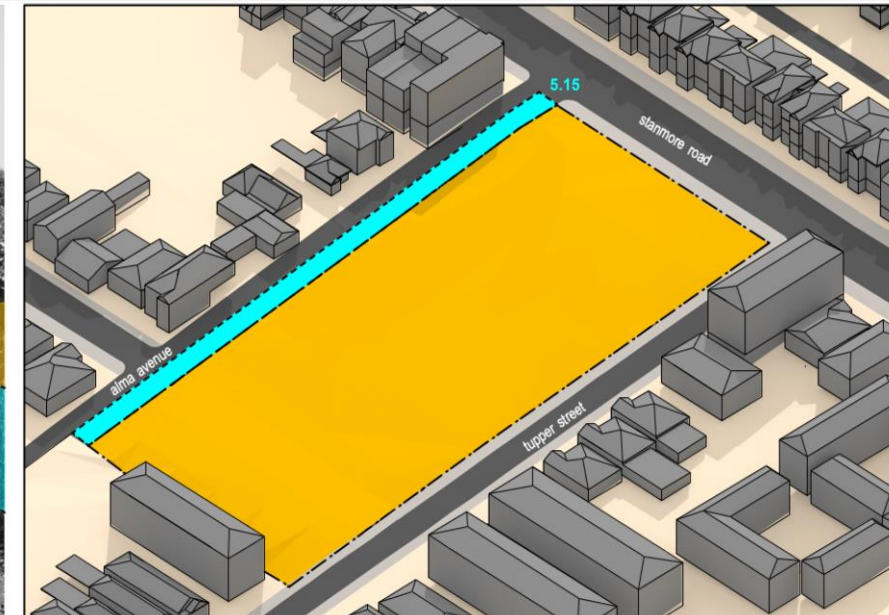
Future Traffic Circulation (Source: PA Studios)

Road Widening

- Future redevelopment of the Site shall make provision to expand Alma Avenue for the entire length of the Site in accordance with Council's requirements
- Provide a 1.2 m wide western footpath, with a 6.5 m road carriageway facilitating two-way traffic flow, and a 1.8 m wide eastern footpath along the site frontage (total road reserve width of 9.5 m)



ALMA AVENUE
VIEW FACING NORTH



source : kennedy associates architects
ROAD WIDENING

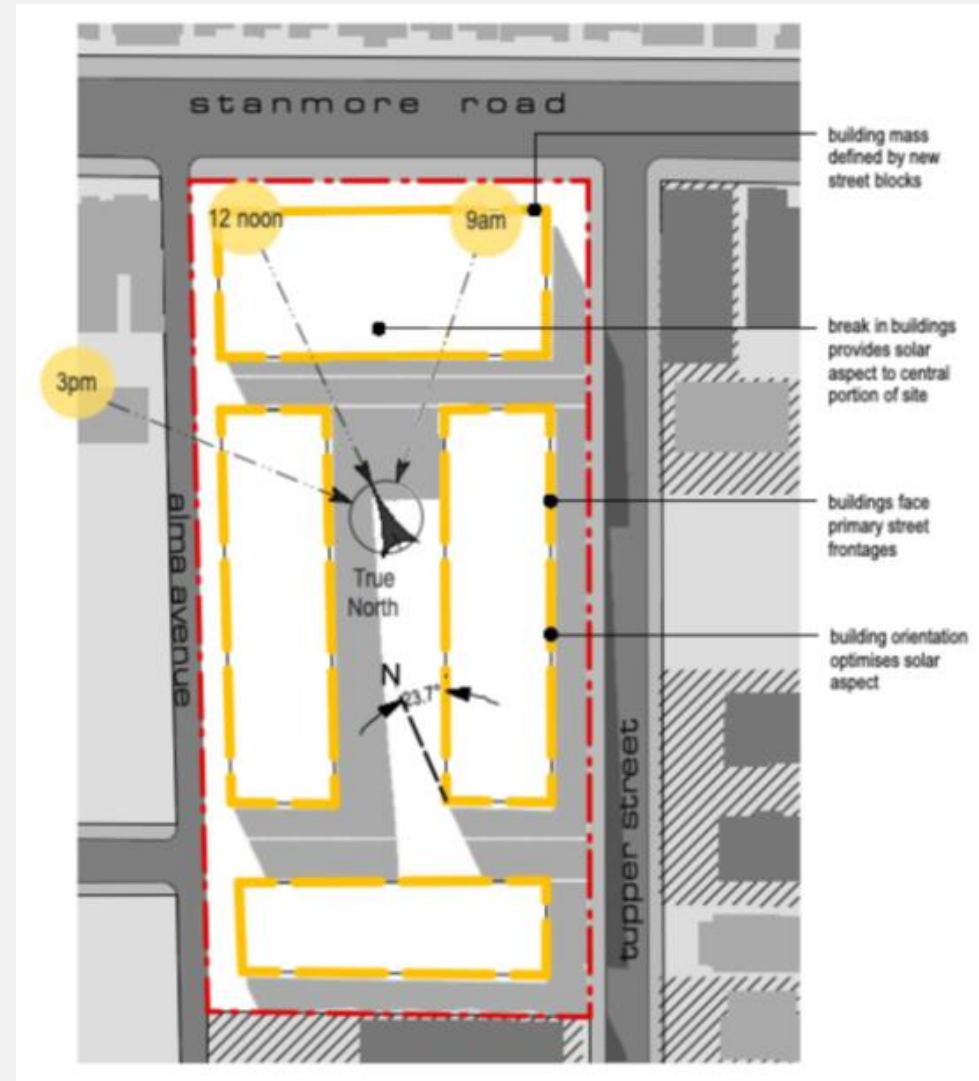
location	width	area (m ²)
ALMA AVENUE	5.15m	697

Indicative Road Widening Plan(Source: PA Studios)

*see diagram PP402 for road widening details

Solar Access and Overshadowing

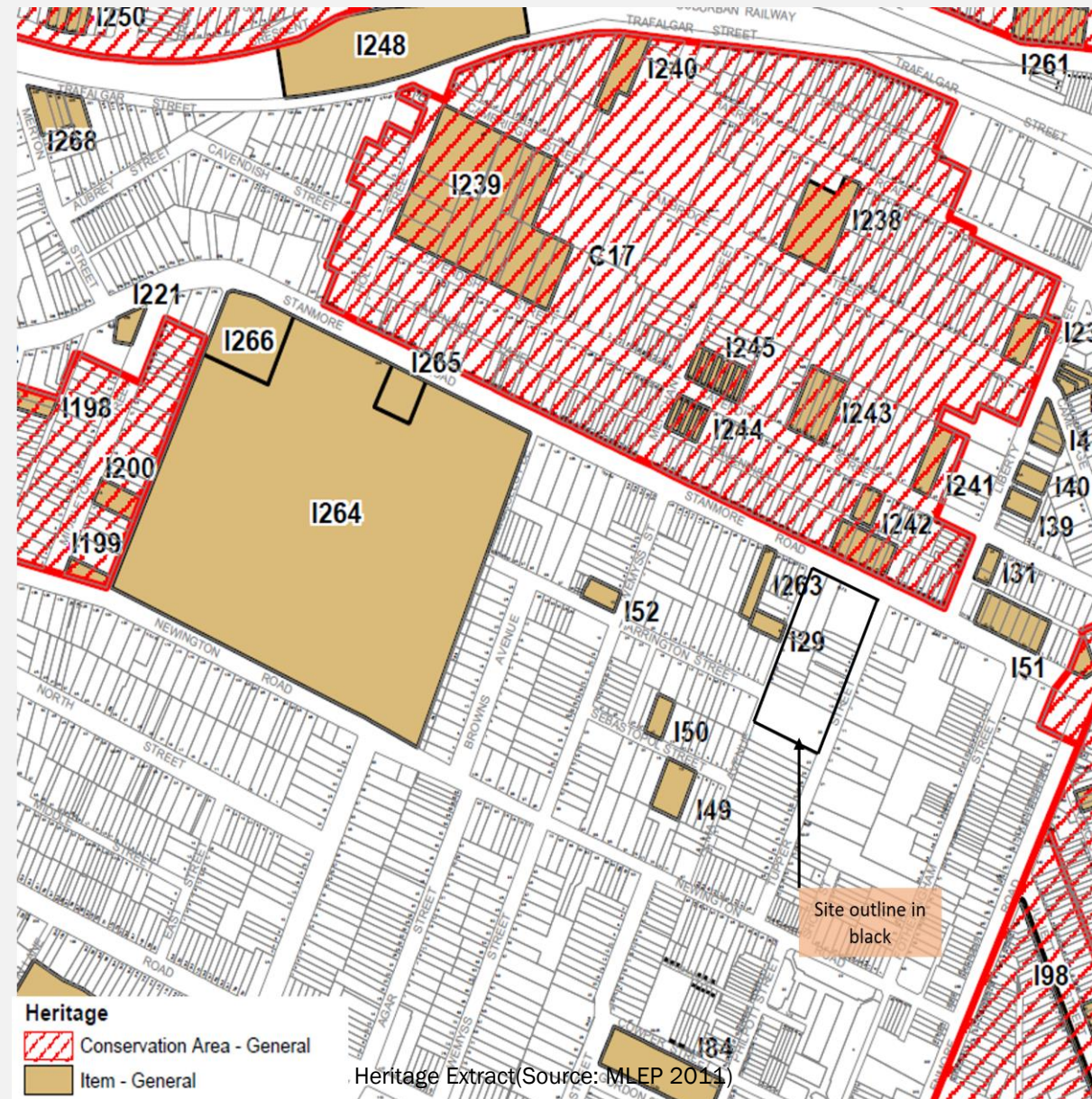
- Solar access to future dwellings and communal open spaces shall be consistent with the Apartment Design Guide
- Overshadowing shall be minimised through design outcomes
- Solar access enjoyed by neighbour's shall be protected



Solar Access and overshadowing (Source: PA Studios)

Heritage

- The development shall be compatible with the Kingston South Heritage Conservation Area
- The proposal shall be compatible with the controls outlined in part 8, 8.2.19 Kingston South Heritage Conservation Area – HCA 17 of the Marrickville DCP 2010
- Built form shall respond to the corresponding heritage character
- Building materials including render and face brick shall be incorporated to reflect and complement common materials in the locality



Sustainability

- Sustainability measures and building performance standards will be included
- Require BASIX for future development applications
- Implement Environmentally Sustainable Development principles and initiatives such as:
 - Green Star Design, Built and Communities (5 Star)
 - Exceed minimum water targets and encourage sustainable water management practices
- Maximise passive design strategies (ie, daylighting, cross-ventilation, adequate shading).
- Promote the use of low emission vehicles by providing electric vehicle charging and dedicated parking for small car.



Sustainability Cycle (Source: GlassShape)

Waste Management

Waste Management

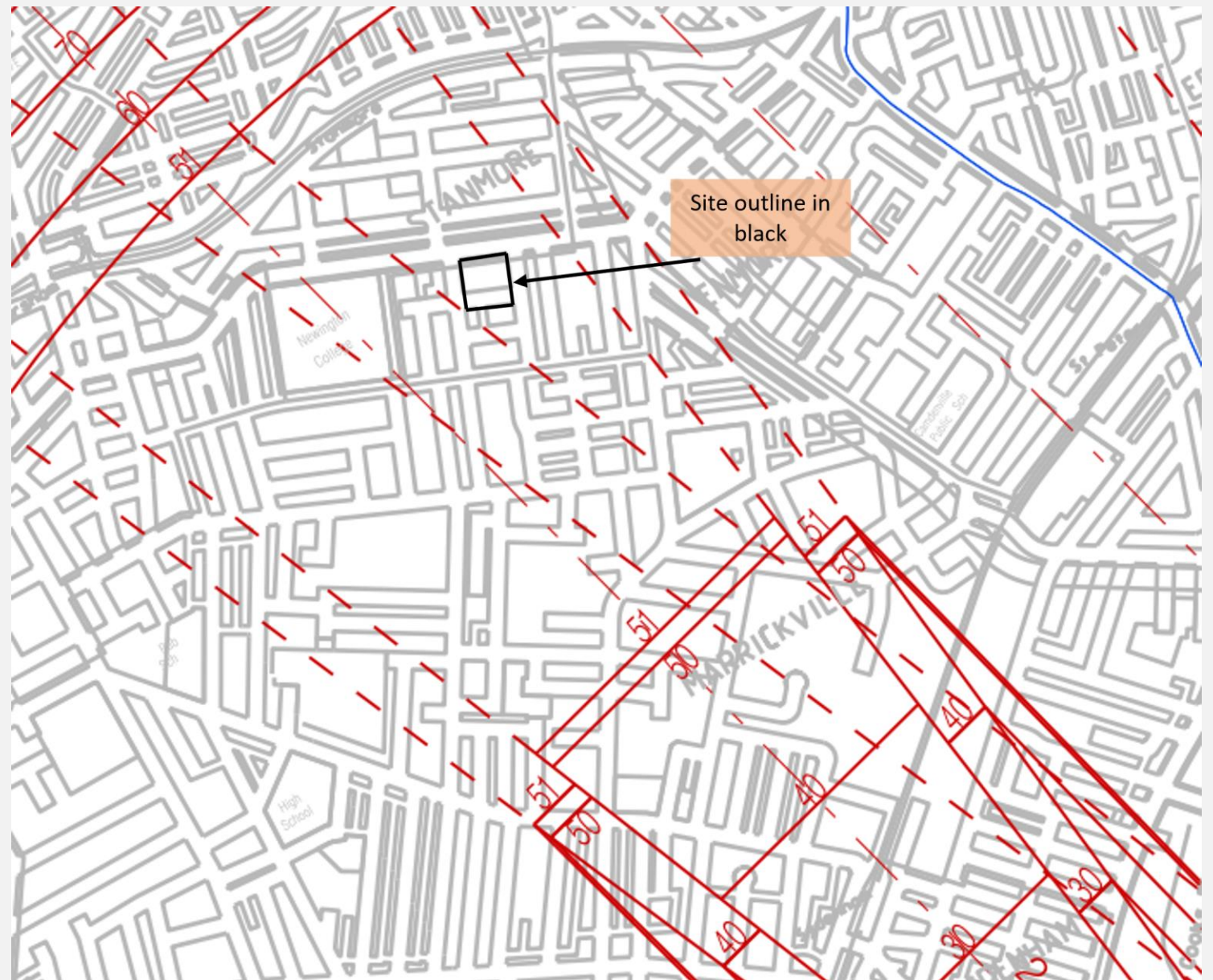
- A detailed waste management plan to be submitted as part of any development application for the Site.
- Plans are required to show separate bin storage areas for residential and commercial uses, bulky waste/clean up room and waste collection points.
- Promote the principles of ecologically sustainable development (ESD) through waste avoidance, resource recovery, recycling and alternate waste treatment method
- Development to accord with control 2.21 Site Facilities and Waste Management of the Marrickville DCP 2010



Waste Management (Source: Australia's National Waste Management Strategy 2019)

Operation of Sydney Airport

- To ensure development does not adversely affect the ongoing operation of Sydney Airport or its ability to grow in accordance with the Airport's approved masterplan.
- Maximum building heights shall not exceed 55.4 m AHD inclusive of all lift over-runs.
- New development must not incorporate reflective materials as part of the walls, windows or roofing structure.
- Where construction cranes are required to operate at a height greater than that of the proposed development, approval is required to be obtained from the Civil Aviation Safety Authority



OLS Map Extract (Source Sydney Airport Obstacle Height Limitation Map)

Noise and Vibration

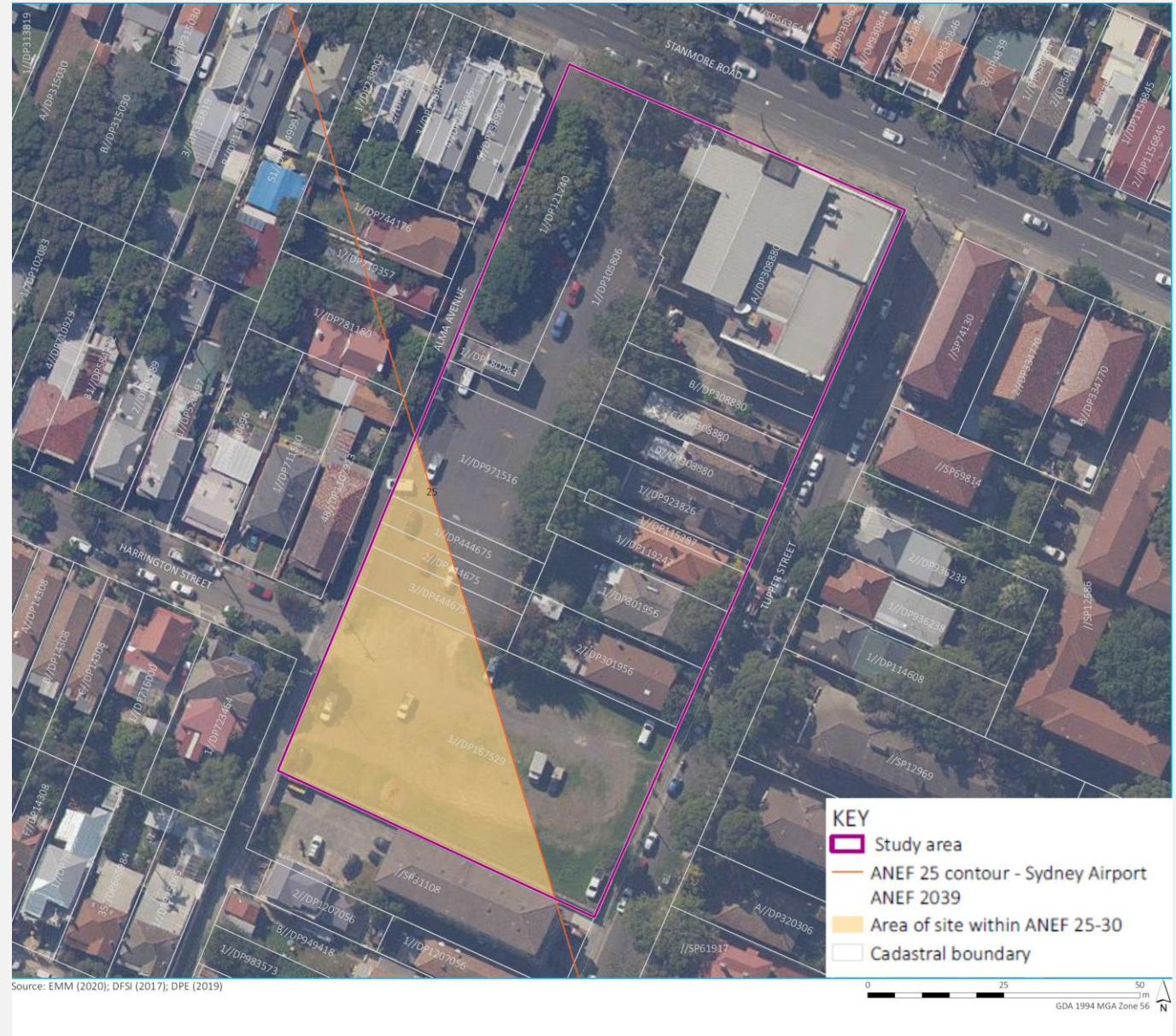
- To ensure that development on the Site
 - is not unreasonably impacted by traffic or aircraft noise
 - does not unreasonably impact on the amenity of residential and other sensitive land uses by way of noise or vibration

Impacts of traffic and aircraft noise

- Buildings design requirements to include measures such as winter gardens, glazed windows and doors, external wall and rooves to be of masonry construction to protect the amenity of future residence

Impacts of noise from the Site

- Mechanical services noise emissions to comply with relevant EPA criteria
- Operational noise from the Club to comply with relevant Office of Liquor and Gaming and EPA acoustic criteria.
- Detailed acoustic assessment of operational noise impacts to be undertaken at DA stage



Site and Sydney Airport ANEF(Source: EMM Consulting)

Contamination

- Site to be remediated to an acceptable standard to accommodate residential development
- Development to accord with control 2.24 Contaminated Land of the Marrickville DCP 2010

